

Public Document Pack



PLANNING COMMITTEE

Tuesday, 20th November, 2018 at 7.30 pm
Venue: Conference Room
Civic Centre, Silver Street,
Enfield EN1 3XA

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MEMBERS

Councillors : Mahmut Aksanoglu (Chair), Maria Alexandrou, Chris Bond,
Sinan Boztas, Elif Erbil, Ahmet Hasan, Gina Needs, Sabri Ozaydin,
Michael Rye OBE, George Savva MBE, Jim Steven and Mahtab Uddin

N.B. Any member of the public interested in attending the meeting should ensure that they arrive promptly at 7:15pm
Please note that if the capacity of the room is reached, entry may not be permitted. Public seating will be available on a first come first served basis.

Involved parties may request to make a deputation to the Committee by contacting the committee administrator before 12:00 noon on 19/11/18

AGENDA – PART 1

1. **WELCOME AND APOLOGIES FOR ABSENCE**
2. **DECLARATION OF INTERESTS**

Members of the Planning Committee are invited to identify any disclosable pecuniary, other pecuniary or non pecuniary interests relevant to items on the agenda.

3. **REPORT OF THE ASSISTANT DIRECTOR, REGENERATION AND PLANNING (REPORT NO.119) (Pages 1 - 2)**

To receive the covering report of the Assistant Director, Regeneration & Planning.

4. **18/02191/FUL - LAND TO EAST OF 95 VICARS MOOR LANE, LONDON, N21 1BL (Pages 3 - 24)**

RECOMMENDATION: Approval subject to conditions
WARD: Winchmore Hill

5. **18/02237/FUL - ABERDEEN PARADE, ANGEL ROAD, LONDON, N18 2EB** (Pages 25 - 52)

RECOMMENDATION: Approval subject to conditions
WARD: Edmonton Green

6. **18/03000/RE4 - DE BOHUN PRIMARY SCHOOL, GREEN ROAD, NEW SOUTHGATE, N14 4AD** (Pages 53 - 72)

RECOMMENDATION: In accordance with Regulation 4 of the Town and Country Planning General Regulations 1992, planning permission be deemed to be granted subject to conditions
WARD: Cockfosters

7. **18/03001/LBC - DE BOHUN PRIMARY SCHOOL, GREEN ROAD, NEW SOUTHGATE, N14 4AD** (Pages 73 - 92)

RECOMMENDATION: That the Head of Development Management / Planning Decisions Manager(s) be authorised to finalise conditions and to grant listed building consent subject to conditions
WARD: Cockfosters

8. **18/02514/FUL - PONDERS END INDUSTRIAL ESTATE, EAST DUCK LEES LANE, ENFIELD, EN3 7SP** (Pages 93 - 138)

RECOMMENDATION: That subject to the completion of a S106 Agreement to secure the obligations set out in the report the Head of Development Management / Planning Decisions Manager(s) be authorised to grant planning permission subject to conditions
WARD: Ponders End

9. **18/03009/FUL - GENOTIN ROAD CAR PARK, GENOTIN ROAD, ENFIELD, EN1 2AG** (Pages 139 - 212)

RECOMMENDATION: That subject to the completion of the S106 Agreement the Head of Development Management be authorised to grant planning permission subject to conditions
WARD: Grange

10. **ADDITIONAL MEETING OF PLANNING COMMITTEE**

To consider scheduling an additional meeting of the Planning Committee.

MUNICIPAL YEAR 2018/2019 - REPORT NO 119

COMMITTEE:
 PLANNING COMMITTEE
 20.11.2018

AGENDA - PART 1	ITEM 3
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SUBJECT -

MISCELLANEOUS MATTERS

REPORT OF:
 Assistant Director, Regeneration
 and Planning

Contact Officer:
 Planning Decisions Manager
 David Gittens Tel: 020 8379 8074
 Kevin Tohill Tel: 020 8379 5508

3.1 APPLICATIONS DEALT WITH UNDER DELEGATED POWERS INF

3.1.1 In accordance with delegated powers, 186 applications were determined between 18/10/2018 and 7/11/2018, of which 136 were granted and 50 refused.

3.1.2 A Schedule of Decisions is available in the Members' Library.

Background Papers

To be found on files indicated in Schedule.

3.2 PLANNING APPLICATIONS AND APPLICATIONS TO DISPLAY ADVERTISEMENTS DEC

On the Schedules attached to this report I set out my recommendations in respect of planning applications and applications to display advertisements. I also set out in respect of each application a summary of any representations received and any later observations will be reported verbally at your meeting.

Background Papers

- (1) Section 70 of the Town and Country Planning Act 1990 states that the Local Planning Authority shall have regard to the provisions of the development plan, so far as material to the application, and to any other material considerations. Section 54A of that Act, as inserted by the Planning and Compensation Act 1991, states that where in making any determination under the Planning Acts, regard is to be had to the development, the determination shall be made in accordance with the plan unless the material considerations indicate otherwise. The development plan for the London Borough of Enfield is the London Plan (March 2015), the Core Strategy (2010) and the Development Management Document (2014) together with other supplementary documents identified in the individual reports.
- (2) Other background papers are those contained within the file, the reference number of which is given in the heading to each application.

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LONDON BOROUGH OF ENFIELD

PLANNING COMMITTEE

Date: 20th November 2018

Report of

Executive Director - Place

Contact Officer:

Andy Higham / Kevin Tohill
Tel No: 0208 379 3848 / 5508

Ward:

Winchmore Hill

Application Number: 18/02191/FUL

Category: Minor

LOCATION: Land To East Of 95 Vicars Moor Lane
London
N21 1BL

PROPOSAL: Construction of a 5-bedroom single family dwelling with rooms in loft space. (Revised)

Applicant Name & Address:

Peter Petrou
95 Vicars Moor Lane
London
N21 1BL

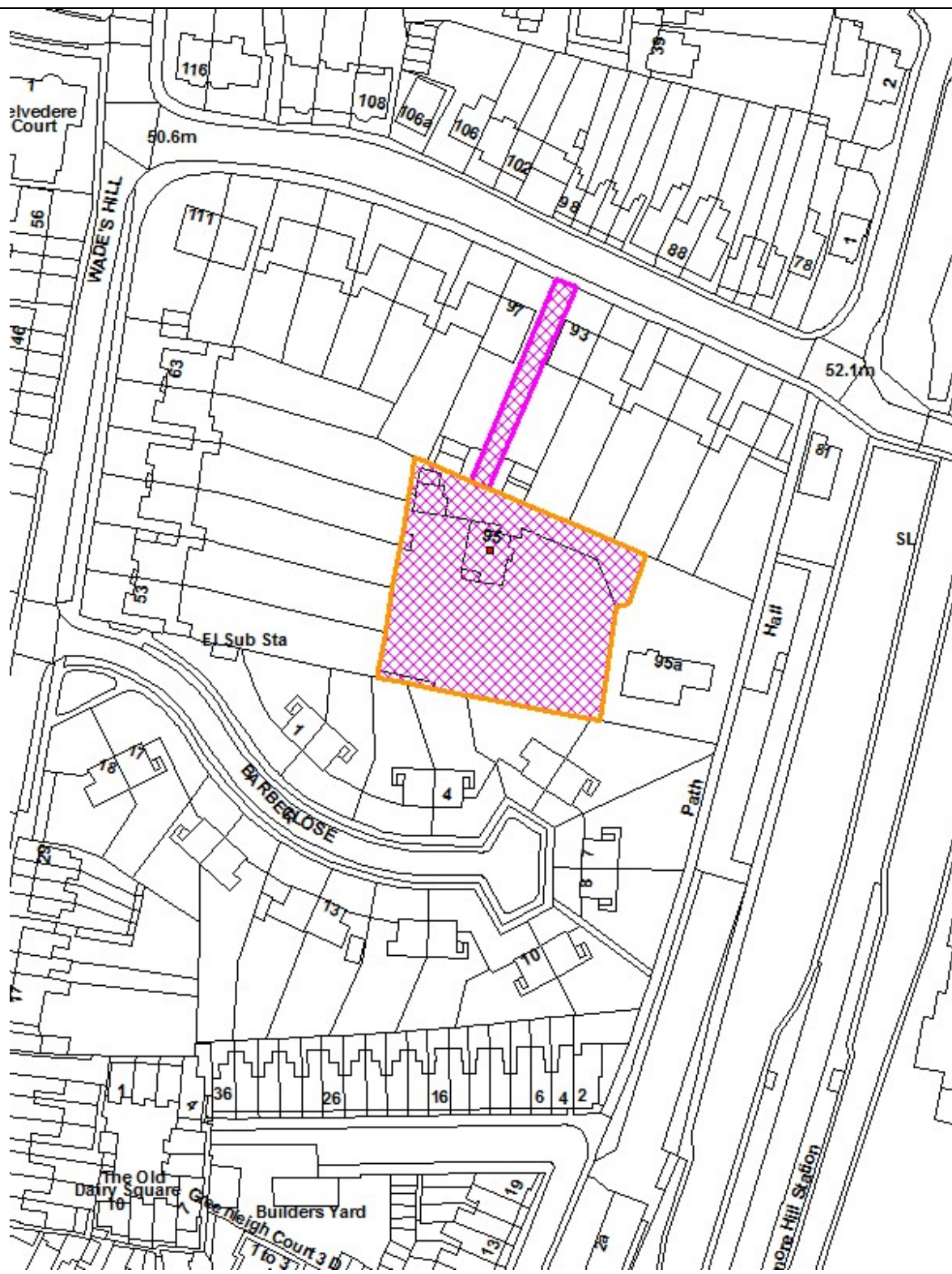
Agent Name & Address:

Kieran Rafferty
183 Seafield Road
Bournemouth
BH6 5LJ

RECOMMENDATION: Grant planning permission subject to conditions.

Note for Members: An application of this nature would not normally be presented to planning committee, however this has been called-in by Cllr Maria Alexandrou.

Ref: 18/02191/FUL LOCATION: Land To East Of 95 Vicars Moor Lane, London, N21 1BL,



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Scale 1:1250



1. Site and Surroundings

- 1.1 The application site is located on a plot to the rear of Vicars Moor Lane to the east of No. 95 Vicars Moor Lane which is located on the southern side of Vicars Moor Lane.
- 1.2 The immediately surrounding area is characterised largely by residential properties with Vicars Moor Lane to the north and Barber Close to the south.
- 1.3 The site is not listed and does not lie within a Conservation Area.

2. Proposal

- 2.1 The applicant seeks full planning permission for the construction of a 5-bedroom single family dwelling with rooms in loft space. (Revised)
- 2.2 The development has been revised during the course of the application to address council concerns in terms of design, height, detailed design and accommodation standards. This has resulted in the proposed dwellinghouse being revised from 6-beds to 5-beds.

3. Relevant Planning Decisions

- 3.1 18/01421/HOU - conditionally granted 29/05/2018
Development description: Erection of an additional garage at side.
- 3.2 17/04146/HOU - conditionally granted 13/11/2017
Development description: Two storey rear extension and rear dormer.
- 3.3 17/04182/CEA - granted 08/11/2017
Development description: Single storey side extension.

4. Enforcement history

- 4.1 There are current investigations pertaining to tree works at the site

5. Consultations

5.1 Statutory and non-statutory consultees

5.1.1 Traffic and Transportation

Details required regarding vehicular access, pedestrian access, cycle parking and refuse/ recycling parking. A Construction Logistics Plan is also required.

5.1.2 Trees

No objection subject to condition ensuring adequate protection of the existing trees on site.

5.1.3 Thames Water

No comment.

5.2 Public

5.2.1 Consultation letters were sent to 23 adjoining and nearby residents on the 19/06/2018 and again on the 09/10/2018. Thirteen responses have been received objecting to the application. Neighbour concerns are summarised as follows:

- Affect local ecology
- Close to adjoining properties
- Conflict with local plan
- Development too high
- General dislike of proposal
- Inadequate access
- Inadequate parking provision
- Inadequate public transport provisions
- Increase danger of flooding
- Increase in traffic
- Increase of pollution
- Information missing from plans
- Loss of light
- Loss of privacy
- More open space needed on development
- Noise nuisance
- Not enough info given on application
- Other - give details
- Out of keeping with character of area
- Over development
- Strain on existing community facilities

5.2.2 Material planning considerations are discussed within the main body of the report below.

5.2.3 In addition, comments have been received from Ward Councillor Cllr Maria Alexandrou stating the following concerns (edited):

- location of underground water wells
- loss of light especially to 95a
- loss of privacy especially to 95a
- height of house too high
- over development and out of character
- loss of trees already removed on this land- tree preservation order
- vehicle access restricted by gates-the owner of the bungalow may be a problem
- driveway to existing properties with shared access regularly blocked by vehicles relating to the build which inconveniences existing occupiers
- strong objections from neighbours
- planned building is too close to the bungalow 95a - future noise problems
- more lorries entering and exiting the driveway on Vicars Moor Lane where children walk due to the nearby schools creates hazards

6. Relevant Policy

6.1 The Development Management Document (DMD) policies have been prepared under the NPPF regime to be NPPF compliant. The DMD provides detailed criteria and standard based policies by which planning applications will be determined.

6.2 The policies listed below are considered to be consistent with the NPPF and therefore it is considered that due weight should be given to them in assessing the development the subject of this application.

6.3 London Plan

- Policy 3.3 Increasing housing supply
- Policy 3.4 Optimising housing potential
- Policy 3.5 Quality and design of housing developments
- Policy 3.8 Housing choice
- Policy 3.9 Mixed and balanced communities
- Policy 3.10 Definition of affordable housing
- Policy 3.11 Affordable housing targets
- Policy 3.12 Negotiating affordable housing
- Policy 3.13 Affordable housing thresholds
- Policy 4.8 Supporting a successful and diverse retail sector
- Policy 5.2 Minimising carbon dioxide emissions
- Policy 5.3 Sustainable design and construction
- Policy 5.13 Sustainable drainage
- Policy 5.14 Water quality and wastewater infrastructure
- Policy 5.15 Water use and supplies
- Policy 5.16 Water self-sufficiency
- Policy 5.18 Construction, excavation and demolition waste
- Policy 6.3 Assessing the effects of development on transport capacity
- Policy 6.7 Better streets and surface transport
- Policy 6.9 Cycling
- Policy 6.10 Walking
- Policy 6.11 Smoothing traffic flow and tackling congestion
- Policy 6.12 Road network capacity
- Policy 6.13 Parking
- Policy 7.1 Building London's neighbourhoods and communities
- Policy 7.2 An inclusive environment
- Policy 7.4 Local character
- Policy 7.6 Architecture
- Policy 7.19 Biodiversity and access to nature
- Policy 8.2 Planning Obligations

6.4 Core Strategy

- CP2 Housing supply and locations for new homes
- CP3 Affordable housing
- CP4 Housing quality
- CP5 Housing types
- CP6 Meeting particular housing needs
- CP20 Sustainable energy use and energy infrastructure
- CP21 Delivering sustainable water supply, drainage and sewerage infrastructure
- CP22 Delivering sustainable waste management

CP24	The road network
CP25	Pedestrians and cyclists
CP30	Maintaining and improving the quality of the built and open environment
CP32	Pollution
CP36	Biodiversity

6.5 Development Management Document

DMD 2	Affordable Housing
DMD 3	Mix of housing
DMD 6	Residential character
DMD 8	Residential Development
DMD 9	Amenity Space
DMD 10	Distancing
DMD 11	Rear Extensions
DMD 13	Roof Extensions
DMD 14	Side Extensions
DMD 37	High quality and design led development
DMD 38	Design Process
DMD 45	Parking Standards and Layout
DMD 47	Roads, access and servicing
DMD 48	Transport assessments
DMD 64	Pollution Control and Assessment
DMD 65	Air Quality
DMD 66	Land Contamination and Instability
DMD 68	Noise
DMD 80	Trees on Development Sites
DMD 81	Landscaping

6.6 Other relevant policy/guidance

National Planning Policy Framework
National Planning Practice Guidance
Section 106 SPD
London Housing SPG
Nationally Described Space Standards

7. Analysis

7.1 The main issues for consideration regarding this application are as follows:

- Principle of the development (para 7.2)
- Mix and housing need (para 7.3)
- Impact on neighbouring occupiers (para 7.5)
- Impact on the character of the surrounding area (para 7.16)
- Quality of accommodation and amenity space (para 7.21)
- Traffic and transport (para 7.26)
- Sustainable Urban Drainage SuDS (para 7.30)
- Ecology (para 7.36)
- Sustainable design and construction (para 7.35)
- Developer contributions (para 7.37)
- Community Infrastructure Levy (para 7.39)

Principle of development

- 7.2 The proposal would be compatible with Policies 3.3 and 7.5 of the London Plan and Core Policy 2 of the Local Development Framework insofar as it provides an addition family sized dwelling to the Borough's housing stock which actively contributes towards both Borough specific and London-wide strategic housing targets. However, this position must be qualified in relation to other material considerations including: achieving an appropriate residential mix; adequate internal floor space and layout; servicing; parking provision and residential amenity and principally in relation to its impact to the character and appearance of the surrounding area. It is in this regard that it is held that the principle of development is considered to be broadly acceptable, subject to assessment against the above-mentioned considerations.

Mix and housing need

- 7.3 Policy seeks to ensure that 'new developments offer a range of housing sizes to meet housing needs' and that the Policy should support the Council's plan for a Borough-wide mix of housing that reflects the needs and level of supply identified in the SHMA (2010). The 'Justification' in support of the Policy 5 of the Core Strategy is instructive. In paragraphs 5.40 and 5.41 it is noted that the supply-to-need shortage is most acute for larger dwelling types and that it is unlikely that the required supply can be met through new build completions and further demonstrates an acute shortage of houses with three or more bedrooms across owner occupier, social and private rented sectors. The greatest requirement in the owner-occupied market housing sector is for family sized housing (ie. 3+ bedrooms). This is equivalent to a need for 1,667 family sized homes of which nearly 40% is for four-bedroom homes over a period of two years. In addition, Policy DMD3 of the Development Management Document states that developers will be expected to take a design led approach to maximising the provision of family units (3 bed+).
- 7.4 The subject site currently consists of a vacant plot of land and as such it is therefore considered that the provision of a large family sized unit (in this instance a 5-bed single dwellinghouse) would comply with the above policies.

Impact on neighbouring occupiers

- 7.5 Policy seeks to ensure that residential developments do not negatively impact on the residential amenities of neighbouring properties with Policy CP30 of the Core Strategy seeking to ensure that development actively enhances the quality of life experience by both existing and future residents.
- 7.6 Policy also seeks to ensure that development is of an appropriate size and location and in keeping with the character of the locality, and not dominant when viewed from the surrounding area.
- 7.7 With regards to neighbouring amenity it is considered the primary properties to consider the proposal against are the existing properties either side (no.95 Vicars Moor Lane to the west and no.95A Vicars Moor Lane to the east). Occupiers of Barber Lane to the south have also raised concern about the impact of the development. All other properties are considered far enough removed from the development to not be affected.

Impact on 95 Vicars Moor Lane

- 7.8 The existing building has been subdivided into flats and is currently undergoing extensive refurbishment works. However, once the units are occupied and the new dwellinghouse built there would be a minimum distance of 8.0m between the flank walls of the properties. Whilst there are existing windows on the eastern elevation of 95 Vicars Moor Lane the new dwellinghouse would be plotted approximately 7.0m back from the front building line of No.95 thus retaining a view from the side/eastern elevation windows onto the adjacent development site.
- 7.9 In light of the above any residential amenity impact to the existing occupiers of No.95 Vicars Moor Lane is considered to be acceptable.

Impact on 95A Vicars Moor Lane

- 7.10 Concerns have been raised in terms of loss of light and privacy amongst other matters. However, given the distance the separation distance of approximately 7.0m between the flank walls of the properties and the main outlook (front and rear) of No.95A being retained it is not considered that any impact would be significant enough to justify refusal of the application.
- 7.11 In addition it is noted that the western elevation of No.95A appears to have a door and a small window which indicates this nearest elevation does not contain a main habitable area and/or provide the main outlook and light to the property.
- 7.12 In light of the above any residential amenity impact to the occupiers of No.95A is considered to be acceptable.

Impact on Barber Lane

- 7.13 Concern has been raised by some neighbouring occupiers of Barber Lane in terms of loss of light and loss of privacy amongst other things. However as above, given the distances between these properties and the proposed dwellinghouse together with the nearest affected property being plotted at an oblique angle to the new development, it is not considered that any impact on amenity would be substantial and/or significant enough to warrant refusal of the application.
- 7.14 In light of the above any residential amenity impact to the occupiers of Barber Lane is considered to be acceptable.

Summary of impact to neighbouring residential occupiers

- 7.15 Whilst it is acknowledged that the new dwellinghouse would be visible to neighbouring occupiers in the immediate vicinity, given the nature, siting and location of the proposed dwellinghouse, it is not considered there would be any additional and/or adverse impact arising which would justify refusal of the application. As such the application is considered acceptable in this respect.

Impact on the character of the surrounding area

- 7.16 The National Planning Policy Framework advises that Local Planning Authorities should not attempt to impose architectural styles or particular

tastes, and that design policies should concentrate on guiding factors such as the layout of the new development in relation to neighbouring buildings. It is however proper to seek to promote or reinforce local distinctiveness. In addition, Policy CP30 seeks to maintain and improve the quality of the built and open environment, whilst Policy DMD37 seeks to promote high standards of functional design in developments. London Plan policies 7.4 Local Character and 7.6 Architecture are also relevant.

- 7.17 The fundamental principle of the NPPF is to secure sustainable development. In order to achieve sustainable development, a development is required to have good design, amongst other things. The NPPF states planning policies and decisions should not attempt to impose architectural styles or particular tastes and they should not stifle innovation, originality or initiative through unsubstantiated requirements to conform to certain development forms or styles. It is, however, does go on to state that it is proper to seek to promote or reinforce local distinctiveness.
- 7.18 The proposal includes a large two storey detached dwellinghouse which will also essentially accommodate an additional storey within the roof area. Given the plot is contained by residential properties on all sides including the existing similarly proportioned No.95 Vicars Moor Lane to the west, it is not considered the proposed dwellinghouse would be out of keeping with the immediate locality.
- 7.19 In addition the revised scheme has sought to be in keeping with the local vernacular in terms of designing a house of a traditional architectural style. Whilst the design of the scheme is considered relatively ordinary, it would not be readily visible from streetview and would not be considered to result in any adverse impacts in terms of design. A condition is recommended requiring materials to be submitted via condition to ensure a high-quality finish is achieved.
- 7.20 On the above assessment, in terms of character and design, the redevelopment of the site is not considered to result in a form of development that would be overly dominant or out of keeping in the setting of the site. The development would replicate design elements of surrounding dwellings and has been designed within the constraints of the DMD.

Quality of accommodation and amenity space

- 7.21 Policies DMD5 and DMD8 of the Development Management Document, Policy 3.5 of the London Plan, Housing SPG, and the National Space Standards set minimum internal space standards for residential development. The London Plan Housing SPG has now been updated to reflect the National Space Standards.
- 7.22 DMD 9 states that the overall quality and design of amenity space is important to how it successfully it functions, screening to facilitate privacy, accessibility, sunlight to allow prolonged usage and management arrangements can help to create a space which is attractive and inviting therefore promotes the use of the space for leisure and relaxation
- 7.23 The NPPF advocates high quality design and the desire to intensify the use of the site should not be at the expense of providing a high-quality form of accommodation capable of meeting the reasonable needs of future residents.

- 7.24 The application proposes a 5-bed single dwellinghouse which would have an internal floor area in excess of 400sqm including circulation areas and stairwells. The dwelling would be dual aspect with all bedrooms having en suites. In terms of floor area, the overall size of the dwellinghouse would exceed London Plan standards. Individual room sizes would also exceed policy requirements as would the external amenity space. The dwelling would also comply with relevant standards in terms of light provision and outlook.
- 7.25 As such the application is considered acceptable in terms of proposed accommodation standards.

Traffic and transportation

- 7.26 Vicars Moor Lane is an adopted unclassified residential road located within the Winchmore Hill CPZ. The site has a PTAL of 2, which is moderately low.
- 7.27 The proposals would have some limited impact on the surrounding highway network, access, servicing and parking facilities at the site.
- 7.28 Whilst the development would increase the number of trips to and from the site 5 car parking spaces are proposed which would be an over provision for the proposed development. However, given the low PTAL rating of the site this is considered acceptable. As the access already serves two other developments (with one of these being sub-divided into flats) further details are recommended via condition including details of how vehicles will pass. Details of cycle parking and refuse and recycling storage is also recommended via condition.
- 7.29 With regards to construction traffic, an appropriate condition could be attached to secure a Construction Logistics/Management Plan and restricted construction hours and therefore it is not considered that the proposed works would have any adverse impacts upon residential amenities or conditions prejudicial to the safety and free flow of traffic.

Sustainable Urban Drainage (SuDS)

- 7.30 According to DMD 61, all developments must maximise the use of, and where possible retrofit, sustainable urban drainage systems (SUDS).
- 7.31 The proposed development must incorporate a sustainable urban drainage system in accordance with the quality and quantity requirements set out in the London Plan Drainage Hierarchy and the Development Management Document. The post-development runoff rate must be lower than the pre-development runoff rate and achieve greenfield runoff rates if possible.
- 7.32 The sustainable urban drainage strategy should include:
- A site plan;
 - A layout plan;
 - A topographical plan of the area with contours and overland flow routes together with details of what happens in exceedance events;
 - The footprint of the area being drained, including all buildings and parking areas;

- Greenfield Runoff Rates for a 1 in 1yr event and a 1 in 100yr event plus climate change;
- Storage volume; and
- Controlled discharge rate.

7.33 This could be required by condition should planning permission be granted.

Ecology

7.34 DMD 79 and Core Policy 36 require that developments should provide on-site ecological enhancements having regard to feasibility and viability. The current proposal would involve a new dwelling. Conditions are recommended to ensure that ecological enhancements are included in the development. These include gaps in fencing to allow small mammals to traverse sites and the provision of native plant species to encourage indigenous wildlife.

Sustainable design and construction

7.35 Adopted policies require that new developments achieve the highest sustainable design and construction standards having regard to technical feasibility and economic viability. A 35% CO₂ reduction is required for new residential units. No energy statement has been submitted with this application but this could be required by condition should planning permission be granted.

7.36 In addition, water efficiency measures will need to be provided. Submitted details will need to demonstrate reduced water consumption through the use of water efficient fittings, appliances and recycling systems to show consumption equal to or less than 105 litres per person per day. This could be required by condition should planning permission be granted.

s106 contributions

7.37 Following the Court of appeal decision from 11 May 2016, small site exemptions from paying S106 affordable housing policy contributions and other similar contributions were reinstated. The National Planning Practice Guideline was subsequently updated to provide guidance on small site exemptions.

7.38 The London Borough of Enfield no longer seeks contributions for education on schemes of 11 units and below. However, it does seek affordable housing contributions which are 10 units or less but have a combined gross floor space of more than 1000m². This is in conjunction with the criteria stipulated within the Planning Practice Guidance. The proposal is question is below the stipulated 1000m² and as such, does not required a S106 contribution towards affordable housing.

Community Infrastructure Levy (CIL)

7.39 The application proposes a net gain of 405sqm whilst providing an additional home, therefore the scheme would be CIL liable as follows:

- Mayors CIL: 405sqm x £20 = £8,100.00
- Borough CIL: 405sqm x £120 = £48,600.00

8. Conclusion and recommendation:

8.1 In light of the issues outlined in this report it is recommended that this application is approved subject to conditions as listed below:

1. Time Limited Permission

The development to which this permission relates must be begun not later than the expiration of three years beginning with the date of the decision notice.

Reason: To comply with the provisions of S.51 of the Planning & Compulsory Purchase Act 2004.

2. Approved Plans

Unless required by any other condition attached to this Decision, the development hereby permitted shall be carried out in accordance with the following approved plans and documents:

Site Location Plan;
Existing Site Plan;
Proposed Plans rev C dated 06/10/2018;
Panoramic Front Elevations rev B;
Front Elevations New Scheme vs Old Scheme rev B;
Proposed Section rev A;
Arboricultural Method Statement Report No: RT-NME-126532 Rev A

Reason: To ensure the development proceeds in accordance with the approved plans.

3. External Finishing Materials

The development shall not commence until details of the external finishing materials to be used have been submitted to and approved in writing by the Local Planning Authority. The development shall be constructed in accordance with the approved details.

Reason: To ensure a satisfactory external appearance.

4. Surfacing Materials

The development shall not commence until details of the surfacing materials to be used within the development including footpaths, access roads and parking areas and road markings have been submitted to and approved in writing by the Local Planning Authority. The surfacing shall be carried out in accordance with the approved detail before the development is occupied or use commences.

Reason: To ensure a satisfactory visual appearance and in the interests of highways safety

5. Landscaping Plan

The development shall not commence until a landscaping plan detailing existing planting to be retained and (where appropriate) trees, shrubs and grass to be planted and the treatment of any hard surfaced amenity areas has been submitted to and approved in writing by the Local Planning Authority.

It shall also (where appropriate) include a landscaping management plan and predicted growth detail so as to ensure the plantings are appropriately maintained. The site shall be landscaped in accordance with the approved details in the first planting season after completion or occupation of the development whichever is the sooner. Any trees or shrubs which die, becomes severely damaged or diseased within five years of planting shall be replaced with new planting in accordance with the approved details.

Reason: To provide a satisfactory appearance and protect the amenity of adjoining properties

6. Site Enclosure

The site shall be enclosed in accordance with details to be submitted to and approved in writing by the Local Planning Authority. The means of enclosure shall be erected in accordance with the approved detail before the development is commenced.

Reason: To ensure satisfactory appearance and safeguard the privacy, amenity and safety of adjoining occupiers and the public and in the interests of highway safety.

7. Hard Surfaced Areas

The development shall not commence until plans detailing the existing and proposed ground levels including the levels of any proposed buildings, roads and/or hard surfaced areas have been submitted to and approved in writing by the Local Planning Authority. The development shall be constructed in accordance with the approved details.

Reason: To ensure that levels have regard to the level of surrounding development, gradients and surface water drainage.

8. Use of Parking Area

The parking area(s) forming part of the development shall only be used for the parking of private motor vehicles and shall not be used for any other purpose.

Reason: To ensure that the development complies with Development Plan Policies and to prevent the introduction of activity which would be detrimental to amenity.

9. Refuse/ Recycling Storage

Prior to the commencement of above ground works, details of the siting and design of refuse storage facilities including facilities for the recycling of waste to be provided within the development, in accordance with the London Borough of Enfield – Waste and Recycling Planning Storage Guidance ENV 08/162, have been submitted to and approved in writing by the Local Planning Authority. The facilities shall be provided in accordance with the approved details before the development is occupied.

Reason: In the interests of amenity and the recycling of waste materials in support of the Boroughs waste reduction target.

10. Water Consumption

Prior to first occupation details of the internal consumption of potable water shall be submitted to and approved in writing by the Local Planning Authority.

Submitted details will demonstrate reduced water consumption through the use of water efficient fittings, appliances and recycling systems to show consumption equal to or less than 105 litres per person per day. The development shall be carried out strictly in accordance with the details so approved and maintained as such thereafter.

Reason: To promote water conservation and efficiency measures in all new developments and where possible in the retrofitting of existing stock in accordance with Policy CP21 of the Core Strategy, Policy 5.15 of the London Plan.

11. Construction Management Plan

The development shall not commence until a construction management plan has been submitted to and approved by the Local Planning Authority. The construction management plan shall be written in accordance with London Best Practice Guidance and contain:

- a. A photographic condition survey of the public roads, footways and verges leading to the site.
- b. Details of construction access and associated traffic management.
- c. Arrangements for the loading, unloading and turning of delivery, construction and service vehicles.
- d. Arrangements for the parking of contractors' vehicles.
- e. Arrangements for wheel cleaning.
- f. Arrangements for the storage of materials.
- g. Hours of work.
- h. The storage and removal of excavation material.
- i. Measures to reduce danger to cyclists.
- j. Dust mitigation measures.
- k. Membership of the Considerate Contractors Scheme

The development shall be carried out in accordance with the approved construction management plan unless otherwise agreed by the Local Planning Authority.

Reason: To ensure construction does not lead to damage of the nearby public road network and to minimise disruption to the neighbouring properties.

12. Energy Performance Certificate

Following practical completion of works a final Energy Performance Certificate shall be submitted to an approved in writing by the Local Planning Authority. Where applicable, a Display Energy Certificate shall be submitted within 18 months following first occupation.

Reason: In the interest of sustainable development and to ensure that the Local Planning Authority may be satisfied that CO2 emission reduction targets are met in accordance with Policy CP20 of the Core Strategy, Policies 5.2, 5.3, 5.7 & 5.9 of the London Plan 2011 and the NPPF

13. Cycle Storage

No above ground works shall commence until details (including elevational details) for covered cycle parking for the storage of a minimum of 2 bicycles per dwelling and 2 short-stay visitor space have been submitted to and approved in writing by the Local Planning Authority. The approved cycle storage shall be provided prior to first occupation of the development and

permanently maintained, kept free from obstruction, and available for the parking of cycles only.

Reason: To provide secure cycle storage facilities free from obstruction in the interest of promoting sustainable travel.

14. Vehicular and Pedestrian Access

No above ground works shall commence until details of vehicular and pedestrian access have been submitted to and approved in writing by the Local Planning Authority. The approved access shall be provided prior to first occupation of the development and permanently maintained, kept free from obstruction thereafter.

Reason: To provide appropriate access to and from the site for existing and future occupiers.

15. Protection of Habitats

In the event that any construction works necessitate the clearance of nesting bird habitats, it is recommended that any such works are undertaken in the period August to February inclusive. Should it prove necessary to remove any breeding bird habitat during the breeding season, the area should be checked in advance for the presence of birds' nests. Once checked, if there is no evidence of breeding birds, clearance work should be completed within 48 hours of inspection. If any active nests are found in this area then vegetation clearance must cease and an appropriate buffer zone should be established. This buffer must be left intact until it has been confirmed that the young have fledged and the nest is no longer in use.

Reason: To ensure that bird nests and bird habitats are provided with appropriate protections during the construction phase.

16. Tree Protection

Prior to the commencement of the development hereby approved (including demolition and all preparatory work), a scheme for the protection of the retained trees, in accordance with BS 5837:2012, including a tree protection plan(s) (TPP) and an arboricultural method statement (AMS) shall be submitted to and approved in writing by the Local Planning Authority.

Specific issues to be dealt with in the TPP and AMS:

- a) A specification for protective fencing to safeguard trees during both demolition and construction phases and a plan indicating the alignment of the protective fencing;
- b) A specification for scaffolding and ground protection within tree protection zones;
- c) Tree protection during construction indicated on a TPP and construction and construction activities clearly identified as prohibited in this area;
- d) Details of site access, temporary parking, on site welfare facilities, loading, unloading and storage of equipment, materials, fuels and waste as well concrete mixing and use of fires.

The development thereafter shall be implemented in strict accordance with the approved details.

Reason: Required prior to commencement of development to satisfy the Local Planning Authority that the trees to be retained will not be damaged during demolition or construction and to protect and enhance the appearance and character of the site and locality, in accordance with DMD 80 and pursuant to section 197 of the Town and Country Planning Act 1990

Informative:

The following British Standards should be referred to:

- a) BS: 3998:2010 Tree work – Recommendations
- b) BS: 5837 (2012) Trees in relation to demolition, design and construction - Recommendations

17. Construction – Good Practice

During the construction phases, including demolition and pre-construction works, all reasonable measures shall be taken to ensure good production practices are adopted to safeguard individual animals. Such practices would include covering of all deep holes and trenches overnight and the provision of planked escape routes for any trapped wildlife. Any liquids held at the site should be stored in a secure lock-up. Such methods shall be employed for the duration the construction period.

Reason: To ensure that any animals who inadvertently enter the site are afforded appropriate protections.

18. Considerate Contractors Scheme

The development shall not commence until an undertaking to meet with best practice under the Considerate Constructors Scheme and achieve formal certification has been submitted to and approved in writing by the Local Planning Authority.

Reason: To ensure the implementation of the development does not adversely impact on the surrounding area and to minimise disruption to neighbouring properties.

19. Sound Insulation

The development shall be constructed/adapted so as to provide sufficient air-borne and structure-borne sound insulation against externally generated noise and vibration. This sound insulation shall ensure that the level of noise generated from external sources shall be no higher than 35 dB(A) from 7am – 11pm in bedrooms, living rooms and dining rooms and 30 dB(A) in bedrooms from 11pm – 7am measured as a $L_{Aeq,T}$. The $L_{AF Max}$ shall not exceed 45dB in bedrooms 11pm – 7am. A scheme for mitigation measures shall be submitted to and approved by the Local Planning Authority prior to development taking place. The scheme of mitigation shall include mechanical ventilation where the internal noise levels exceed those stated in BS8233: 2014 with the windows open. The approved mitigation scheme shall be implemented in its entirety before the unit is occupied/the use commences.

Reason: To protect future residents from noise and disturbance.

20. Sustainable Urban Drainage (SuDS)

No development shall take place until a Sustainable Drainage Strategy has been submitted and approved by the Local Planning Authority. A Sustainable Drainage Strategy must include the following information, and must conform to the landscaping strategy:

1. A plan of the existing site
2. A topographical plan of the area
3. Plans and drawings of the proposed site layout identifying the footprint of the area being drained (including all buildings, access roads and car parks)
4. The controlled discharge rate for a 1 in 1 year event and a 1 in 100 year event (with an allowance for climate change), this should be based on the estimated greenfield runoff rate
5. The proposed storage volume
6. Information (specifications, sections, and other relevant details) on proposed SuDS measures with a design statement describing how the proposed measures manage surface water as close to its source as possible and follow the drainage hierarchy in the London Plan and the principles of a SuDS Management Train
 - a. Geological information including borehole logs, depth to water table and/or infiltration test results
 - b. Details of overland flow routes for exceedance events
 - c. A management plan for future maintenance

Reason: To ensure that the proposal would not result in an unacceptable risk of flooding from surface water run-off or create an unacceptable risk of flooding elsewhere and to ensure implementation and adequate maintenance

21. Energy Statement

The development shall not commence until an 'Energy Statement' has been submitted to and approved by the Local Planning Authority. The details must demonstrate the energy efficiency of the development and shall provide for no less than a 35% improvement in total CO₂ emissions arising from the operation of the development and its services over Part L of the 2013 Building Regulations. The Energy Statement should outline how the reductions are achieved through the application of the following energy hierarchy, with each tier utilised fully before a lower tier is employed:

- a. Fabric Energy Efficiency performance (inclusive of the use of energy efficient fittings) and the benefits of passive design;
- b. The potential to connect to existing or proposed decentralised energy networks; and
- c. Demonstrating the feasibility and use of zero and low carbon technology.

Unless otherwise required by any other condition attached, the development shall be carried out strictly in accordance with the details so approved and maintained as such thereafter.

Reason: In the interest of sustainable development and to ensure that the Local Planning Authority may be satisfied that CO₂ emission reduction targets are met.

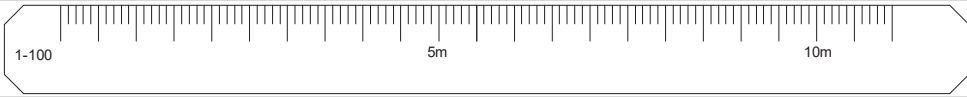
22. Permitted Development Rights
Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) Order 2015, or any amending Order, no buildings or extensions to buildings shall be erected without the prior approval in writing of the Local Planning Authority.

Reason: To prevent the overdevelopment of the site and in the interests of residential amenity.

Informatives:

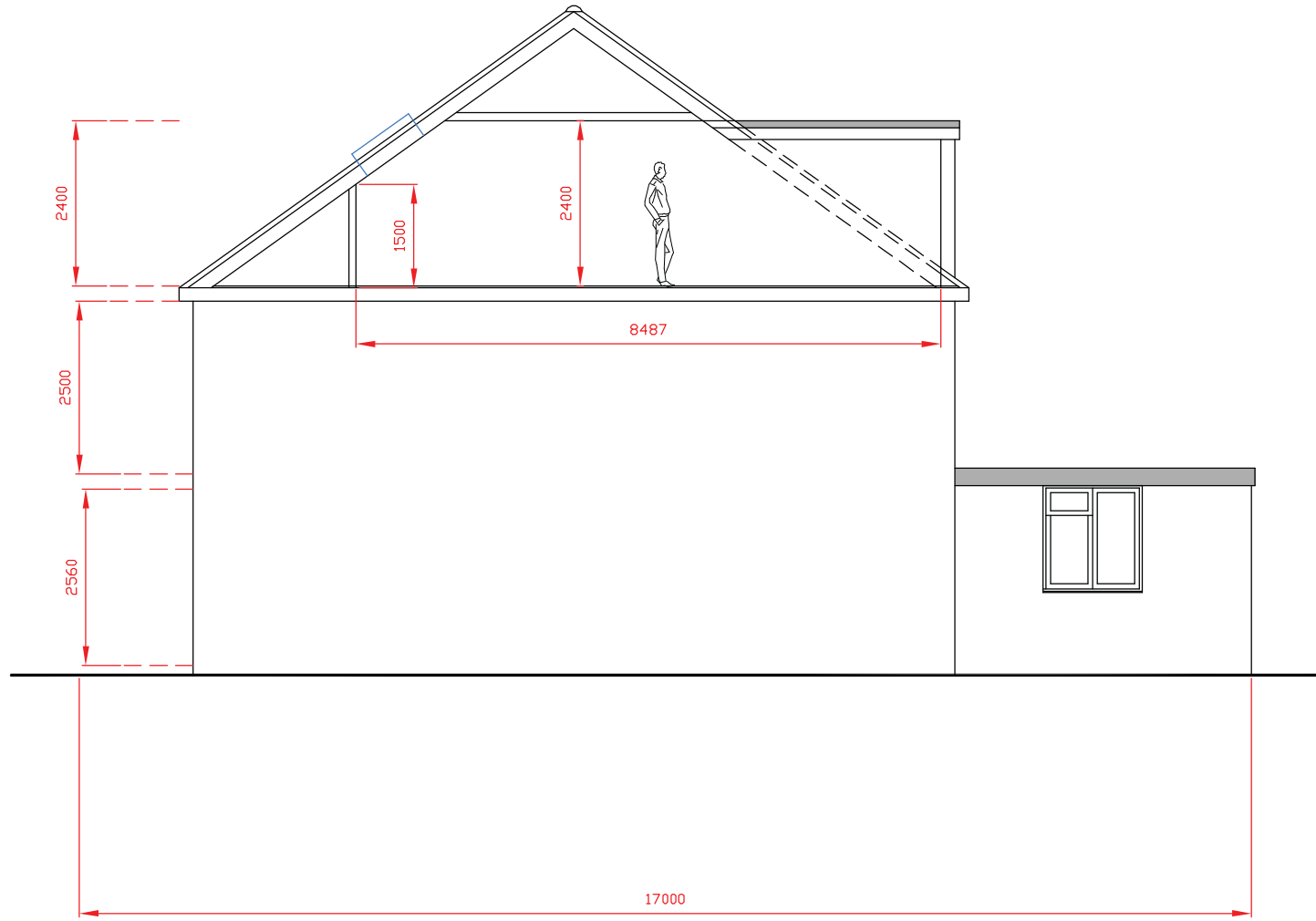
1. You are advised that in relation to Conditions 6 (Means of Enclosure) of this permission, where reference is made to the provision enclosure that include gaps under which mammals can pass, these need be no more than 15 x 15cm and can be achieved, if fencing is proposed, by lifting the gravel board off the ground. The reason for requiring this is that there has been a decline in the populations of many small mammal species such as hedgehogs and this is in part due to gardens being made impermeable to their movements.
2. The applicant is reminded of their obligations under the Conservation of Habitats and Species Regulations (2010) as amended and the Wildlife and Countryside Act 1981 (as amended) in relation to the protection of bats and bat habitats. In the event that any harm occurs under the works herein approved the developer may be liable to prosecution under the aforementioned legislation.

Proposed Section



01273 311229
www.justplans.net

e: joseph@justplans.net
 e: ames.smith@justplans.net
 e: info@justplans.net



Terms of Planning Service

AGREEMENT TO THE DRAWINGS

1. All drawings, advice and services supplied by JustPlans Ltd are subject to approval under the Town and Country Planning Acts.
2. All drawings and information supplied are designed for exclusive use as part of a residential planning application.
3. No drawing or information supplied is designed for any purpose other than as part of a planning application, especially and including any building works or any building regulations application.
4. No drawing or information supplied should be used as an on-site reference.
5. No drawing supplied should be relied upon, created by a consultant officer or administrator for the purpose of either verifying or granting permission for the planning application.
6. No drawing supplied should be used, either as the basis of, or to corroborate, any drawings, statements or words of any kind by a third party. Especially they should not be used as the basis of or to corroborate any future building regulations application.
7. No drawing supplied should be used for a building contractor to build from.
8. Any discrepancy between information contained within the drawings and the actuality on site must be rectified by request to JustPlans Ltd upon discovery.
9. JustPlans Ltd's liability for any discrepancy between drawing information and the actuality on site is limited to the drawing's purpose and does not include construction-related issues.
10. In the event that a council awards planning approval but subsequently withdraws approval on the basis that drawings or information supplied are inaccurate or incorrect, JustPlans does not accept liability for:
 - (a) any direct or indirect financial expense (i.e. demolition, re-construction, planning application or amendment) incurred;
11. Any dimensions contained within any drawing supplied are accurate only to a standard suitable for obtaining planning permission, regulations and legislation. Especially they should not be relied upon for any structural work. Especially they should not be relied upon for any calculations relating to building services.

RELATING TO THE TERMS OF SERVICE

1. Any changes or amendments required to the draft extension design must be clearly drawn and / or annotated on the draft drawings and returned to JustPlans Ltd in either hard or soft copy format.
2. JustPlans Ltd commits itself to providing one opportunity for amendments to the draft design to be received in clause 10, unless otherwise agreed in writing. JustPlans Ltd does not agree to receive additional amendments to the design.
3. In the event that more than one change to the draft design is nevertheless provided by you, beyond the terms of clause 10 above, we warrant that each additional modification, drawing, revision, amendment or change to the drawings will be made to any other affected parties. Digital drawing files may become corrupted in email or other file transfer methods. JustPlans Ltd is not held for any resultant problems, whether relating to our own digital files or those of other third parties including the local council.
4. Although our drawings are normally produced using CAD, we offer no guarantee that digital copies of drawings will be used on site.
5. Any digital copies of drawings which JustPlans Ltd agrees to supply are subject to these terms.
6. Where a planning application is required by the local authority it is checked for suitability by the planning administration team. JustPlans Ltd gives no guarantee that any application made on your behalf will receive immediate validation and indeed it is common for the validation process to take a considerable period of time.
7. JustPlans Ltd offers no commitment to provide any services for taking part in or negotiating with any parish or external person or body for any part of the construction process, including any building contracts, structural engineer or sound officer.
8. No guarantee is provided that any drawing supplied will be planning approval or can be built, without obtaining planning approval from the relevant planning authority. Planning authorities are diverse in nature, vary substantially between councils and are in a state of constant flux. Most councils award or deny planning approval based in part upon a subjective assessment of the merits of each individual property, in relation to the immediate surroundings. Because of the broad discretionary power it is impossible to predict with consistency or high accuracy whether a proposal will be accepted or not by a council planning authority.

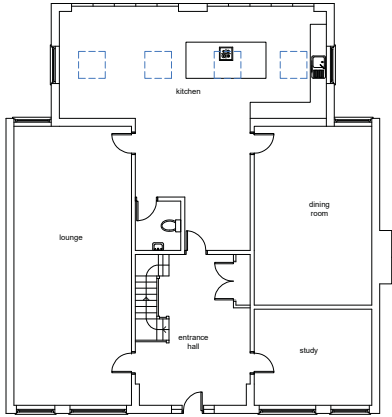
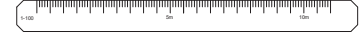
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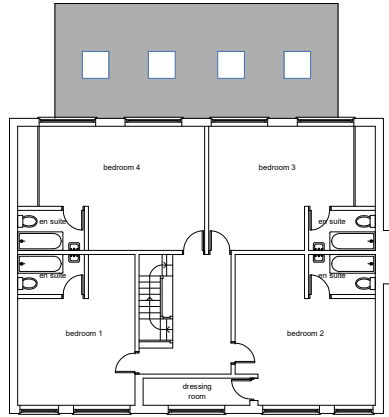
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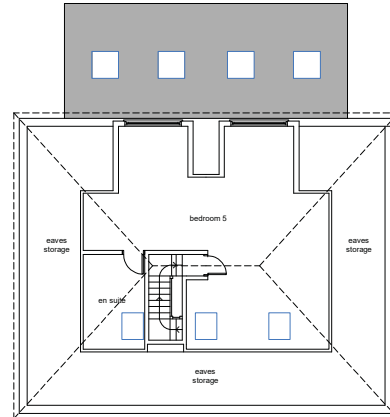
Proposed Plans - 2018-10-06



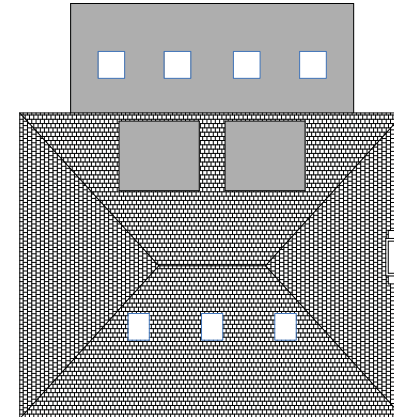
Ground Floor Plan



First Floor Plan



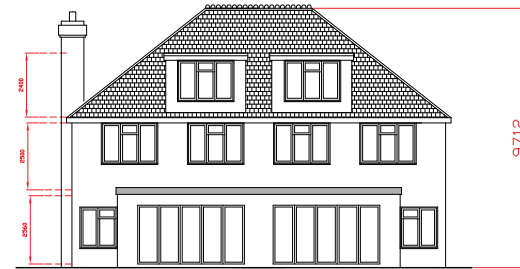
Second Floor Plan



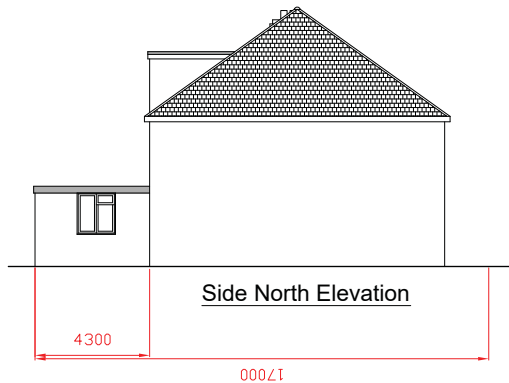
Roof Plan



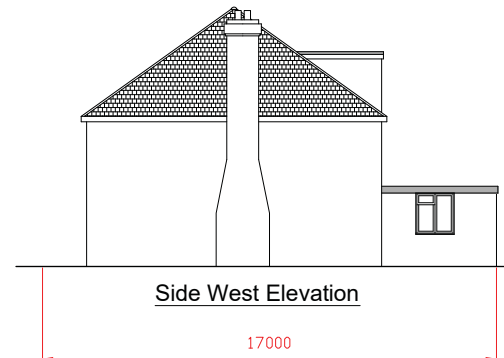
Front West Elevation



Rear East Elevation



Side North Elevation



Side West Elevation



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- No drawings or information supplied is designed for any purpose other than as part of a planning application, especially and including any building notice or any building regulations application.
- No drawings or information supplied should be used as an invoice reference.
- No drawings supplied should be relied upon, except for a second office or submission for the purpose of other validation or getting permission for the planning application.
- No drawings or information supplied should be relied upon on the basis of its contents, any drawings, dimensions or notes of any kind to a third party. Especially they should not be used to inform or to corroborate any planning application.
- No drawings supplied should be used for a building contract to build from.
- Any discrepancy between information contained within the drawings and the actuality on site must be immediately reported to JustPlans upon discovery.
- JustPlans shall not be held liable for any discrepancy between drawing information and the reality, in the course of the planning process and upon completion of the project, but we will be happy to assist in the event that a third party planning approval has subsequently been approved on the basis that drawings or information supplied are incorrect. JustPlans does not accept any liability for such a situation.
- Any drawings or information supplied are provided as a guide only and should not be relied upon for any calculation relating to building work.

RELATING TO THE TERMS OF SERVICE

- Any changes or amendments required to the draft drawings should be made within 14 days of completion of the draft drawings and returned to JustPlans in either hard or soft copy format.
- JustPlans cannot take responsibility for providing any opportunity for amendments to the draft design as described in clause 11 above. Unless otherwise agreed in writing, JustPlans does not agree to provide additional opportunities for amendments to the design.
- In the event that more than one change to the draft design is requested, provided for in the terms of clause 11 above, the client shall be advised that such additional modifications are subject to the availability of time and resources. Digital drawings may be subject to change in order to comply with digital copies requirements in each circumstance. JustPlans is not liable for any resident projects, including relating to our digital files or any other third party including the local council.
- Although our drawings are made available using CAD, we offer no guarantee that digital copies of drawings will be kept on file.
- Any digital copies of drawings which JustPlans agrees to supply are subject to these terms.
- When a planning application is received by the local authority it is subject to their liability to the planning application team. JustPlans does not guarantee that any application made on your behalf will receive immediate validation and whilst it is common for the validation process to take a considerable period of time.
- JustPlans offers no commitment to provide any service for any plan or requirement with any professional areas of work for any part of the construction process including any building contracts, structural engineer or related other.
- No guarantee is provided that any drawings supplied will be granted planning approval or can be built without obtaining specific planning approval. Planning permission applications are subject to their own terms and conditions. JustPlans does not accept any liability for any such applications. JustPlans is not liable for any such applications. JustPlans is not liable for any such applications. JustPlans is not liable for any such applications.

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LONDON BOROUGH OF ENFIELD

PLANNING COMMITTEE

Date: 20th November 2018

Report of

Executive Director - Place

Contact Officer:

Andy Higham
Tel No: 0208 379 3848

Ward:

Edmonton Green

Application Number: 18/02237/FUL

Category: Minor Dwellings

LOCATION: Aberdeen Parade, Angel Road, London, N18 2EB

PROPOSAL: Construction of additional storey at third floor level to provide 6 self-contained flats, comprising 2 x 1-bed and 4 x 2-bed with balconies and associated car parking.

Applicant Name & Address:

Ms E Endzweig

1 Aberdeen Parade
London
N18 2EB

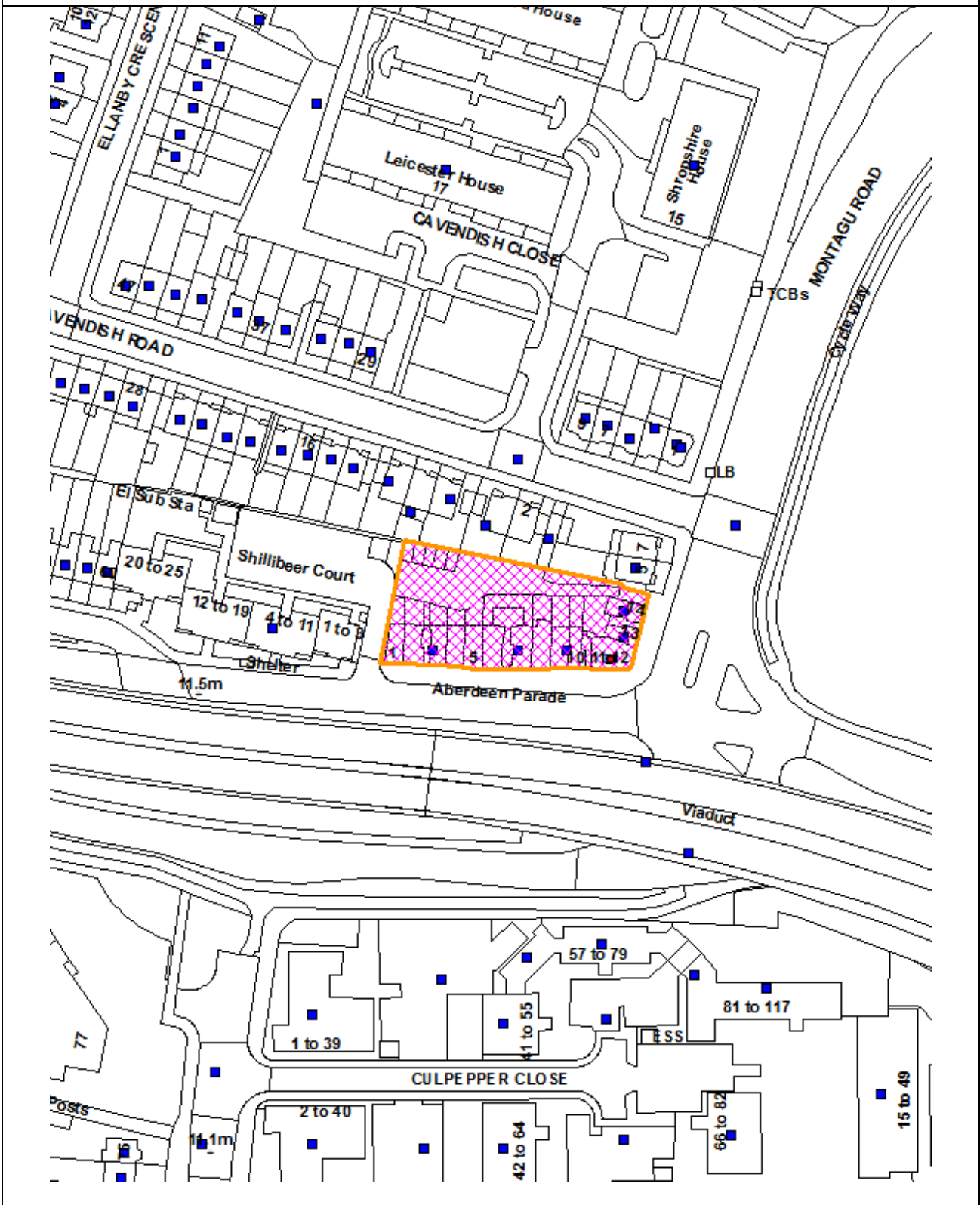
Agent Name & Address:

Mr Ali Ay
Anva
PO Box 1827
Ilford
IG2 7WJ

RECOMMENDATION: Planning permission be granted subject to conditions.

Note for Members: An application of this nature would not normally be presented to planning committee, however this has been called-in by Cllr Ergin Erbil.

Ref: 18/02237/FUL LOCATION: Aberdeen Parade, Angel Road, London, N18 2EB



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North



1. Site and Surroundings

- 1.1 The application site is a three-storey detached block of flats located on the northern side of Aberdeen Parade. The surrounding area largely comprises residential properties with a newer block of flats lying to the west and semi-detached dwellinghouses lying on Cavendish Road to the rear. The A10 lies in close proximity to the front of the site.
- 1.2 The site is not located in a Conservation Area and it is not a Listed Building.

2. Proposed Development

- 4.1 The development description is as follows: *Construction of additional storey at third floor level to provide 6 self-contained flats, comprising 2 x 1-bed and 4 x 2-bed with balconies and associated car parking.*

3. Relevant Planning History

- 3.1 17/03032/FUL - Withdrawn 16/05/2018

Development description: Construction of third floor loft extension to provide 9 x 1 bed self-contained flats with front and side dormers.

- 3.2 TP/98/1063 - Granted with conditions: 22/12/1998

Development description: Conversion of eleven shops into ground floor flats (comprising 9 one-beds and 2 two-beds) involving erection of single storey rear extension to Nos.9 and 10 and provision of car parking at rear.

- 3.3 The application has been substantially revised during the assessment period to address concerns in terms of design, height, dwelling mix, accommodation standards and car parking provision. In order to address these concerns, the unit numbers have decreased from 8 to 6.

4. Consultations

Internal

- 4.1 Traffic and Transportation:

No objection subject to further details of cycle and waste storage facilities being submitted.

- 4.2 Environmental Health:

No objection subject to condition - needed in relation to minimising noise and air pollution.

- 4.3 Conservation and Design:

No objection in principal, as they were consulted on previous scheme - advised scheme could better integrate into existing architectural style which has references to an Art Deco style. Materiality should be of a high quality.

External

4.4 Thames Water:

No response received

4.5 Neighbours: 66 neighbours consulted individually by letter on 15/06/2018 and again on the 18/07/2018. Ten (10) responses received including a 16-signatory petition from the residents of Shillibeer Court. It is noted that some of the responses received are duplicate comments from the same address.

1. Correspondence from *Aberdeen Parade Tenants and Residents Association* with the following concerns:

Overall (poor) quality of the submission in terms of some documents either being inaccurate or not being submitted at all (such as a Design and Access Statement and a Transport Statement).

Other concerns regarding the proposed development including as follows:

- Local, strategic, national planning policies; (contrary to)
 - Emerging new plans; (contrary to)
 - Overlooking and loss of privacy;
 - Highway issues: traffic generation, vehicular access;
 - Noise or disturbance resulting from use;
 - Lack of open space & biodiversity opportunities; and
 - Layout and density of building design, visual appearance and finishing materials.
2. Letter from *Residents of Shillibeer Court* (adjacent to the application site) with the following concerns:
- Application site has no proper access road; as such Shillibeer Court would be used by construction vehicles during the construction works;
 - Likely increase in more occupiers of Aberdeen Court using the car parking at Shillibeer Court;
 - Likely increase in more occupiers of Aberdeen Court using the waste bins at Shillibeer Court;
 - Loss of light;
 - Loss of privacy;
 - More open space needed on development;
 - Noise nuisance; and
 - Strain on existing community facilities.
3. Concerns raised individually by neighbours as follows:
- Overlooking and loss of privacy for those on Cavendish Road;
 - Highway issues: traffic generation, vehicular access, no parking;
 - Noise or disturbance resulting from use;
 - Lack of open space & biodiversity opportunities;
 - Layout and density of building design, visual appearance and finishing materials;
 - Lack of proper consultation;
 - Not a good mix of housing;
 - Not an energy efficient design and

- Noise and disturbance to existing occupiers of the building whilst building works are undertaken.

4.6 Neighbour concerns which are also material planning considerations are discussed within the main body of the report below.

5. Relevant Planning Policies

5.1 The London Plan

Policy 3.1	Ensuring equal life chances for all
Policy 3.3	Increasing housing supply
Policy 3.4	Optimising housing potential
Policy 3.5	Quality and design of housing development
Policy 3.8	Housing choice
Policy 3.9	Mixed and balanced communities
Policy 3.10	Definition of affordable housing
Policy 3.11	Affordable housing targets
Policy 3.12	Negotiating affordable housing on schemes
Policy 3.13	Affordable housing thresholds
Policy 5.2	Minimising carbon dioxide emissions
Policy 5.3	Sustainable design and construction
Policy 5.7	Renewable energy
Policy 5.13	Sustainable drainage
Policy 5.14	Water quality and wastewater infrastructure
Policy 5.15	Water use and supplies
Policy 5.16	Waste self sufficiency
Policy 6.9	Cycling
Policy 6.10	Walking
Policy 6.13	Parking
Policy 7.2	An inclusive environment
Policy 7.3	Designing out crime
Policy 7.4	Local character
Policy 7.5	Public realm
Policy 7.6	Architecture
Policy 7.14	Improving air quality
Policy 7.19	Biodiversity
Policy 7.21	Trees

5.2 Core Strategy

CP2	Housing Supply and Locations for New Homes
CP3	Affordable Housing
CP4	Housing Quality
CP5	Housing Types
CP9	Supporting community cohesion
CP20	Sustainable energy use and energy infrastructure
CP21	Delivering sustainable water supply, drainage and sewerage infrastructure
CP24	The road network
CP26	Public transport
CP25	Pedestrians and cyclists
CP28	Managing flood risk
CP30	Maintaining and improving the quality of the built and open environment

CP31	Built and landscape heritage
CP32	Pollution
CP46	Infrastructure Contribution

5.3 Development Management Document

DMD2	Affordable Housing for Developments of less than 10 units
DMD3	Providing a Mix of Different Sized Homes
DMD5	Residential conversions
DMD6	Residential Character
DMD8	General Standards for New Residential Development
DMD9	Amenity Space
DMD10	Distancing
DMD37	Achieving High Quality and Design-Led Development
DMD38	Design Process
DMD45	Parking Standards and Layout
DMD47	New Roads, Access and Servicing
DMD48	Transport assessment, travel plans, servicing and delivery plans
DMD49	Sustainable Design and Construction Statements
DMD50	Environmental Assessment Methods
DMD51	Energy Efficiency Standards
DMD58	Water Efficiency
DMD59	Avoiding and Reducing Flood Risk
DMD60	Assessing Flood Risk

6. Other Relevant Policy Considerations

- National Planning Policy Framework (2018)
- Section 106 Supplementary Planning Document
- London Housing SPG

7. Analysis

7.1 The main issues for consideration regarding this application are as follows:

- Principle of development (para 7.2)
- Neighbouring amenity (para 7.3)
- Unit mix/ standard of accommodation/ amenity space (para 7.4)
- Design/ character and appearance (para 7.5)
- Highways/ Servicing (para 7.6)
- Sustainable Design and Construction (para 7.7)
- S106/CIL requirements (para 7.8)

Principle of Development

7.2.1 The principle of providing additional residential accommodation to an existing block of flats is acceptable subject to consideration of the impact of the development. The proposed development has the potential to result in impact in terms of neighbouring amenity; design of the development and how this will impact the existing building and immediate area; type of proposed unit and the standard of accommodation in the proposed new units; and highways issues. These are discussed in further detail below.

7.2.2 The application would provide for additional living accommodation in the borough and as such would assist in achieving borough housing targets. Therefore, in this respect additional residential accommodation should be encouraged.

7.2.3 However, this acceptability in principle must be balanced against the need to ensure that any new developments preserve and enhance the character and appearance of the surrounding area and safeguard the amenities of adjoining residents along with providing for an acceptable standard of living accommodation.

7.3 Neighbouring amenity

7.3.1 Policy DMD11 seeks to ensure that residential extensions do not negatively impact on the residential amenities of neighbouring properties with Policy CP30 of the Core Strategy seeking to ensure that development actively enhances the quality of life experience by both existing and future residents.

7.3.2 Policy DMD13 notes that roof extensions should be of an appropriate size and location within the roof plane and be in keeping with the character of the property, and not dominant when viewed from the surrounding area.

7.3.3 With regards to neighbouring amenity it is considered the primary properties to consider the proposal against are the existing (32) units on the three floors below; properties to the west (Shillibeer Court) and properties to the rear (Cavendish Road). All other properties are considered far enough removed from the development to not be affected.

Impact on existing occupiers of Aberdeen Parade

7.3.4 The existing building contains 32 residential units which includes 11 units at ground floor level which have been converted from shops. As the proposed development at third floor comprises further residential units it is not considered there would be any amenity impact arising from the use of the additional floor of residential accommodation.

7.3.5 The new units would contain small balconies in order to comply with external amenity provision standards, required for new residential development. Whilst it is acknowledged that the existing on the floors below do not contain balconies or terraces, given the size and location of the proposed balconies (which would also be partially enclosed so as to minimise the projection out from the building) it is not considered there would be any increase in noise arising from the use of the balconies over and above what could be reasonably expected in a private dwelling. Notwithstanding were there to be any unreasonable levels of noise and disturbance arising from the use of these balconies the matter would be dealt with under legislation outside of planning control.

7.3.6 In light of the above any residential amenity impact to the existing occupiers of Aberdeen Parade is considered to be acceptable.

Impact on Shillibeer Court

7.3.7 Given there is substantial separation between Shillibeer Court and Aberdeen Parade in the way of a vehicular access way into Shillibeer Court, it is not

considered the occupiers of Shillibeer Court would be in close enough proximity to the new units at third floor level to be unduly impacted by the development.

- 7.3.8 Concerns have been raised by occupiers of Shillibeer Court in relation to loss of light, loss of privacy and noise nuisance however given the orientation of Shillibeer Court in relation to Aberdeen Court together with the separation resulting from the vehicular access into Shillibeer Court mentioned above, officers are of the view that any impact would not be significant enough to adversely impact occupiers of Shillibeer Court.
- 7.3.9 Other concerns have been raised by the occupiers of Shillibeer Court in relation to construction vehicles, use of car park and use of waste bins. Whilst the use of the car park and waste bins is a site management matter outside of planning control, a condition is recommended requiring a Construction Management Plan be submitted to ensure any impact on neighbouring occupiers during the course of construction is so far as possible mitigated.
- 7.3.10 In light of the above any residential amenity impact to the occupiers of Shillibeer Court is considered to be acceptable.

Impact on Cavendish Road

- 7.3.11 Concern has been raised by neighbours in relation to overlooking to the occupiers of Cavendish Road which is located to the rear of the subject site. However given the window to window distance of approximately a minimum 17m and the orientation of Cavendish Road which sits at an oblique angle to Aberdeen Parade, it is not considered there would be any unacceptable levels of overlooking over and above that which already exists
- 7.3.12 In light of the above any residential amenity impact to the occupiers of Cavendish Road is considered to be acceptable.

Summary of impact to neighbouring residential occupiers

- 7.3.13 Given the nature, siting and location of the proposed additional units, it is not considered there would be any additional and/or adverse impact arising in terms of residential amenity. As such the application is considered acceptable in this respect.

7.4 Mix of units, standard of accommodation and private amenity

- 7.4.1 Policies DMD5 and DMD8 of the Development Management Document, Policy 3.5 of the London Plan, Housing SPG, and the National Space Standards set minimum internal space standards for residential development. The London Plan Housing SPG has now been updated to reflect the National Space Standards.
- 7.4.2 DMD 9 states that the overall quality and design of amenity space is important to how it successfully it functions, screening to facilitate privacy, accessibility, sunlight to allow prolonged usage and management arrangements can help to create a space which is attractive and inviting therefore promotes the use of the space for leisure and relaxation.

- 7.4.3 The NPPF advocates high quality design and the desire to intensify the use of the site should not be at the expense of providing a high-quality form of accommodation capable of meeting the reasonable needs of future residents.
- 7.4.4 The application proposes 2 x 1-bed and 4 x 2-bed flats. Within these units, one of the 2-bed flats (Flat C) would be considered large enough be a small family sized unit. This proposed dwelling mix is considered acceptable when measured against the Borough's current and emerging housing requirements and the relatively constrained position and location of the site.
- 7.4.5 In terms of individual unit sizes the application proposes as follows:

<i>Proposal</i>	<i>London Plan requirement</i>
Unit 1 (Flat A) – 59.00 sqm (1b2p)	50sqm
Unit 2 (Flat B) – 73.70 sqm (2b3p)	61sqm
Unit 3 (Flat C) – 76.20 sqm (2b4p)	70sqm
Unit 4 (Flat D) – 61.90 sqm (2b3p)	61sqm
Unit 5 (Flat E) – 67.30 sqm (2b3p)	61sqm
Unit 6 (Flat F) – 54.50 sqm (1b2p)	50sqm

- 7.4.6 Of these units, all comply with London Plan standards in terms of floor areas, layout, and individual room sizes. The units also comply with relevant standards in terms of light provision, outlook and private amenity space.
- 7.4.7 As such the application is considered acceptable in terms of proposed accommodation standards.

7.5 Design/ character and appearance

- 7.5.1 The National Planning Policy Framework advises that Local Planning Authorities should not attempt to impose architectural styles or particular tastes, and that design policies should concentrate on guiding factors such as the layout of the new development in relation to neighbouring buildings. The fundamental aim of the NPPF is to secure sustainable development. In order to achieve sustainable development, a development is required to have a good design. The matter of design is discussed within Section 12 of the NPPF.
- 7.5.2 It is however proper to seek to promote or reinforce local distinctiveness. In addition, Policy CP30 seeks to maintain and improve the quality of the built and open environment, whilst Policy DMD37 seeks to promote high standards of functional design in developments. London Plan policies 7.4 Local Character and 7.6 Architecture are also relevant.
- 7.5.3 DMD 6 of the DMD provides standards for new development with regards to scale and form of development, housing quality and density. DMD 8 provides general standards for new residential development and reiterates the requirement for a development to be of an appropriate scale, mass and bulk, provide high quality amenity space and provide access to parking and refuse areas. DMD 37 encourages achieving a high quality and design led development. The design of an extension would need to respect the character of the surrounding area but also make a positive contribution to the places identity. This policy is re-iterated by CP30 of the Core Strategy as well as the fundamental aims of the NPPF.

- 7.5.4 In relation to the subject site the character of the area is predominantly residential with properties immediately to the west (3-4 storey block of flats) and to the rear (semi-detached dwellinghouses) along Cavendish Road being largely two-storey single dwelling houses with rear gardens abutting onto the existing car park of the subject site. The North Circular Road lies in close proximity to the front of the site.
- 7.5.6 In terms of bulk, scale and massing, it should be noted that the proposed scheme as submitted proposed two additional storey's which have been negotiated down to one additional story. This would result in a four-storey block of flats similar in scale to the adjacent block at Shillibeer Court, which is considered acceptable.
- 7.5.7 The fenestration has been designed to, insofar as possible, be in line with and proportionate to, the windows and doors on the floors below. However, as the materiality of the scheme is integral to its success, a condition is recommended requiring details of materials and landscaping as indicated to the rear car park area of the site to be submitted prior to any works commencing on site.
- 7.5.8 In conclusion, from the perspective of design, character and appearance the proposal is considered acceptable and as such no objection is raised to this element of the scheme subject to conditions pertaining to materiality and landscaping

7.6 Highways issues and servicing

- 7.6.1 The exiting site comprises 13 car parking spaces which will be increased to 23 with the proposed development. Cycle spaces will also be provided numbering 28 in total. Given the low PTAL 2 rating of the site (meaning access to public transport is poor) the number of proposed car parking spaces is acceptable. Conditions pertaining to the details of cycle storage and waste/recycling storage being submitted for approval, is recommended.
- 7.6.2 Concern has been raised by neighbours regarding the increase in vehicular traffic to and from the site (and potentially the adjacent Shillibeer Court site) and increased pressure on parking. Given the relatively small number of units together with the proposed increase in car parking spaces and cycle storage being proposed at the subject site, it is not considered the development would give rise to an unacceptable impact in terms of traffic and transportation. It should be noted that following additional information being submitted by the applicant, no objection has been raised by Transportation officers on the grounds of parking and/or access.

7.7 Sustainable Design and Construction

- 7.7.1 The National government's policy is that planning permissions should not be granted requiring, or subject to conditions requiring, compliance with any technical housing standards other than for those areas where authorities have existing policies on access, internal space, or water efficiency. Where there is an existing plan policy which references the Code for Sustainable Homes, authorities may continue to apply a requirement for a water efficiency standard equivalent to the new national technical standard, or in the case of energy a standard consistent with the policy set out in the earlier paragraph in this statement, concerning energy performance.

- 7.7.2 Policy 5.3 of the London Plan relates to sustainable design and construction seeking to ensure that the design and construction of new developments have regard to environmental sustainability issues such as energy and water conservation, renewable energy generation, and efficient resource use. Policy CP4 of the adopted Core Strategy states that the Council would adopt a strategic objective to achieve the highest standard of sustainable design and construction throughout the Borough, as such details of water efficiency and energy efficiency are required to be assessed for this planning application.
- 7.7.3 An energy statement has been supplied with the application which notes that as a minor development the proposal has no planning requirement to undertake a formal Code for Sustainable Homes Assessment as stipulated by local policies. It is however recognised that there is still a requirement to construct dwellings with an energy performance standard beyond the mandatory requirements of Part L of the Building Regulations. As such the proposed development targets the equivalent energy performance to Code Level 4, which requires a reduction of regulated carbon dioxide emissions of at least 19% in comparison to Part L. As the submitted Energy Statement outlines how this reduction will be achieved (with an average reduction of 19.72% achieved) then no further information is required to be submitted (via condition) in this respect.
- 7.7.4 Information has been provided in relation to water consumption, which is showing policy compliance which requires development to not use more than 105L of water per person per day. However in the event of approval, information demonstrating the specifics of how this will be achieved would be required by way of condition.

7.8 s106 Obligations

- 7.8.1 Following the Court of appeal decision from 11 May 2016, small site exemptions from paying S106 affordable housing policy contributions and other similar contributions were reinstated. The National Planning Practice Guideline was subsequently updated to provide guidance on small site exemptions.
- 7.8.2 The London Borough of Enfield no longer seeks contributions for education on schemes of 11 units and below. However, it does seek affordable housing contributions which are 10 units or less but have a combined gross floor space of more than 1000m². This is in conjunction with the criteria stipulated within the Planning Practice Guidance. The proposal is question is below the stipulated 1000m² and as such, does not required a S106 contribution towards affordable housing.

7.9 Community Infrastructure Levy (CIL)

- 7.9.1 The application proposes a net gain of 392.60sqm whilst providing additional homes, therefore the scheme would be CIL liable as follows:
- Mayors CIL: 392.60sqm x £20 = £7,852.00
 - Borough CIL: 392.60sqm x £40 = £15,704.00

8. **Conclusion**

- 8.1 Officers consider that on balance the proposal would deliver much needed additional homes for the borough. The development would be in general

compliance with Council policy and there are no material considerations of sufficient weight that would suggest that the application should be refused.

9. Recommendation

- 9.1 In light of the issues outlined in this report it is recommended that this application be granted subject to conditions.

3 Years

1. The development to which this permission relates must be begun not later than the expiration of three years beginning with the date of the decision notice.

Reason: To comply with the provisions of S.51 of the Planning & Compulsory Purchase Act 2004.

Approved Plans

2. The development hereby permitted shall be carried out in accordance with the following approved plans:

Design and Access Statement

Energy Statement

Site Location Plan

048.18.01 Revised Existing Ground Site Plan

048.18.04 Revised Proposed Ground Site Plan

048.18.05 Proposed Second Floor Plan

048.18.06 Proposed Third Floor Plan / Loft Plan

048.18.08 Proposed Front / Rear Elevation

048.18.08A Proposed Front Elevation A-A

048.18.08B Proposed Front Elevation B-B

048.18.08C Proposed Rear Elevation C-C

048.18.08D Proposed Rear Elevation D-D

048.18.09 Proposed Side Elevation

048.18.10 Proposed Side Elevation

048.18.11 Existing and Proposed Streetscene (Montagu Road)

048.18.12 Existing and Proposed Streetscene (Aberdeen Road)

048.18.13 Existing and Proposed Block Plan

048.18.16 Existing Pictures

Reason: For the avoidance of doubt and in the interests of proper planning.

Construction Management Plan

3. That development shall not commence until a construction methodology for the relevant phase has been submitted to and approved in writing by the Local Planning Authority. The construction methodology shall contain:
- a. Arrangements for wheel cleaning;
 - b. Arrangements for the storage of materials;
 - c. Hours of work;
 - d. Arrangements for the securing of the site during construction;
 - e. The arrangement for the parking of contractors' vehicles clear of the highway;

- f. The siting and design of any ancillary structures;
- g. Arrangements for the loading and unloading of plant and materials;
- h. Scheme for recycling/disposing of waste resulting from demolition and construction works;
- i. Enclosure hoarding details; and
- j. Measures that will be taken to control dust, noise and other environmental impacts of the development in accordance with 'London Best Practice Guidance: The control of dust and emission from construction and demolition'.

The development shall be carried out in accordance with the approved construction methodology unless otherwise agreed in writing by the Local Planning Authority.

Reason: To ensure the implementation of the development does not lead to damage to the existing highway and to minimise disruption to neighbouring properties and the environment.

Detailed Drawings

- 4. Prior to the commencement of building works, detailed drawings to a scale of 1:20 to confirm the detailed design and materials of the:
 - a. Schedule and sample of materials used in all elevations;
 - b. Details of all windows and doors at scale 1:10, windows shall be set at least 115mm within window reveal scale 1:10;
 - c. Construction details of all external elements at 1:20 scale (including sections). This should include: entrances and exits, glazing, masonry, weathering and flashings, balustrades and parapets, roof, plant and plant screening, health and safety systems;
 - d. Full drawn details (1:20 scale elevations, 1:2 scale detailing) of the railings and gates (including hinges, fixings, locks, finials).

Shall be submitted to and approved in writing by the Local Planning Authority prior to the commencement of development above ground hereby permitted. The development shall thereafter be carried out solely in accordance with the approved details.

Reason: To safeguard and enhance the visual amenities of the locality.

Samples and Materials

- 5. Prior to the commencement of building works, a sample panel and a schedule of materials to be used in all external elevations including walls, doors, windows front entrances and balconies within the development hereby permitted shall be submitted to and approved in writing by the Local Planning Authority before any building work commences and this condition shall apply notwithstanding any indications as to these matters which have been given in the application. The development shall thereafter be carried out solely in accordance with the approved details.

Reason: In order to ensure that the building has an acceptable external appearance and preserves the character and appearance of the conservation area.

Surfacing Materials

6. Prior to the completion of the external building works, details and design of the surfacing materials to be used within the development including footpaths, shared surfaces, access roads, parking areas, road markings and all other hard surfacing shall be submitted to and approved in writing by the Local Planning Authority. The surfacing shall be carried out in accordance with the approved detail before the development is occupied or use commences.

Reason: To ensure that the development does not prejudice highway safety and a satisfactory appearance.

7. Prior to the completion of the external building works, a Landscape and Public Realm Strategy for all external public realm areas within the curtilage of the site hereby approved shall be submitted to and approved by the Local Planning Authority. This Strategy is to include, amongst other things, details of proposed plant and tree maintenance, paving materials, pedestrian priority materials and shared surface treatments, plant species, ground levels, green roofs, green walls, boundary treatments and water features. The development shall be in accordance with the approved details, unless otherwise agreed in writing by the Local Planning Authority.

Reason: To ensure that the proposed landscaping areas are of a high quality and for consistent treatment of the public realm.

Soft Landscaping

8. Prior to the completion of the external building works on the relevant phase of development, details of trees, shrubs, grass and all other soft landscaped areas of internal and external amenity spaces to be planted on the site shall be submitted to and approved in writing by the Local Planning Authority. The planting scheme shall be carried out in accordance with the approved details in the first planting season after completion or occupation of the development whichever is the sooner. Any planting which dies, becomes severely damaged or diseased within five years of planting shall be replaced with new planting in accordance with the approved details.

Reason: To provide a satisfactory appearance and ensure that the development does not prejudice highway safety.

Use of Parking Area

9. The parking area(s) forming part of the development shall only be used for the parking of private motor vehicles and shall not be used for any other purpose.

Reason: To ensure that the development complies with Development Plan Policies and to prevent the introduction of activity which would be detrimental to amenity.

Refuse Stores

10. Prior to occupation of the development, details of refuse storage facilities including facilities for the recycling of waste to be provided within the development, in accordance with the London Borough of Enfield Waste and Recycling Planning Storage Guidance ENV 08/162, shall be submitted and

approved in writing by the Local Planning Authority. The facilities shall be provided in accordance with the approved details before the development is occupied or use commences.

Reason: In the interests of amenity and the recycling of waste materials in support of the Boroughs waste reduction targets.

Cycle Parking

11. Prior to occupation of each phase of the development, details of the siting, number and design of secure/covered cycle parking spaces shall be submitted and approved in writing by the Local Planning Authority. The approved details shall thereafter be installed and permanently retained for cycle parking.

Reason: To ensure the provision of cycle parking spaces in line with the Council's adopted standards.

Water Consumption

12. Prior to first occupation details of the internal consumption of potable water shall be submitted to and approved in writing by the Local Planning Authority. Submitted details will demonstrate reduced water consumption through the use of water efficient fittings, appliances and recycling systems to show consumption equal to or less than 105 litres per person per day. The development shall be carried out strictly in accordance with the details so approved and maintained as such thereafter.

Reason: To promote water conservation and efficiency measures in all new developments and where possible in the retrofitting of existing stock in accordance with Policy CP21 of the Core Strategy, Policy 5.15 of the London Plan.

Energy Performance Certificate

13. Following practical completion of works a final Energy Performance Certificate shall be submitted to and approved in writing by the Local Planning Authority. Where applicable, a Display Energy Certificate shall be submitted within 18 months following first occupation.

Reason: In the interest of sustainable development and to ensure that the Local Planning Authority may be satisfied that CO2 emission reduction targets are met in accordance with Policy CP20 of the Core Strategy, Policies 5.2, 5.3, 5.7 & 5.9 of the London Plan 2011 and the NPPF

Protection of Habitats

14. In the event that any construction works necessitate the clearance of nesting bird habitats, it is recommended that any such works are undertaken in the period August to February inclusive. Should it prove necessary to remove any breeding bird habitat during the breeding season, the area should be checked in advance for the presence of birds' nests. Once checked, if there is no evidence of breeding birds, clearance work should be completed within 48 hours of inspection. If any active nests are found in this area then vegetation clearance must cease and an appropriate buffer zone should be established.

This buffer must be left intact until it has been confirmed that the young have fledged and the nest is no longer in use.

Reason: To ensure that bird nests and bird habitats are provided with appropriate protections during the construction phase.

Construction - Good Practice

15. During the construction phases, including demolition and pre-construction works, all reasonable measures shall be taken to ensure good production practices are adopted to safeguard individual animals. Such practices would include covering of all deep holes and trenches overnight and the provision of planked escape routes for any trapped wildlife. Any liquids held at the site should be stored in a secure lock-up. Such methods shall be employed for the duration the construction period.

Reason: To ensure that any animals who inadvertently enter the site are afforded appropriate protections.

Air Quality Assessment

16. The development shall not commence until an air quality assessment report has been submitted to and approved by the local planning authority. The assessment shall compare the levels of nitrogen dioxide and PM10 to the objective levels set out in the Air Quality Standards Regulations 2010 and propose mitigation where the objective levels are exceeded for either pollutant at the façade of the development. Mitigation measures shall be fully implemented prior to occupation.

Reason: To protect residents from poor air quality

Sound insulation

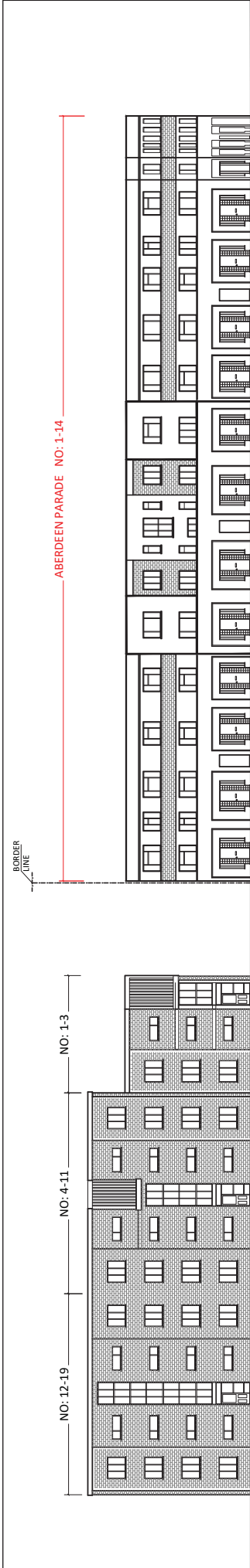
17. The development shall be constructed/adapted so as to provide sufficient air-borne and structure-borne sound insulation against externally generated noise and vibration. This sound insulation shall ensure that the level of noise generated from external sources shall be no higher than 35 dB(A) from 7am – 11pm in bedrooms, living rooms and dining rooms and 30 dB(A) in bedrooms from 11pm – 7am measured as a $L_{Aeq,T}$. The $L_{AF Max}$ shall not exceed 45dB in bedrooms 11pm – 7am. A scheme for mitigation measures shall be submitted to and approved by the Local Planning Authority prior to development taking place. The scheme of mitigation shall include mechanical ventilation where the internal noise levels exceed those stated in BS8233: 2014 with the windows open. The approved mitigation scheme shall be implemented in its entirety before any of the units are occupied/the use commences.

Reason: To protect future residents from noise and disturbance.

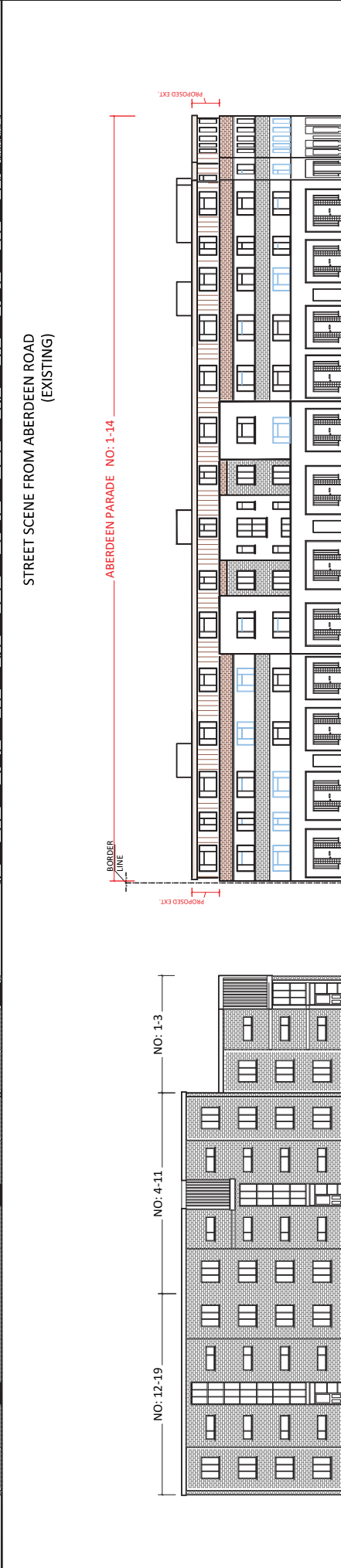
Informatives:

1. The applicant is reminded of their obligations under the Conservation of Habitats and Species Regulations (2010) as amended and the Wildlife and Countryside Act 1981 (as amended) in relation to the protection of bats and bat habitats. In

the event that any harm occurs under the works herein approved the developer may be liable to prosecution under the aforementioned legislation.



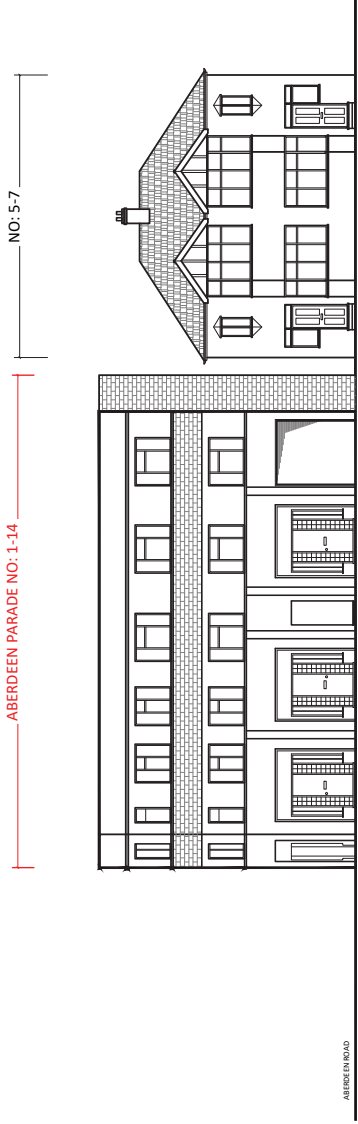
STREET SCENE FROM ABERDEEN ROAD
(EXISTING)



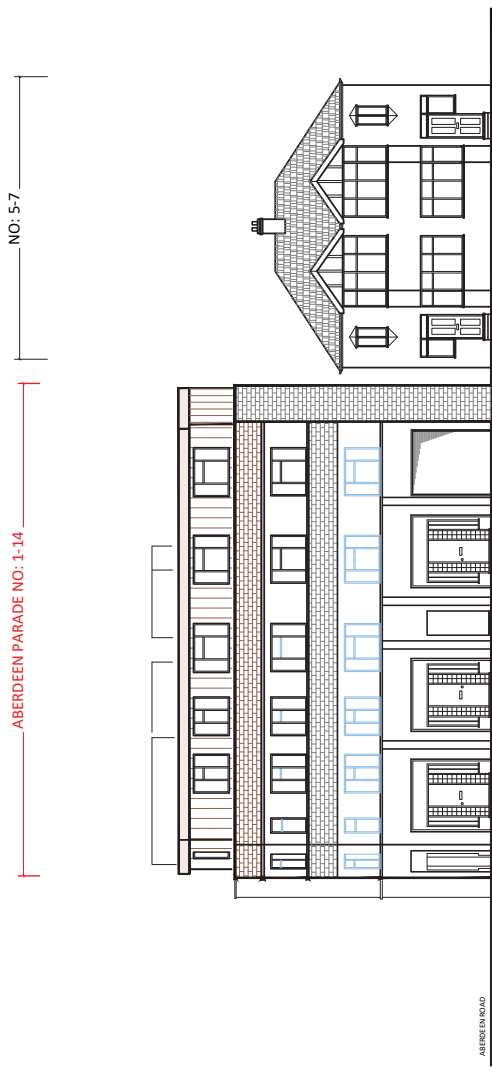
STREET SCENE FROM ABERDEEN ROAD
(PROPOSED)

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<p>EXISTING & PROPOSED - STREET SCENE (FROM ABERDEEN ROAD)</p>		
SCALE: N/S 1/500	REF. NO: 048.18/11	
DATE: MAY 2018	DRG BY: N.K.	
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STREET SCENE FROM MONTAGU ROAD
(EXISTING)
SCALE:1/200



STREET SCENE FROM MONTAGU ROAD
(PROPOSED)
SCALE:1/200



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<p>EXISTING & PROPOSED - STREET SCENE (FROM MONTAGU ROAD)</p>		
SCALE: 1/200	REF. NO: 048.18/12	
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FRONT VIEW



FRONT VIEW



FRONT VIEW



REAR VIEW



REAR VIEW

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1-14 ABERDEEN PARADE
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- PICTURES

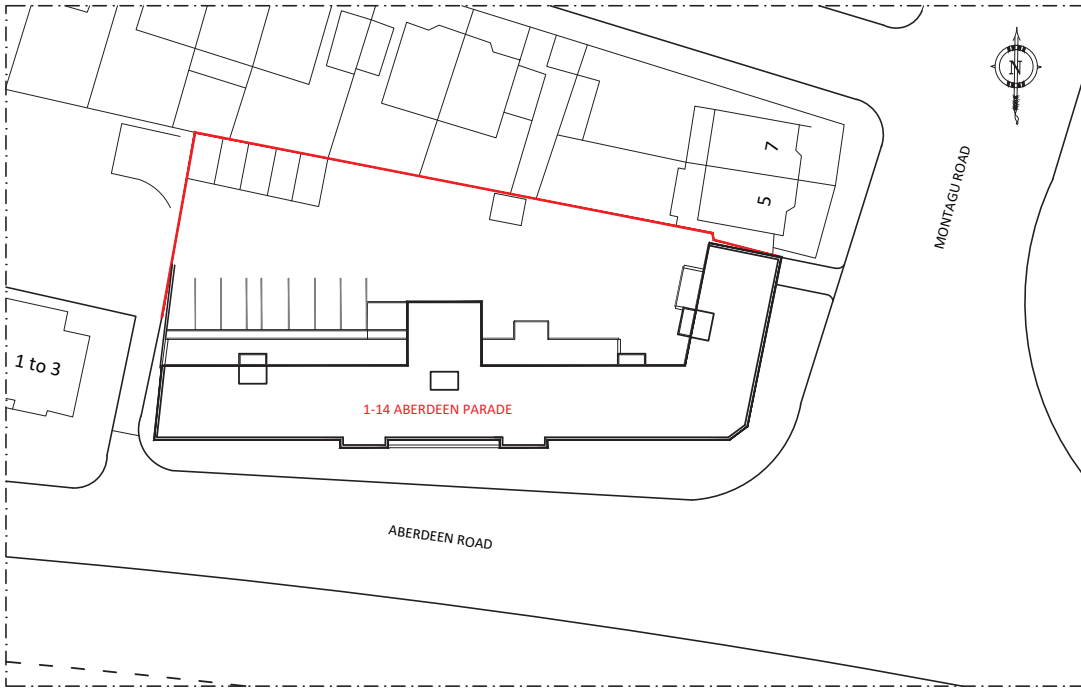
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REF. NO : 048.18/16

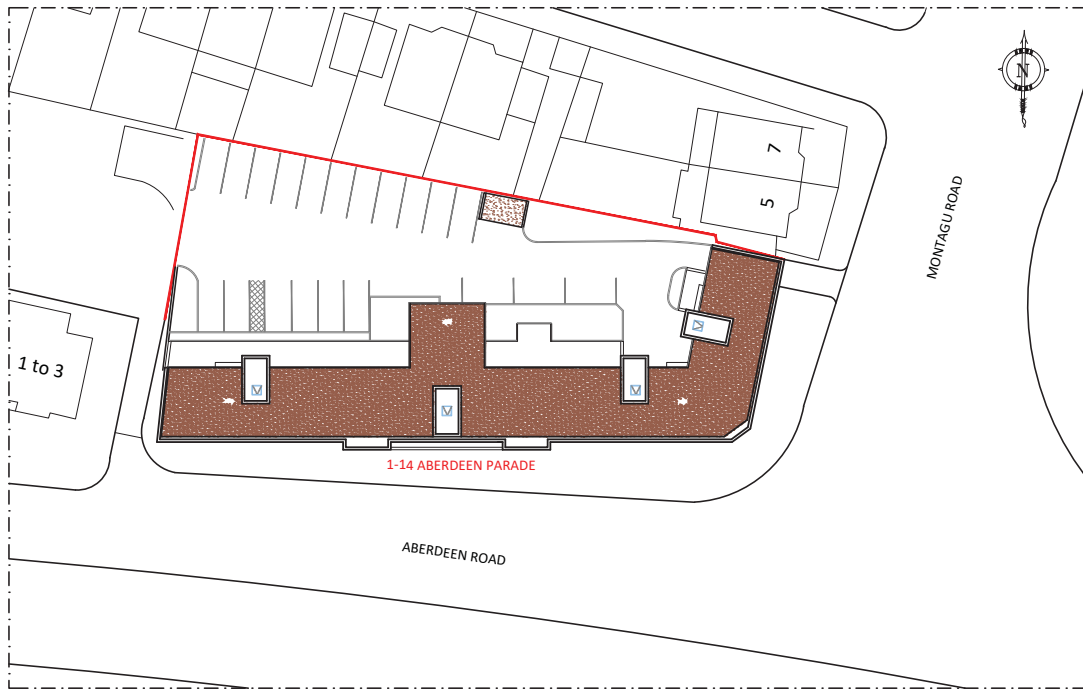
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EXISTING SITE PLAN
SCALE: 1/500



PROPOSED SITE PLAN
SCALE: 1/500



REV.	SEP.	REV. DRAWING
A	18	

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- EXISTING BLOCK PLAN
- PROPOSED BLOCK PLAN

SCALE: 1/500 REF. NO: 048.18/13

DATE: MAY 2018 DRG BY: N.K.

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A < 1/4



A < 1/4

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<p>1-14 ABERDEEN PARADE ABERDEEN ROAD LONDON N18 2EB</p>		
<p>- FRONT ELEVATION (A-A)</p>		
SCALE: 1/100	REF. NO: 048.18/08A	
DATE: MAY 2018	DRG BY: N. K.	
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FRONT ELEVATION
 SCALE: 1/100

PROPOSED EXT.





FRONT ELEVATION
SCALE: 1/200



REAR ELEVATION
SCALE: 1/200

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<p>PROPOSED - FRONT ELEVATION - REAR ELEVATION</p>		
SCALE: 1/200	REF. NO: 048.18/08	
DATE: MAY 2018	DRG BY: N.K.	
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D<1



D<1

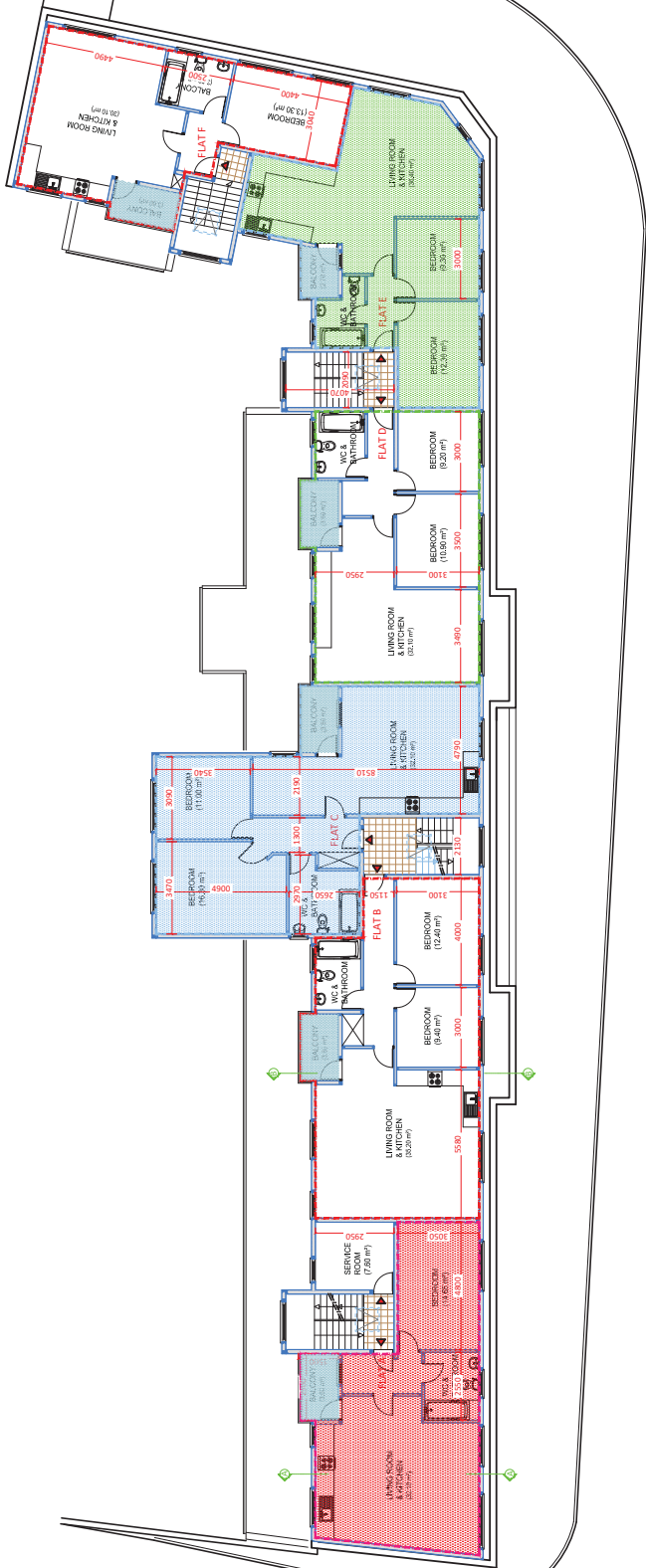
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<p>- REAR ELEVATION (D-D)</p>		
SCALE: 1/100	REF. NO: 048.18/08D	
DATE: MAY 2018	DRG BY: N. K.	
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REAR ELEVATION (D-D)
SCALE: 1/100

PROPOSED EXT.

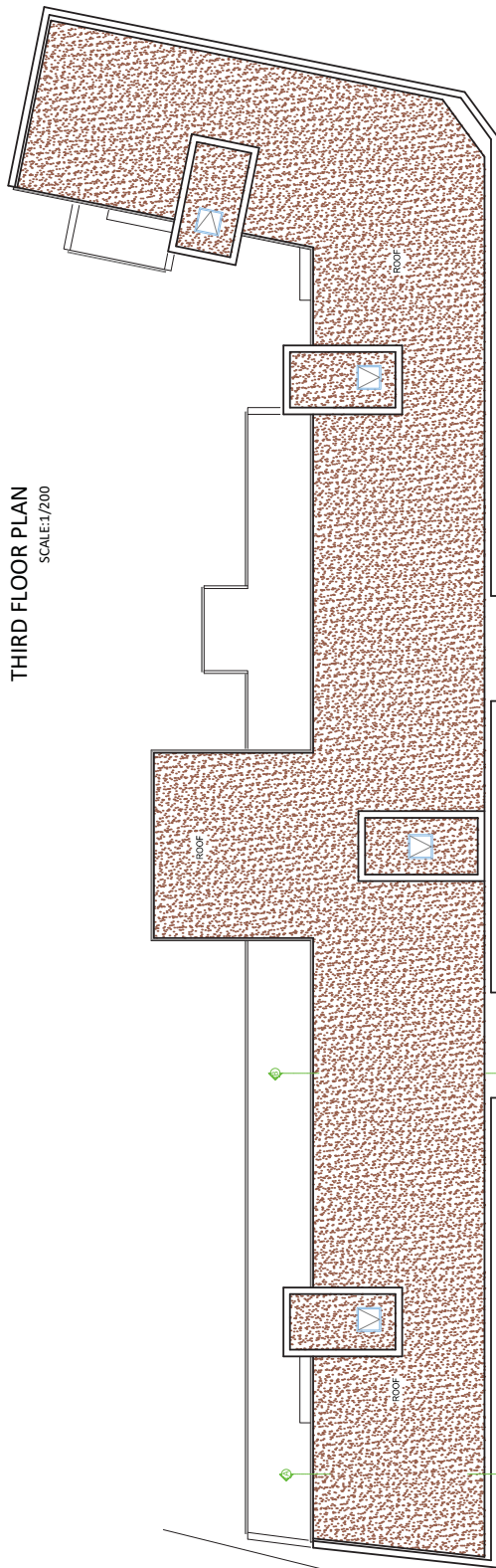
FLAT NO	BEDROOM	SQ
FLAT A	1	59.00 m ²
FLAT B	2	73.70 m ²
FLAT C	2	76.20 m ²
FLAT D	2	61.90 m ²
FLATE	2	67.30 m ²
FLAT F	1	54.50 m ²

2x4 BEDROOM
4x2 BEDROOM
6 UNIT



THIRD FLOOR PLAN
SCALE:1/200

ABERDEEN ROAD



LOFT PLAN
SCALE:1/200

0m 2m 5m 10m 20m

REV.	SEP.	REV.
A	18	DRAWING

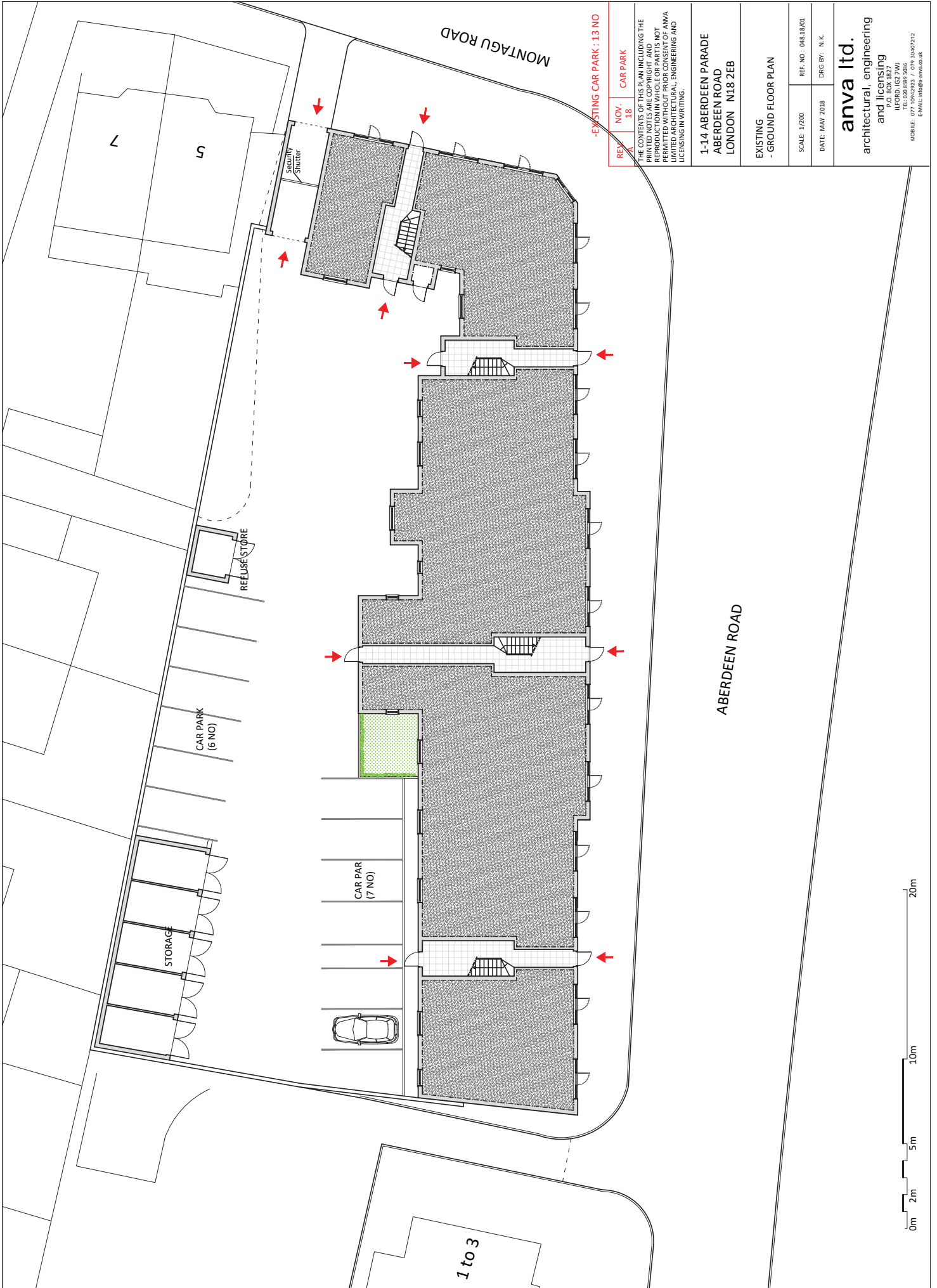
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**1-14 ABERDEEN PARADE
ABERDEEN ROAD
LONDON N18 2EB**

**PROPOSED
- THIRD FLOOR PLAN
- LOFT PLAN**

SCALE: 1/200 REF. NO: 048.48/06
DATE: MAY 2018 DRG BY: N.K.

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TEL: 020 8999 5086
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-EXISTING CAR PARK : 13 NO

REV. A	NOV. 18	CAR PARK
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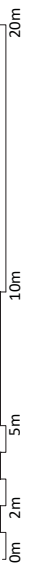
1-14 ABERDEEN PARADE
 ABERDEEN ROAD
 LONDON N18 2EB

EXISTING
 - GROUND FLOOR PLAN

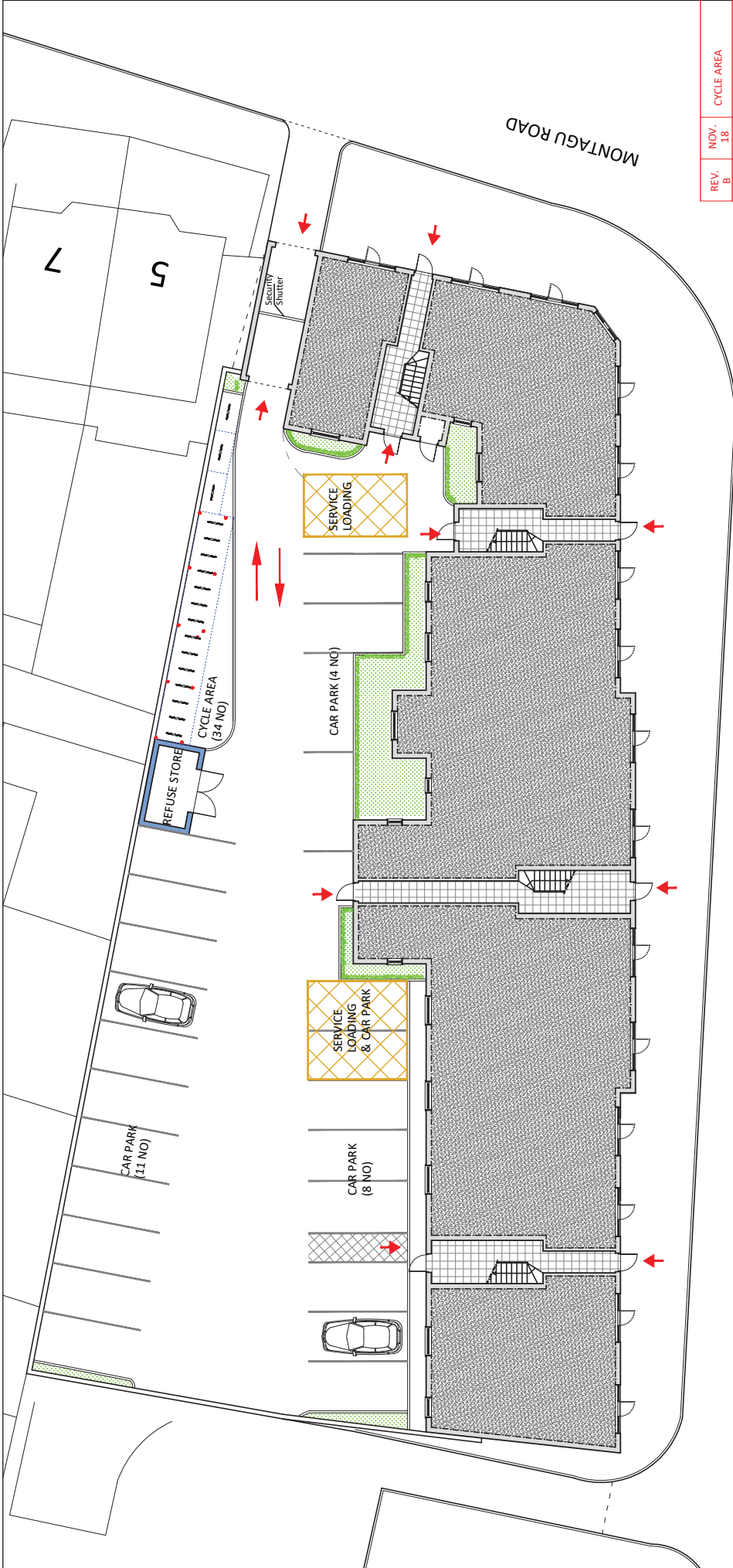
SCALE: 1/200	REF. NO: 048.18/01
DATE: MAY 2018	DRG. BY: N.K.

anva ltd.
 architectural, engineering
 and licensing
 115-117, BRISTOL ROAD
 LONDON, E15 2AW
 TEL: 020 8598 5036
 EMAIL: info@anva.co.uk

ABERDEEN ROAD



1 to 3



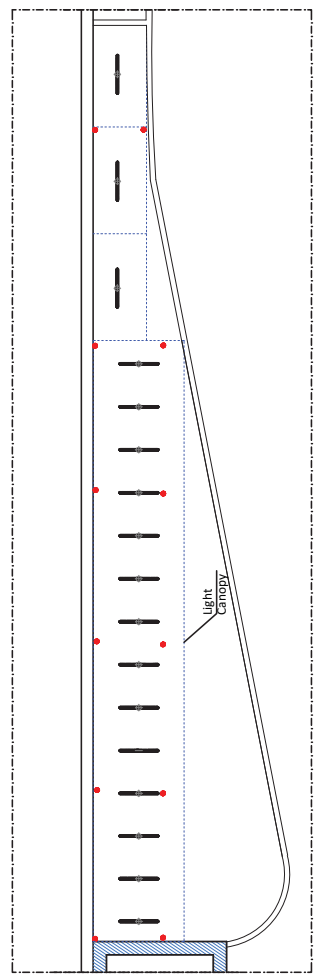
REV. B	NOV. 18	CYCLE AREA
REV. A	JULY 18	ADDITIONAL INF. FOR CAR PARKING/CYCLE /LOADING

1-14 ABERDEEN PARADE
 ABERDEEN ROAD
 LONDON N18 2EB

PROPOSED
 - GROUND FLOOR PLAN

SCALE: 1/200
 REF. NO: 048.18/04
 DATE: MAY 2018
 DRG BY: N.K.

anva ltd.
 architectural, engineering
 and licensing
 110 GERRARD STREET
 LONDON EC2A 4AW
 TEL: 020 8598 5086
 EMAIL: info@anva.co.uk



CYCLE AREA
 SCALE :1/100

ADDITIONAL INFORMATION FOR:
 -CAR PARK : 23 NO
 -CYCLE : 34 NO
 -LOADING : 2 SPACES

ABERDEEN ROAD

MONTAGU ROAD

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LONDON BOROUGH OF ENFIELD

PLANNING COMMITTEE

Date: 20th November 2018

Report of

Executive Director - Place

Contact Officer:

Carolyn Southall
Andy Higham

Tel No: 0208 379 2592

Ward:

Cockfosters

Ref: 18/03000/RE4 and 18/03001/LBC

Category: RE4 - LBE (Dev by LA) and LBC - Listed Building Consent

LOCATION: De Bohun Primary School, Green Road, New Southgate N14 4AD

PROPOSAL: Construction of single storey classroom building with external canopy, associated fencing, gates, renewal of existing footpaths, play areas and minor internal alterations provide quiet space within classroom of existing main school building.

Applicant Name & Address:

Schools & Children's Services
London Borough of Enfield
Civic Centre
Silver Street
Enfield
EN1 3ES

Agent Name & Address:

Mr T Nadaraju
Enfield Council
Civic Centre
Silver Street
Enfield
EN1 3ES

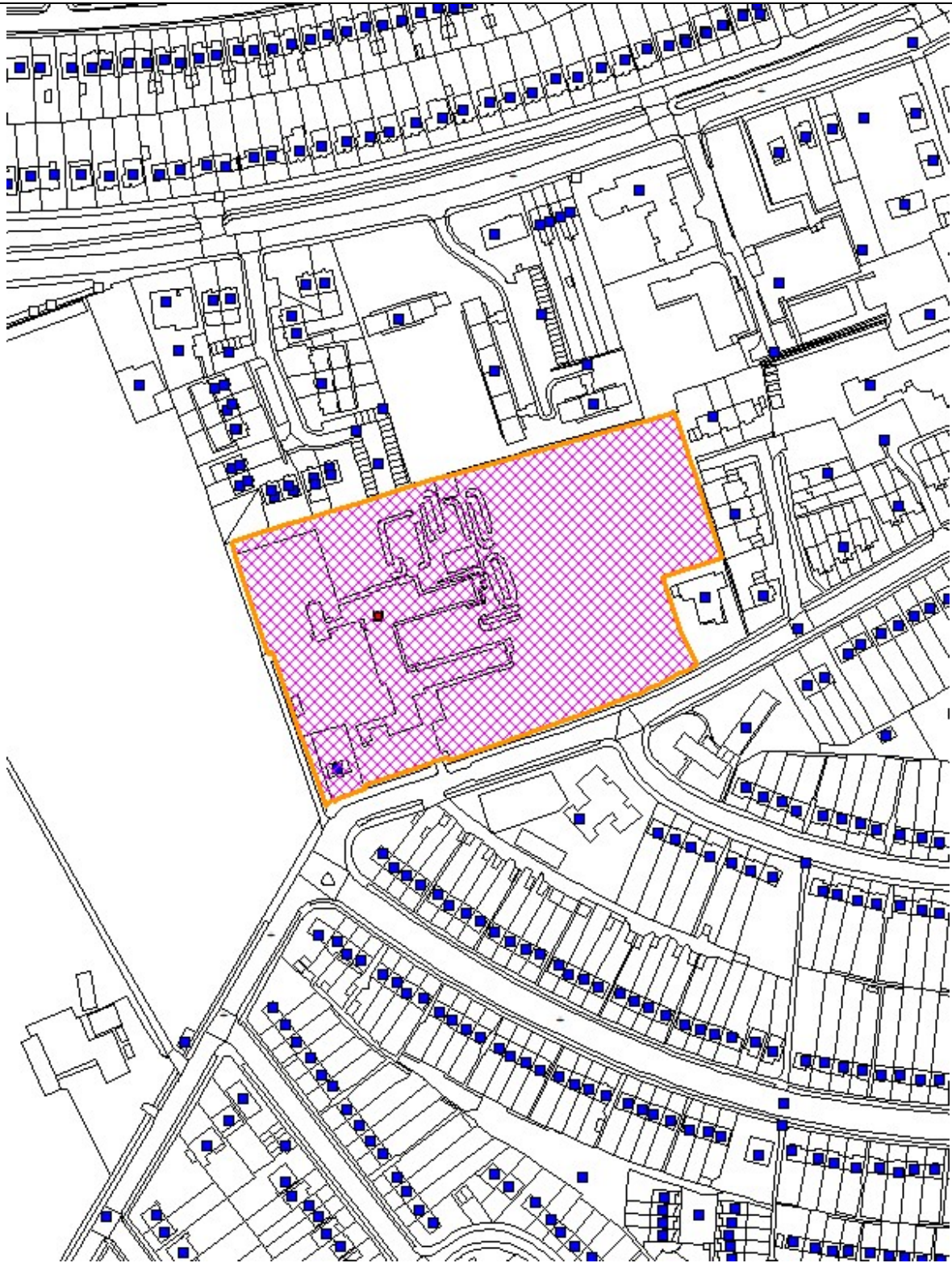
RECOMMENDATION:

18/03000/RE4: In accordance with Regulation 4 of the Town and Country Planning General Regulations 1992, planning permission be deemed to be granted subject to conditions.

18/03001/LBC: That the Head of Development Management/Planning Decisions Manager(s) be authorised to finalise conditions and to grant listed building consent subject to conditions.

Note for Members: A council department is acting as the agent for this application, meaning that, in the interests of transparency, the application has been referred to Committee for decision.

Ref: 18/03000/RE4 LOCATION: De Bohun Primary School, Green Road, New Southgate, N14



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Scale 1:1250

North



1. Summary of Main Issues

1.1 The main issues involved in this application are:

The visual impact of the proposed building on the openness, character and appearance of the listed building and surrounding area;

The impact of the proposal upon surrounding residential amenity in terms of visual intrusion, a sense of enclosure, loss of light, privacy, noise and disturbance;

The impact of the development upon the highway network, conditions of highway safety and levels of parking provision.

2. Site and Surroundings

2.1 De Bohun Primary School is situated on the northern side of Green Road. The school, with an overall area of approximately 2 hectares, contains a number of buildings. It is a two-form entry school over seven school years with 60 children in each year group resulting in a school roll of 420 pupils.

2.2 The location of the additional classroom is to the north-west of the school site bounded by garages serving Bramley Close and the communal outside space to The Poplars, a block of three-storey flats. Salcombe Preparatory School, a Grade II Listed building and Homestead Paddock, terraces of two-storey houses, lie to the west of the site separated by the school playing fields. The area to the south of the site, across Green Road, comprise terraces of flats/houses and community buildings.

2.3 De Bohun Primary School is a Grade II Listed building. Constructed in 1936 by W T Curtis and H W Burchett of the Middlesex County Council's Architects Department, listed for its special architectural interest. It forms a cluster of three separately Grade II Listed structures including the school, Salcombe Preparatory School (originally constructed as De Bohun Library, Health Centre and gates) designed by the same architect, and De Bohun Park Clock Tower, all constructed in the mid to late 1930s.

3. Proposed Development

3.1 This application seeks planning permission for the erection of a permanent classroom block with associated facilities to provide additional accommodation at De Bohun Primary School. The proposed accommodation will be sited adjacent to the existing Pre-school accommodation located towards the north of the site.

3.2 The proposed rectangular classroom has a height of 3.6m to the top of the parapet, 14m in length and 7.5m wide. The classroom will have a shallow pitch roof hidden by the parapet. It would provide a classroom of 75m² incorporating classroom space, entrance lobby/coats area, classroom storage, pupil toilets and external storage. The classroom has two canopies at 2.4m high projecting from the building, one over the entrance door the second over double doors towards the external playground. The building will be finished in bricks and bonding to match the existing school, white concrete parapet coping, white polyester powder coating finish to metal windows and door with colour mosaics or tiles placed within a render bands around the building and above the doors and windows.

3.3 The courtyard space created between the existing nursery building and the proposed classroom will provide an external play area for the Nursery and Reception classes and will be finished in a porous macadam.

- 3.4 There are multiple accesses to the site, the main vehicular and pedestrian access located to the front of the main building. Access to the proposed classroom is via an existing, separate, gated, pedestrian access from Green Road taking a northerly direction past the main school building to the north west and the playing field to the north east. Replacement fencing between the existing building and the new classroom will match existing.
- 3.5 A number of trees are located in the vicinity of the proposed classroom and associated works. A liquidamber styraciflua and a prunus subhirtella (classified as Category B) would require felling in order to facilitate the new development.
- 3.6 Internal alterations involve the installation of an enclosed space to a ground floor classroom in the eastern wing of the existing school. The 6.25sqm enclosed space would be used as a quiet room for pupils and would be constructed in metal studwork.

4. Planning History

- 4.1 The current proposal supersedes an earlier, larger, aluminium clad development which was withdrawn at planning stage. Prior to the current submission, the agent on behalf of the Council conducted detailed pre-application discussion with the Councils various Officers over the evolution of the design proposal, which have evolved into the current proposal.
- 4.2 There have been many applications for work to the school – none of which are relevant to the current proposal.

5. Consultation

- 5.1 Neighbours: Application reference **18/03000/RE4** was referred to 57 surrounding properties (21 days expired on 19.09.2018). Press notice expired 05.09.2018. Application reference **18/03001/LBC** was referred to 114 neighbours (21 days expired on 06.09.2018). One objection was received objecting to an increase in parking problems as a result of the new classroom.

Press Notice expired 05.09.2018; Site Notice expired 15.11.2018.

Internal Consultation

- 5.2 Traffic and Transportation: The Traffic and Transportation Briefing Note accompanying the application is based on a previous larger application. It notes that the increase in pupil numbers will have a limited impact compared to the existing arrangements; the main change will be an increase in PM peak trips. There are also a range of mitigation measures proposed including the updating of the existing school travel plan and more active management of the parking issues arising from the school.

Taking this into account, the change in pupil numbers and resultant trips will not be significant in terms of the wider transport network, therefore no objection subject to the following conditions:

- Cycle parking and / or scooter provision of 4 long stay and 2 short stay spaces;
- Pedestrian access particularly the width of the footway in the vicinity of the pre-school access and the width and lighting of on-site paths to the new provision;

- Updated travel plan for the site including details of the proposed mitigation measures; and
- Construction traffic management and safe operation on site.

Given the relative increase in pupil numbers it is acceptable that existing servicing and refuse arrangements will be utilised.

- 5.3 SuDs: Concerns were initially raised however following discussions with the applicant's consultants the issues were resolved and officers raise no objection subject to condition.
- 5.4 Trees: The two trees to be removed are not particularly significant and can be easily replaced via condition of a landscape scheme to included tree planting. Recommend a Tree Protection Plan and Arboricultural Method Statement, in accordance with BS5837:2012, are submitted via condition.

External Consultation

- 5.5 Historic England: No response
- 5.6 Ancient Monuments Society: No response
- 5.7 The Council For British Archaeology: No response
- 5.8 The Georgian Group: No response
- 5.9 The Twentieth Century Society: No response
- 5.10 The Society For The Protection Of Ancient Monuments: No response
- 5.11 The Victorian Society: No response
- 5.12 Conservation Area Advisory Group: Now a much-reduced proposal that overcomes CAG's concerns relating to the original proposal.
- 5.13 Environment Agency: No response

6. Relevant Policy

6.1 Development Management Document

DMD37	Achieving High Quality and Design-Led Development
DMD44	Conserving and Enhancing Heritage Assets
DMD48	Transport Assessments
DMD49	Sustainable Design and Construction Statements
DMD50	Environmental Assessment Methods
DMD51	Energy Efficiency Standards
DMD80	Trees on Development Sites

6.2 Core Strategy

CP8	Education
CP21	Delivering sustainable water supply, drainage and sewerage infrastructure
CP30	Maintaining and improving the quality of the built and open environment
CP31	Built and Landscape Heritage

6.3 **London Plan**

Policy 5.13	Sustainable drainage
Policy 6.9	Cycling
Policy 6.10	Walking
Policy 6.11	Smoothing traffic flow and tackling congestion
Policy 6.13	Parking
Policy 7.4	Local character
Policy 7.6	Architecture
Policy 7.8	Heritage Assets and Archaeology

Other Relevant Policy

6.4 **National Planning Policy Framework (2018)**

The National Planning Policy Framework (NPPF) introduces a presumption in favour of sustainable development. In this respect, sustainable development is identified as having three dimensions - an economic role, a social role and an environmental role. For decision taking, this presumption in favour of sustainable development means:

- Approving development proposals that accord with the development plan without delay; and
- Where the development plan is absent, silent or relevant policies are out of date, granting permission unless:

Any adverse impacts of doing so would significantly and demonstrably outweigh the benefits, when assessed against the policies in the Framework taken as a whole.

6.5 **National Planning Practice Guidance (2018)**

Of particular note for members, paragraph 184 states: Heritage assets range from sites and buildings of local historic value to those of the highest significance, such as World Heritage Sites which are internationally recognised to be of Outstanding Universal Value. These assets are an irreplaceable resource and should be conserved in a manner appropriate to their significance, so that they can be enjoyed for their contribution to the quality of life of existing and future generations.

7. Analysis

7.1 The main issues for consideration regarding this application as follows;

- Principle of the Development;
- Design and Impact on the character and setting of the Grade II Listed building;
- Neighbouring Amenity;
- Traffic and parking issues; and
- Sustainability.

Principle of the Development

- 7.2 De Bohun School comprises a complex of buildings located on Green Road, and is a long-established school, constructed in 1936.
- 7.3 The proposal reflects the increase in local demand for pre-school and nursery places within the borough, and as such seeks to address the lack of space in the main school building. Following a period of pupil number reduction, the school is now in a position that it has a waiting list and has continued to increase the number of pupils and is now, for the first time in at least 6 years, in a situation where there is no space capacity.
- 7.4 The proposed works are vital to ensuring the viable use of this Grade II Listed building, consequently the increase of pupil spaces through the extension of the established school grounds is, in principle, acceptable.

Heritage Considerations

- 7.5 Section 16 of the National Planning Policy Framework (2018) (Conserving and enhancing the historic environment) advises Local Planning Authorities to recognise heritage assets as an “irreplaceable resource and should be conserved in a manner appropriate to their significance, so that they can be enjoyed for their contribution to the quality of life of existing and future generations”.
- 7.6 London Plan policy 7.8 (“Heritage Assets and Archaeology”) advises what boroughs should do at a strategic level to identify, preserve, and enhance London’s heritage assets. Policy CP31 (“Built and Landscape Heritage”) of the Core Strategy sets out a requirement that development should conserve and enhance designated and non-designated heritage assets. Policy DMD44 (“Conserving and Enhancing Heritage Assets”) states that development which fails to conserve and enhance the special interest, significance or setting of a heritage asset will be refused. The design, materials and detailing of development affecting heritage assets or their setting should conserve the asset in a manner appropriate to its significance.
- 7.7 De Bohun School is statutory listed (Grade II) due to its architectural and historic interest. It is set in its own grounds providing uninterrupted views from Green Road and the surrounding area and as a whole, its open plan setting remains intact from the date of construction.
- 7.8 The overall scale and massing of the building would be similar to the existing additional building to which this proposal will be associated. Finished in a mixed palette of brick, render and coloured panels, the applicant has adopted a contemporary approach to the design of the new building. In many cases a pastiche approach would be desirable, however, for this type of building, in this location and for the specific needs of the end user, i.e. the children, it is an acceptable addition and is endorsed by the Conservation Area Advisory Group.
- 7.9 It is inevitable that the building would be visible when viewed from the surrounding area. However, located towards the northern boundary, significantly separated from the northern wing of the original school, and beyond an existing ancillary classroom, the proposed building would conserve the integrity of the main school building and the setting in which it is located.
- 7.10 Also included in the application is the introduction of a “quiet space” located within an existing classroom within the main school building.

- 7.11 Subject to suitable conditions to ensure all new internal works and finishes match the existing adjacent work, the subdivision of the exiting classroom would sustain and enhance the integrity of the listed building.

Impact on Trees

- 7.10 London Plan policy 7.21 considers, existing trees of value should be retained and any loss as the result of development should be replaced. Wherever appropriate, the planting of additional trees should be included in new developments, particularly large-canopied species. Enfield policy DMD 80 also resists the loss of or harm to trees covered by Tree Preservation Orders or trees of significant amenity or biodiversity value. Where there are exceptional circumstances to support the removal of such trees, adequate replacement must be provided.
- 7.11 The planning application was supported by a Tree Survey and Arboricultural Implications Assessment which recognises, in the short term, will have a negative impact on the locality. In order to mitigate the impact, replacement planting is supported in keeping with the existing species and locality.
- 7.12 The Council's Tree Officer has been consulted on the development and is able to support the scheme. He feels the two trees to be removed are not particularly significant and can be easily replaced via a suitable condition for a landscape scheme

Impact on Amenities of adjoining occupiers

- 7.13 The proposed building is located within an existing school complex, approximately 30 metres from the nearest residential building.
- 7.14 Although this proposal has come about following an increase in the overall number of pupils to the nursery and reception classes, the intensification in numbers is considered to be a low increase overall within an existing school environment. Consequently, any impact on the amenity of adjoining residents is de minimis. It should be noted that no objection has been raised by neighbours on this particular issue.

Highway Considerations

- 7.15 The application site has a Public Transport Accessibility Level (PTAL) level of 2 (poor) which indicates that the area is not well connected to public transport services. There is no controlled parking zone (CPZ) in the locality. Orange hazard markings limit parking directly outside the school.
- 7.16 An objection has been received from a local resident objecting to the existing parking pressure in the vicinity of the school.
- 7.17 Following consultation on the proposal, the Council's Transportation Officer has assessed The Traffic and Transportation Briefing Note accompanying the application, and states it is based on a previous larger application. It notes that the increase in pupil numbers will have a limited impact compared to the existing arrangements; the main change will be an increase in PM peak trips. There are also a range of mitigation measures proposed including the updating of the existing school travel plan and more active management of the parking issues arising from the school. Taking this into

account, the change in pupil numbers and resultant trips will not be significant in terms of the wider transport network, therefore no objection is raised subject to conditions.

- 7.18 When visiting the site, the case officer, in discussion with reception teachers, was advised that driving from relatively short distances at drop off/pickup was common place. With no provision for secure on-site scooter/bike (i.e. children's) provision, there is little visible incentive to encourage non-vehicle trips for children and their parents/carers. A condition incorporating suitable provision for the storage of scooters/bikes within close proximity of the nursery/reception classes could alleviate current perceived parking congestion and contribute to a healthier school environment.

Servicing and Refuse

- 7.19 Given the relative increase in pupil numbers it is acceptable that existing servicing and refuse arrangements will be utilised.

Community Infrastructure Levy

- 7.20 The proposal would not seek to create net additional floor space of 100 square metres or more, therefore it would not be liable for the levy.

8. Conclusion

- 8.1 The proposed development would provide a good quality classroom to meet the need of the immediate local community. The scale, design and location of the building would preserve the setting of the listed building and have a negligible impact on residents' amenity on-street parking provision.

9. Conditions

18/03000/RE4

3 Years

1. The development to which this permission relates must be begun not later than the expiration of three years beginning with the date of the decision notice.

Reason: To comply with the provisions of S.51 of the Planning & Compulsory Purchase Act 2004.

Approved Plans

2. The development hereby permitted shall be carried out in accordance with the approved plans, as set out in the attached schedule which forms part of this notice.

Reason: For the avoidance of doubt and in the interests of proper planning.

3 Sustainable Drainage Strategy

Notwithstanding the details set out in the submitted Preliminary Drainage Strategy, prior to the commencement of any construction work, details of the Sustainable

Drainage Strategy shall be submitted to and approved in writing by the Local Planning Authority and must conform with the Landscaping Strategy. The details shall include:

- Sizes, storage volumes, cross-sections, long-sections (where appropriate) and specifications of all the source control SuDS measures including rain gardens, raised planters, green roofs, swale and permeable paving
- Details and specifications of the outfall/ discharge control mechanism
- Management Plan for future maintenance

Reason: To ensure the sustainable management of water, minimise flood risk, minimise discharge of surface water outside of the curtilage of the property and ensure that the drainage system will remain functional throughout the lifetime of the development in accordance with Policy CP28 of the Core Strategy, DMD Policy 61, and Policies 5.12 & 5.13 of the London Plan and the NPPF and to maximise opportunities for sustainable development, improve water quality, biodiversity, local amenity and recreation value.

Samples and Materials

4. Prior to commencement of development above ground, a sample panel and a schedule of materials to be used in all external elevations including walls, doors, windows front entrances and balconies within the development hereby permitted shall be submitted to and approved in writing by the Local Planning Authority before any building work commences and this condition shall apply notwithstanding any indications as to these matters which have been given in the application. The development shall thereafter be carried out solely in accordance with the approved details.

Reason: In order to ensure that the building has an acceptable external appearance and preserves the character and appearance of the conservation area.

Restriction of Use of Roof

5. Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) Order 1995, or any amending Order, no plant or equipment etc. shall be erected on the roof of the building. The roof shall not be used for any recreational purpose and access shall only be for the purposes of the maintenance of the building or means of emergency escape.

Reason: To safeguard the privacy of the occupiers of adjoining properties.

Surfacing Materials

6. Prior to commencement of development above ground, details and design of the surfacing materials to be used within playground and all other hard surfacing shall be submitted to and approved in writing by the Local Planning Authority. The surfacing shall be carried out in accordance with the approved detail before the development is occupied or use commences.

Reason: To ensure that the development does not prejudice highway safety and a satisfactory appearance.

Soft Landscaping

7. Prior to commencement of development above ground, details of trees, shrubs, grass and all other soft landscaped areas of internal and external amenity spaces to be planted on the site shall be submitted to and approved in writing by the Local Planning Authority. The planting scheme shall be carried out in accordance with the approved details in the first planting season after completion or occupation of the development whichever is the sooner. Any planting which dies, becomes severely damaged or diseased within five years of planting shall be replaced with new planting in accordance with the approved details.

Reason: To provide a satisfactory appearance and ensure that the development does not prejudice highway safety.

Replacement Planting

8. That replacement planting shall take place during the first autumn following completion of development, unless otherwise agreed in writing by the Local Planning Authority.

Reason: To secure early replacement planting, and to maintain the Borough's stock of amenity trees.

Tree Protection

9. The work to be undertaken in accordance with good arboricultural practice and British Standard 3998.

Reason: In order to maintain the tree(s) (in the vicinity of the development) amenity value and health.

Energy Statement

10. The development shall not commence until a detailed 'Energy Statement' and relevant SAP calculations has been submitted and approved in writing by the Local Planning Authority. Submitted details will demonstrate the energy efficiency of the development and shall provide a significant reduction in total CO₂ emissions arising from the operation of a development and its services over Part L of Building Regs 2010 in line with Council and London Plan Policy. The Energy Statement should outline how the reductions are achieved through the use of Fabric Energy Efficiency performance, energy efficient fittings, and the use of renewable technologies.

The development shall be carried out strictly in accordance with the details so approved and maintained as such thereafter.

Reason: In the interest of sustainable development and to ensure that the Local Planning Authority may be satisfied that CO₂ emission reduction targets are met in accordance with Policy CP20 of the Core Strategy, Policies 5.2, 5.3, 5.7 & 5.9 of the London Plan 2011 and the NPPF.

Energy Performance Certificate

11. Following practical completion of works a final Energy Performance Certificate shall be submitted to and approved in writing by the Local Planning Authority prior to occupation of the development.

Reason: In the interest of sustainable development and to ensure that the Local Planning Authority may be satisfied that CO₂ emission reduction targets are met in

accordance with Policy CP20 of the Core Strategy, Policies 5.2, 5.3, 5.7 & 5.9 of the London Plan 2011 and the NPPF.

No Pipes

12. No pipes or vents (including gas mains and boiler flues) shall be constructed on the external elevations unless they have first been submitted to the Local Planning Authority and approved in writing. Any pipes and vents shall be installed as approved.

Reason: Such works would detract from the appearance of the building and would be detrimental to the visual amenities of the locality.

Travel Plan

13. A Sustainable Travel Plan shall be submitted to and approved in writing by the Local Planning Authority prior to the use hereby permitted commencing. The measures approved in the Travel Plan shall be implemented prior to the use hereby permitted commencing and shall be so maintained for the duration of the use, unless the prior written approval of the Local Planning Authority is obtained to any variation.

Reason: To ensure that the travel arrangements to the development are appropriate and to limit the effects of the increase in travel movements.

16/00684/LBC

3 years

1. The works approved by this Listed Building Consent must be begun not later than the expiration of three years beginning with the date of this decision notice.

Reason: To comply with S.51 of the Planning and Compulsory Purchase Act 2004.

Approved Plans

2. The development hereby permitted shall be carried out in accordance with the approved plans, as set out in the attached schedule which forms part of this notice.

Reason: For the avoidance of doubt and in the interests of proper planning.

Repairs Samples

3. Unless required by any other condition, all new internal works and finishes and works of making good to the retained fabric, shall match the existing adjacent work with regard to the methods used, material, colour, unless otherwise on the drawings or other documentation hereby approved or required by any condition(s) attached to this consent.

Reason: In order to safeguard the architectural and historic interest of this Grade II Listed Building.

The Poplars

to 27



Boundary

DE BOHUN
PRIMARY SCHOOL

Playing Field

KEY

- Proposed New Classroom Unit
- Existing Classroom Unit
- Existing De Bohun School
- 2 Existing Trees Removed

SITE MANAGER'S HOUSE

EXISTING PARKING

GREEN ROAD

ACCESS TO PRE-SCHOOL

ACCESS TO PLAYING FIELDS



Project
De Bohun New Classroom Block

Drawing Title
PROPOSED OVERALL SITE LAYOUT

Scale
1:500@ A3

Date
13.04.18

Drawn

Drawing No.

Revision

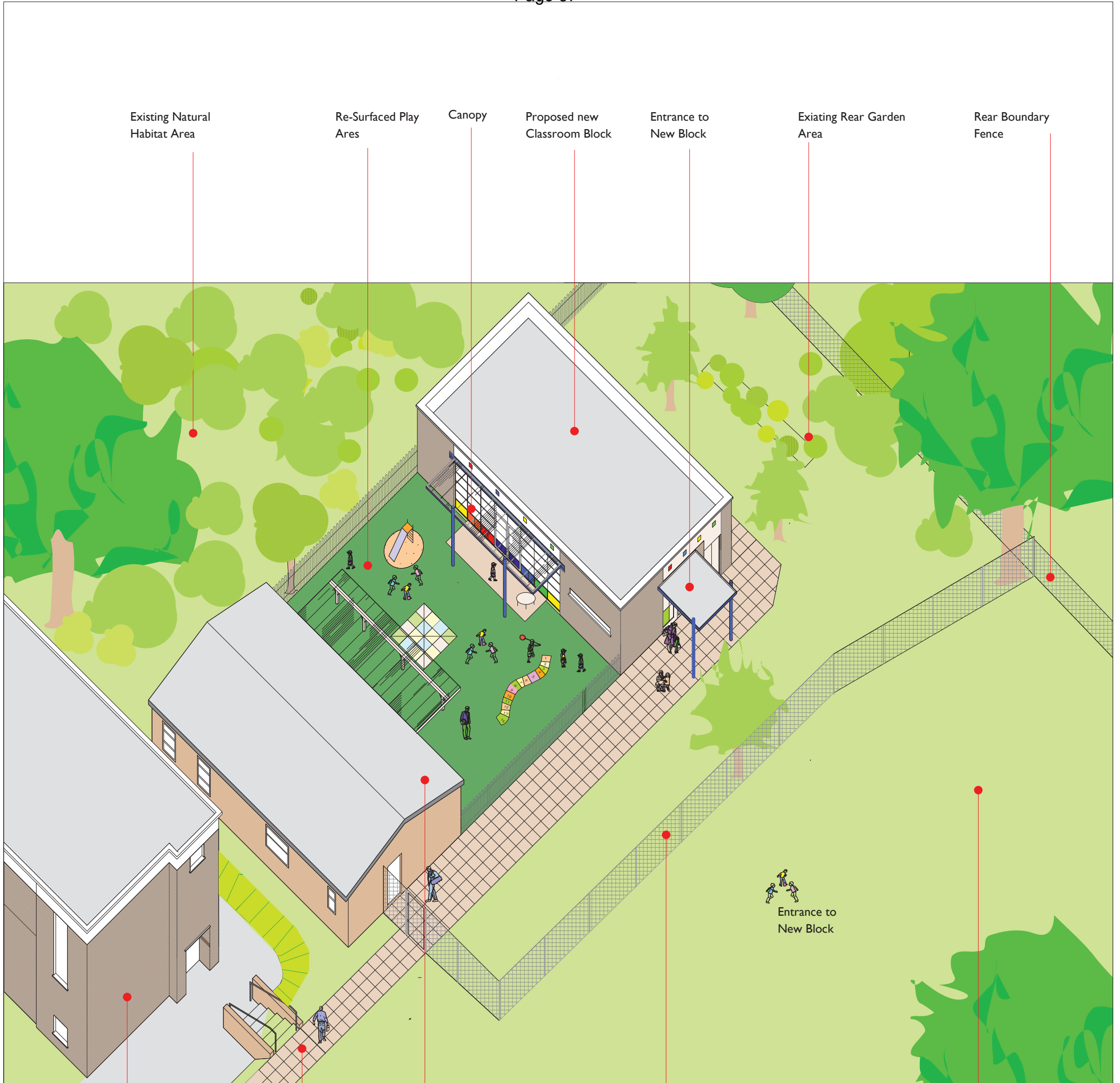
Architect I.S.O.
TN

CAD File No.

D/E222/005209

AL(0)P002

Corporate Maintenance & Construction Team
Civic Centre Enfield EN1 3XA



Existing Natural Habitat Area

Re-Surfaced Play Ares

Canopy

Proposed new Classroom Block

Entrance to New Block

Exiating Rear Garden Area

Rear Boundary Fence

Entrance to New Block

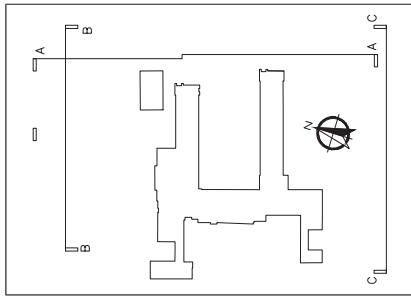
Existing Classrooms Block

Existing Access Path

Existing Prefab Classroom

Existing Chain-link Fencing

Existing Playing Field



KEY PLAN (NTS)

STATUS
Planning

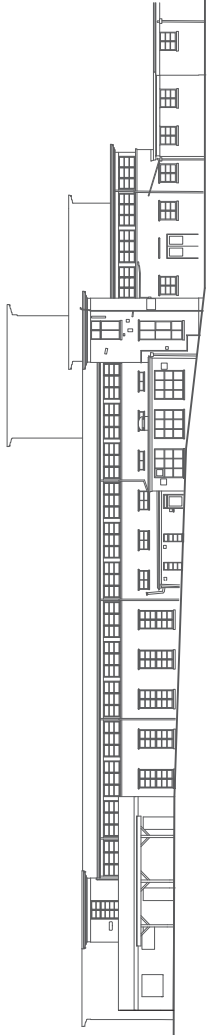
CLIENT
Education

JOB TITLE
De Bohun New
Classroom Block

DRAWING TITLE
EXISTING & PROPOSED
OVERALL ELEVATION &
SECTION B-B



CORPORATE MANAGEMENT & CONSTRUCTION TEAM	DATE	
	DRAWN	T.M.
	CHECKED	
	SCALE	1:250 @ A0
	DRAWING NO.	D/E/222/005209
	REVISION	AL(0)P06



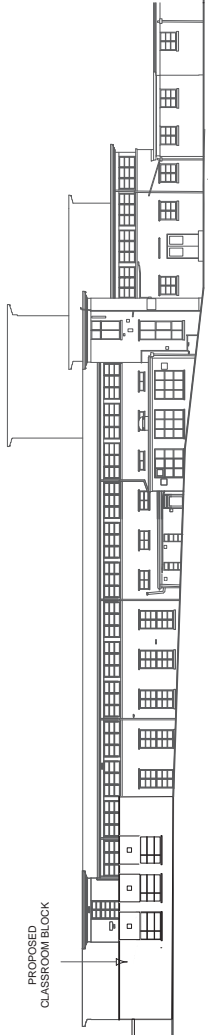
EAST PLAYING FIELD

EXISTING FOOTPATH
CONNECTING
UPPER (NORTH)
PLAYGROUND
TO SOUTH END OF SITE
AND GREEN ROAD

EXISTING UNUSED FENCED OFF AREA
AT NORTH END OF SCHOOL SITE
AND EXISTING MAIN BUILDING BEYOND

EXISTING MOBILE CLASSROOM

EXISTING SECTIONAL SITE ELEVATION D (NORTH)
Scale 1:500 @A2
Scale 1:250 @A0



EAST PLAYING FIELD

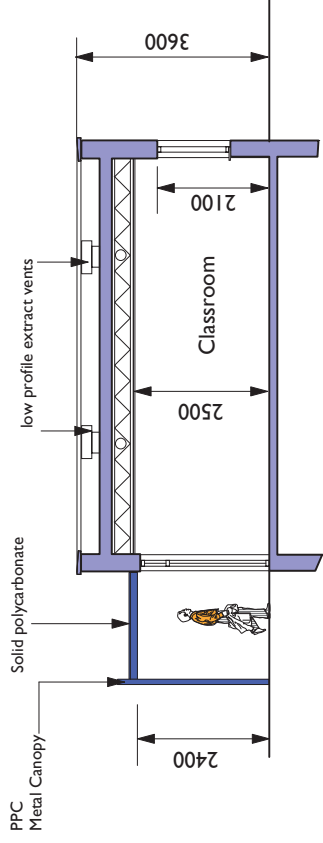
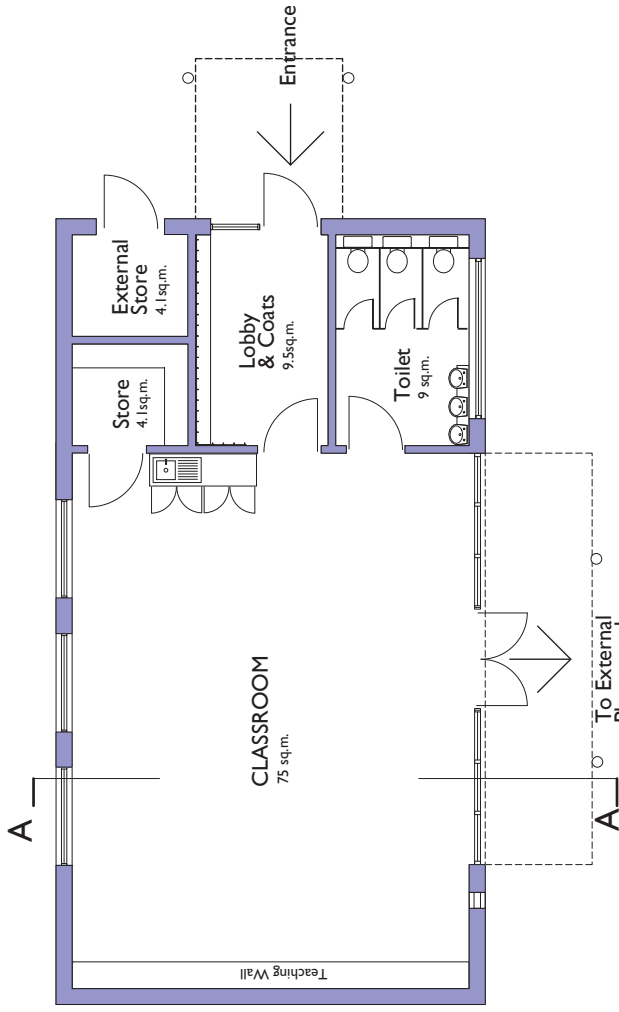
FOOTPATH CONNECTING
UPPER (NORTH)
PLAYGROUND
TO SOUTH END OF SITE
AND GREEN ROAD

EXISTING UNUSED FENCED OFF AREA
AT NORTH END OF SCHOOL SITE
AND EXISTING MAIN BUILDING BEYOND

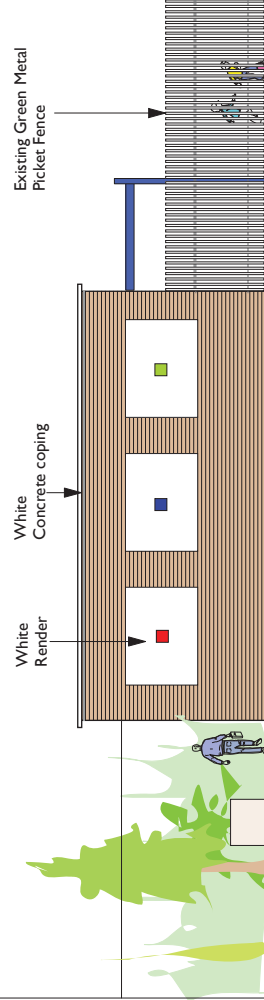
PROPOSED CLASSROOM BLOCK

PROPOSED SECTIONAL SITE ELEVATION D (NORTH)
Scale 1:500 @A2
Scale 1:250 @A0

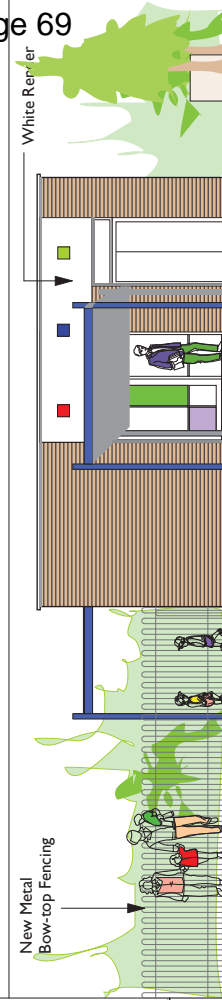




SECTION A - A



WEST ELEVATION

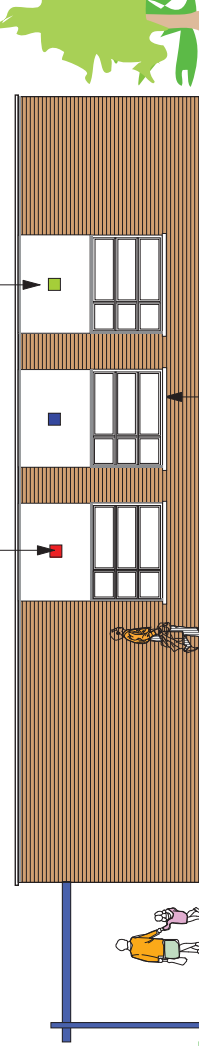


EAST ELEVATION

NOTES - MATERIALS

- 1) Bricks & bonding to match Existing School with selected areas of white render
- 2) White concrete Parapet Coping
- 3) White PPC finish , metal windows & doors
- 4) Colour accents in render, mosaic or tiles in PPC colour Steel & Solid polycarbonate
- 5) Entrance & External Canopies in PPC colour Steel & Solid polycarbonate

SOUTH ELEVATION



NORTH ELEVATION

Revisions



Project
De Bohun New Classroom Block

Drawing Title
PLAN, SECTION & ELEVATIONS

Scale
1:100@A3

Date
17.04.18

Architect I.S.O.
TN

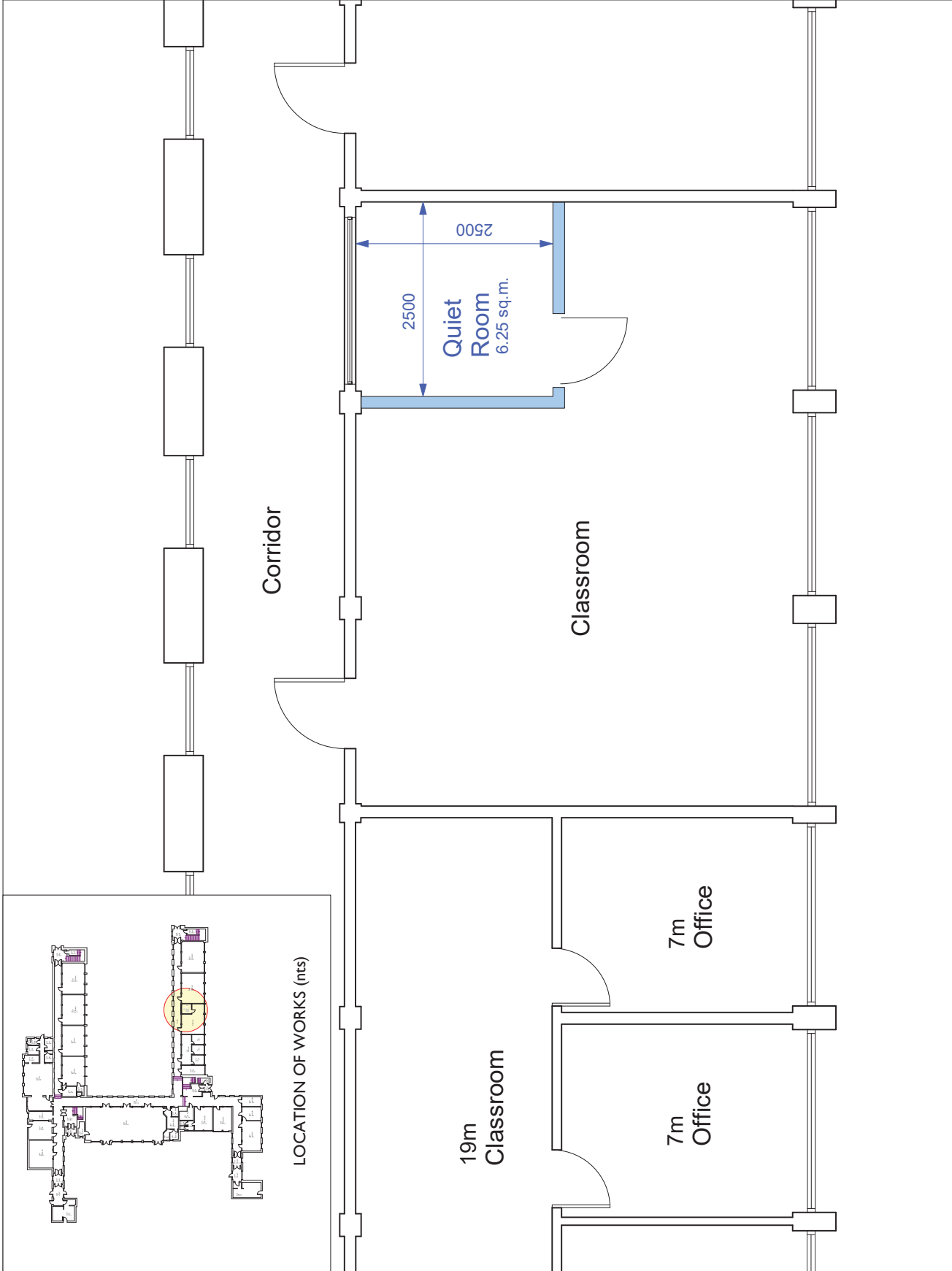
Drawing No.
D/E222/005209

Revision
AL(0)P005

A

SCOPE OF WORKS

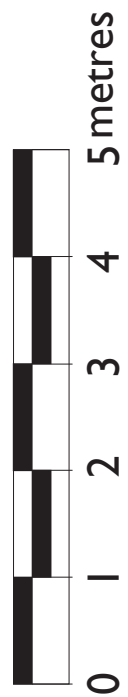
- 1) Corner Quiet Room Added
- 2) Walls of Room constructed with metal studwork and plasterboard from floor to underside of concrete ceiling (note: all works are reversible)
- 3) Outward opening door with vision panel.

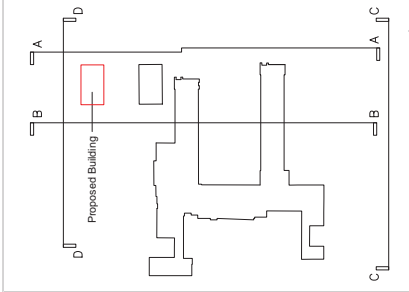


Drawing Title
ADDITION TO EXISTING SCHOOL CLASSROOM

Project
De Bohun New Classroom Block

Revision	
Drawing No.	D/E222/005209
Architect	AL(0)P013
Date	19.04.18
Scale	1:50@ A3
Architect's S.O.	CAD File No. TN





STATUS
Planning

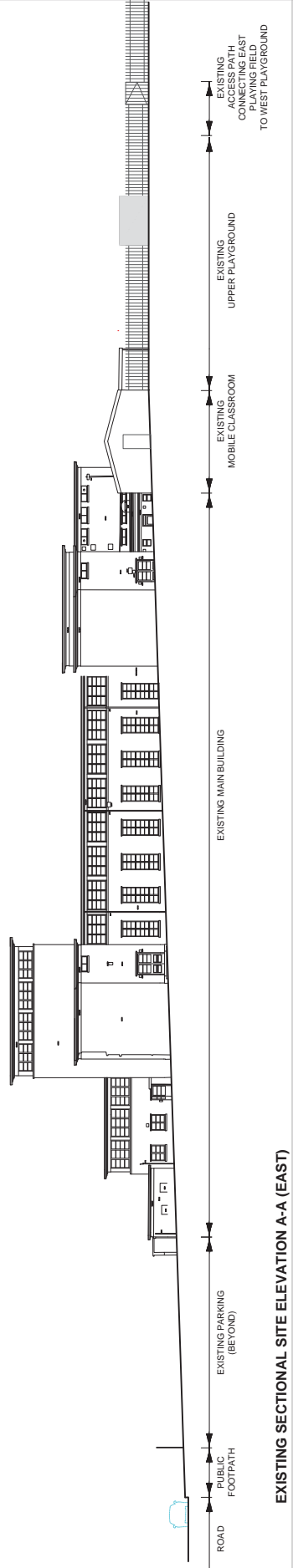
CLIENT
Education

JOB TITLE
De Bohun New Classroom Block

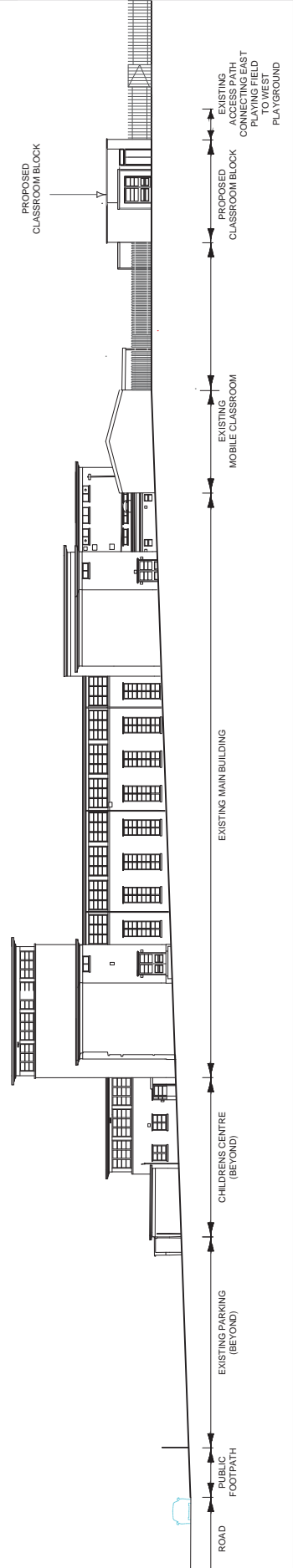
DRAWING TITLE
EXISTING & PROPOSED OVERALL SITE ELEVATION & SECTION AA



CORPORATE TEAM MAINTENANCE CONSTRUCTION		ARCHITECT NO.	DRAWN	DATE
SCALE A0 1:200 @ A2	CHECKED	RIBA WORKSHEET		REVISION
DRAWING NO. D/E222/005209		AL(0)P005		



EXISTING SECTIONAL SITE ELEVATION A-A (EAST)



PROPOSED SECTIONAL SITE ELEVATION A-A (EAST)



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LONDON BOROUGH OF ENFIELD

PLANNING COMMITTEE

Date: 20th November 2018

Report of

Executive Director - Place

Contact Officer:

Carolyn Southall
Andy Higham

Tel No: 0208 379 2592

Ward:

Cockfosters

Ref: 18/03000/RE4 and 18/03001/LBC

Category: RE4 - LBE (Dev by LA) and LBC - Listed Building Consent

LOCATION: De Bohun Primary School, Green Road, New Southgate N14 4AD

PROPOSAL: Construction of single storey classroom building with external canopy, associated fencing, gates, renewal of existing footpaths, play areas and minor internal alterations provide quiet space within classroom of existing main school building.

Applicant Name & Address:

Schools & Children's Services
London Borough of Enfield
Civic Centre
Silver Street
Enfield
EN1 3ES

Agent Name & Address:

Mr T Nadaraju
Enfield Council
Civic Centre
Silver Street
Enfield
EN1 3ES

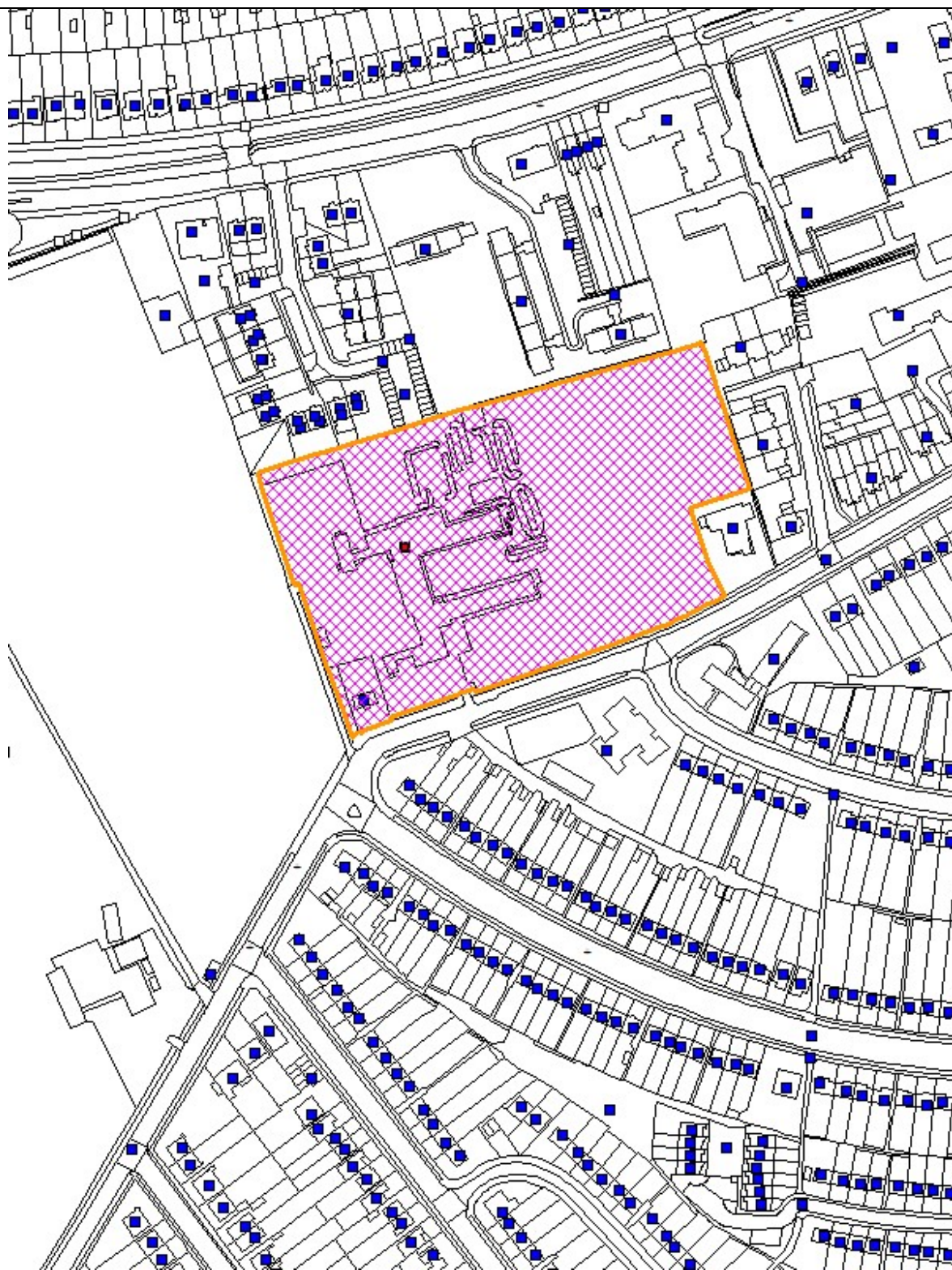
RECOMMENDATION:

18/03000/RE4: In accordance with Regulation 4 of the Town and Country Planning General Regulations 1992, planning permission be deemed to be granted subject to conditions.

18/03001/LBC: That the Head of Development Management/Planning Decisions Manager(s) be authorised to finalise conditions and to grant listed building consent subject to conditions.

Note for Members: A council department is acting as the agent for this application, meaning that, in the interests of transparency, the application has been referred to Committee for decision.

Ref: 18/03001/LBC LOCATION: De Bohun Primary School, Green Road, New Southgate, N14



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Scale 1:1250

North



1. Summary of Main Issues

1.1 The main issues involved in this application are:

The visual impact of the proposed building on the openness, character and appearance of the listed building and surrounding area;

The impact of the proposal upon surrounding residential amenity in terms of visual intrusion, a sense of enclosure, loss of light, privacy, noise and disturbance;

The impact of the development upon the highway network, conditions of highway safety and levels of parking provision.

2. Site and Surroundings

2.1 De Bohun Primary School is situated on the northern side of Green Road. The school, with an overall area of approximately 2 hectares, contains a number of buildings. It is a two-form entry school over seven school years with 60 children in each year group resulting in a school roll of 420 pupils.

2.2 The location of the additional classroom is to the north-west of the school site bounded by garages serving Bramley Close and the communal outside space to The Poplars, a block of three-storey flats. Salcombe Preparatory School, a Grade II Listed building and Homestead Paddock, terraces of two-storey houses, lie to the west of the site separated by the school playing fields. The area to the south of the site, across Green Road, comprise terraces of flats/houses and community buildings.

2.3 De Bohun Primary School is a Grade II Listed building. Constructed in 1936 by W T Curtis and H W Burchett of the Middlesex County Council's Architects Department, listed for its special architectural interest. It forms a cluster of three separately Grade II Listed structures including the school, Salcombe Preparatory School (originally constructed as De Bohun Library, Health Centre and gates) designed by the same architect, and De Bohun Park Clock Tower, all constructed in the mid to late 1930s.

3. Proposed Development

3.1 This application seeks planning permission for the erection of a permanent classroom block with associated facilities to provide additional accommodation at De Bohun Primary School. The proposed accommodation will be sited adjacent to the existing Pre-school accommodation located towards the north of the site.

3.2 The proposed rectangular classroom has a height of 3.6m to the top of the parapet, 14m in length and 7.5m wide. The classroom will have a shallow pitch roof hidden by the parapet. It would provide a classroom of 75m² incorporating classroom space, entrance lobby/coats area, classroom storage, pupil toilets and external storage. The classroom has two canopies at 2.4m high projecting from the building, one over the entrance door the second over double doors towards the external playground. The building will be finished in bricks and bonding to match the existing school, white concrete parapet coping, white polyester powder coating finish to metal windows and door with colour mosaics or tiles placed within a render bands around the building and above the doors and windows.

3.3 The courtyard space created between the existing nursery building and the proposed classroom will provide an external play area for the Nursery and Reception classes and will be finished in a porous macadam.

- 3.4 There are multiple accesses to the site, the main vehicular and pedestrian access located to the front of the main building. Access to the proposed classroom is via an existing, separate, gated, pedestrian access from Green Road taking a northerly direction past the main school building to the north west and the playing field to the north east. Replacement fencing between the existing building and the new classroom will match existing.
- 3.5 A number of trees are located in the vicinity of the proposed classroom and associated works. A liquidamber styraciflua and a prunus subhirtella (classified as Category B) would require felling in order to facilitate the new development.
- 3.6 Internal alterations involve the installation of an enclosed space to a ground floor classroom in the eastern wing of the existing school. The 6.25sqm enclosed space would be used as a quiet room for pupils and would be constructed in metal studwork.

4. Planning History

- 4.1 The current proposal supersedes an earlier, larger, aluminium clad development which was withdrawn at planning stage. Prior to the current submission, the agent on behalf of the Council conducted detailed pre-application discussion with the Councils various Officers over the evolution of the design proposal, which have evolved into the current proposal.
- 4.2 There have been many applications for work to the school – none of which are relevant to the current proposal.

5. Consultation

- 5.1 Neighbours: Application reference **18/03000/RE4** was referred to 57 surrounding properties (21 days expired on 19.09.2018). Press notice expired 05.09.2018. Application reference **18/03001/LBC** was referred to 114 neighbours (21 days expired on 06.09.2018). One objection was received objecting to an increase in parking problems as a result of the new classroom.

Press Notice expired 05.09.2018; Site Notice expired 15.11.2018.

Internal Consultation

- 5.2 Traffic and Transportation: The Traffic and Transportation Briefing Note accompanying the application is based on a previous larger application. It notes that the increase in pupil numbers will have a limited impact compared to the existing arrangements; the main change will be an increase in PM peak trips. There are also a range of mitigation measures proposed including the updating of the existing school travel plan and more active management of the parking issues arising from the school.

Taking this into account, the change in pupil numbers and resultant trips will not be significant in terms of the wider transport network, therefore no objection subject to the following conditions:

- Cycle parking and / or scooter provision of 4 long stay and 2 short stay spaces;
- Pedestrian access particularly the width of the footway in the vicinity of the pre-school access and the width and lighting of on-site paths to the new provision;

- Updated travel plan for the site including details of the proposed mitigation measures; and
- Construction traffic management and safe operation on site.

Given the relative increase in pupil numbers it is acceptable that existing servicing and refuse arrangements will be utilised.

- 5.3 SuDs: Concerns were initially raised however following discussions with the applicant's consultants the issues were resolved and officers raise no objection subject to condition.
- 5.4 Trees: The two trees to be removed are not particularly significant and can be easily replaced via condition of a landscape scheme to included tree planting. Recommend a Tree Protection Plan and Arboricultural Method Statement, in accordance with BS5837:2012, are submitted via condition.

External Consultation

- 5.5 Historic England: No response
- 5.6 Ancient Monuments Society: No response
- 5.7 The Council For British Archaeology: No response
- 5.8 The Georgian Group: No response
- 5.9 The Twentieth Century Society: No response
- 5.10 The Society For The Protection Of Ancient Monuments: No response
- 5.11 The Victorian Society: No response
- 5.12 Conservation Area Advisory Group: Now a much-reduced proposal that overcomes CAG's concerns relating to the original proposal.
- 5.13 Environment Agency: No response

6. Relevant Policy

6.1 Development Management Document

DMD37	Achieving High Quality and Design-Led Development
DMD44	Conserving and Enhancing Heritage Assets
DMD48	Transport Assessments
DMD49	Sustainable Design and Construction Statements
DMD50	Environmental Assessment Methods
DMD51	Energy Efficiency Standards
DMD80	Trees on Development Sites

6.2 Core Strategy

CP8	Education
CP21	Delivering sustainable water supply, drainage and sewerage infrastructure
CP30	Maintaining and improving the quality of the built and open environment
CP31	Built and Landscape Heritage

6.3 **London Plan**

Policy 5.13	Sustainable drainage
Policy 6.9	Cycling
Policy 6.10	Walking
Policy 6.11	Smoothing traffic flow and tackling congestion
Policy 6.13	Parking
Policy 7.4	Local character
Policy 7.6	Architecture
Policy 7.8	Heritage Assets and Archaeology

Other Relevant Policy

6.4 **National Planning Policy Framework (2018)**

The National Planning Policy Framework (NPPF) introduces a presumption in favour of sustainable development. In this respect, sustainable development is identified as having three dimensions - an economic role, a social role and an environmental role. For decision taking, this presumption in favour of sustainable development means:

- Approving development proposals that accord with the development plan without delay; and
- Where the development plan is absent, silent or relevant policies are out of date, granting permission unless:

Any adverse impacts of doing so would significantly and demonstrably outweigh the benefits, when assessed against the policies in the Framework taken as a whole.

6.5 **National Planning Practice Guidance (2018)**

Of particular note for members, paragraph 184 states: Heritage assets range from sites and buildings of local historic value to those of the highest significance, such as World Heritage Sites which are internationally recognised to be of Outstanding Universal Value. These assets are an irreplaceable resource and should be conserved in a manner appropriate to their significance, so that they can be enjoyed for their contribution to the quality of life of existing and future generations.

7. Analysis

7.1 The main issues for consideration regarding this application as follows;

- Principle of the Development;
- Design and Impact on the character and setting of the Grade II Listed building;
- Neighbouring Amenity;
- Traffic and parking issues; and
- Sustainability.

Principle of the Development

- 7.2 De Bohun School comprises a complex of buildings located on Green Road, and is a long-established school, constructed in 1936.
- 7.3 The proposal reflects the increase in local demand for pre-school and nursery places within the borough, and as such seeks to address the lack of space in the main school building. Following a period of pupil number reduction, the school is now in a position that it has a waiting list and has continued to increase the number of pupils and is now, for the first time in at least 6 years, in a situation where there is no space capacity.
- 7.4 The proposed works are vital to ensuring the viable use of this Grade II Listed building, consequently the increase of pupil spaces through the extension of the established school grounds is, in principle, acceptable.

Heritage Considerations

- 7.5 Section 16 of the National Planning Policy Framework (2018) (Conserving and enhancing the historic environment) advises Local Planning Authorities to recognise heritage assets as an “irreplaceable resource and should be conserved in a manner appropriate to their significance, so that they can be enjoyed for their contribution to the quality of life of existing and future generations”.
- 7.6 London Plan policy 7.8 (“Heritage Assets and Archaeology”) advises what boroughs should do at a strategic level to identify, preserve, and enhance London’s heritage assets. Policy CP31 (“Built and Landscape Heritage”) of the Core Strategy sets out a requirement that development should conserve and enhance designated and non-designated heritage assets. Policy DMD44 (“Conserving and Enhancing Heritage Assets”) states that development which fails to conserve and enhance the special interest, significance or setting of a heritage asset will be refused. The design, materials and detailing of development affecting heritage assets or their setting should conserve the asset in a manner appropriate to its significance.
- 7.7 De Bohun School is statutory listed (Grade II) due to its architectural and historic interest. It is set in its own grounds providing uninterrupted views from Green Road and the surrounding area and as a whole, its open plan setting remains intact from the date of construction.
- 7.8 The overall scale and massing of the building would be similar to the existing additional building to which this proposal will be associated. Finished in a mixed palette of brick, render and coloured panels, the applicant has adopted a contemporary approach to the design of the new building. In many cases a pastiche approach would be desirable, however, for this type of building, in this location and for the specific needs of the end user, i.e. the children, it is an acceptable addition and is endorsed by the Conservation Area Advisory Group.
- 7.9 It is inevitable that the building would be visible when viewed from the surrounding area. However, located towards the northern boundary, significantly separated from the northern wing of the original school, and beyond an existing ancillary classroom, the proposed building would conserve the integrity of the main school building and the setting in which it is located.
- 7.10 Also included in the application is the introduction of a “quiet space” located within an existing classroom within the main school building.

- 7.11 Subject to suitable conditions to ensure all new internal works and finishes match the existing adjacent work, the subdivision of the exiting classroom would sustain and enhance the integrity of the listed building.

Impact on Trees

- 7.10 London Plan policy 7.21 considers, existing trees of value should be retained and any loss as the result of development should be replaced. Wherever appropriate, the planting of additional trees should be included in new developments, particularly large-canopied species. Enfield policy DMD 80 also resists the loss of or harm to trees covered by Tree Preservation Orders or trees of significant amenity or biodiversity value. Where there are exceptional circumstances to support the removal of such trees, adequate replacement must be provided.
- 7.11 The planning application was supported by a Tree Survey and Arboricultural Implications Assessment which recognises, in the short term, will have a negative impact on the locality. In order to mitigate the impact, replacement planting is supported in keeping with the existing species and locality.
- 7.12 The Council's Tree Officer has been consulted on the development and is able to support the scheme. He feels the two trees to be removed are not particularly significant and can be easily replaced via a suitable condition for a landscape scheme

Impact on Amenities of adjoining occupiers

- 7.13 The proposed building is located within an existing school complex, approximately 30 metres from the nearest residential building.
- 7.14 Although this proposal has come about following an increase in the overall number of pupils to the nursery and reception classes, the intensification in numbers is considered to be a low increase overall within an existing school environment. Consequently, any impact on the amenity of adjoining residents is de minimis. It should be noted that no objection has been raised by neighbours on this particular issue.

Highway Considerations

- 7.15 The application site has a Public Transport Accessibility Level (PTAL) level of 2 (poor) which indicates that the area is not well connected to public transport services. There is no controlled parking zone (CPZ) in the locality. Orange hazard markings limit parking directly outside the school.
- 7.16 An objection has been received from a local resident objecting to the existing parking pressure in the vicinity of the school.
- 7.17 Following consultation on the proposal, the Council's Transportation Officer has assessed The Traffic and Transportation Briefing Note accompanying the application, and states it is based on a previous larger application. It notes that the increase in pupil numbers will have a limited impact compared to the existing arrangements; the main change will be an increase in PM peak trips. There are also a range of mitigation measures proposed including the updating of the existing school travel plan and more active management of the parking issues arising from the school. Taking this into

account, the change in pupil numbers and resultant trips will not be significant in terms of the wider transport network, therefore no objection is raised subject to conditions.

- 7.18 When visiting the site, the case officer, in discussion with reception teachers, was advised that driving from relatively short distances at drop off/pickup was common place. With no provision for secure on-site scooter/bike (i.e. children's) provision, there is little visible incentive to encourage non-vehicle trips for children and their parents/carers. A condition incorporating suitable provision for the storage of scooters/bikes within close proximity of the nursery/reception classes could alleviate current perceived parking congestion and contribute to a healthier school environment.

Servicing and Refuse

- 7.19 Given the relative increase in pupil numbers it is acceptable that existing servicing and refuse arrangements will be utilised.

Community Infrastructure Levy

- 7.20 The proposal would not seek to create net additional floor space of 100 square metres or more, therefore it would not be liable for the levy.

8. Conclusion

- 8.1 The proposed development would provide a good quality classroom to meet the need of the immediate local community. The scale, design and location of the building would preserve the setting of the listed building and have a negligible impact on residents' amenity on-street parking provision.

9. Conditions

18/03000/RE4

3 Years

1. The development to which this permission relates must be begun not later than the expiration of three years beginning with the date of the decision notice.

Reason: To comply with the provisions of S.51 of the Planning & Compulsory Purchase Act 2004.

Approved Plans

2. The development hereby permitted shall be carried out in accordance with the approved plans, as set out in the attached schedule which forms part of this notice.

Reason: For the avoidance of doubt and in the interests of proper planning.

3 Sustainable Drainage Strategy

Notwithstanding the details set out in the submitted Preliminary Drainage Strategy, prior to the commencement of any construction work, details of the Sustainable

Drainage Strategy shall be submitted to and approved in writing by the Local Planning Authority and must conform with the Landscaping Strategy. The details shall include:

- Sizes, storage volumes, cross-sections, long-sections (where appropriate) and specifications of all the source control SuDS measures including rain gardens, raised planters, green roofs, swale and permeable paving
- Details and specifications of the outfall/ discharge control mechanism
- Management Plan for future maintenance

Reason: To ensure the sustainable management of water, minimise flood risk, minimise discharge of surface water outside of the curtilage of the property and ensure that the drainage system will remain functional throughout the lifetime of the development in accordance with Policy CP28 of the Core Strategy, DMD Policy 61, and Policies 5.12 & 5.13 of the London Plan and the NPPF and to maximise opportunities for sustainable development, improve water quality, biodiversity, local amenity and recreation value.

Samples and Materials

4. Prior to commencement of development above ground, a sample panel and a schedule of materials to be used in all external elevations including walls, doors, windows front entrances and balconies within the development hereby permitted shall be submitted to and approved in writing by the Local Planning Authority before any building work commences and this condition shall apply notwithstanding any indications as to these matters which have been given in the application. The development shall thereafter be carried out solely in accordance with the approved details.

Reason: In order to ensure that the building has an acceptable external appearance and preserves the character and appearance of the conservation area.

Restriction of Use of Roof

5. Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) Order 1995, or any amending Order, no plant or equipment etc. shall be erected on the roof of the building. The roof shall not be used for any recreational purpose and access shall only be for the purposes of the maintenance of the building or means of emergency escape.

Reason: To safeguard the privacy of the occupiers of adjoining properties.

Surfacing Materials

6. Prior to commencement of development above ground, details and design of the surfacing materials to be used within playground and all other hard surfacing shall be submitted to and approved in writing by the Local Planning Authority. The surfacing shall be carried out in accordance with the approved detail before the development is occupied or use commences.

Reason: To ensure that the development does not prejudice highway safety and a satisfactory appearance.

Soft Landscaping

7. Prior to commencement of development above ground, details of trees, shrubs, grass and all other soft landscaped areas of internal and external amenity spaces to be planted on the site shall be submitted to and approved in writing by the Local Planning Authority. The planting scheme shall be carried out in accordance with the approved details in the first planting season after completion or occupation of the development whichever is the sooner. Any planting which dies, becomes severely damaged or diseased within five years of planting shall be replaced with new planting in accordance with the approved details.

Reason: To provide a satisfactory appearance and ensure that the development does not prejudice highway safety.

Replacement Planting

8. That replacement planting shall take place during the first autumn following completion of development, unless otherwise agreed in writing by the Local Planning Authority.

Reason: To secure early replacement planting, and to maintain the Borough's stock of amenity trees.

Tree Protection

9. The work to be undertaken in accordance with good arboricultural practice and British Standard 3998.

Reason: In order to maintain the tree(s) (in the vicinity of the development) amenity value and health.

Energy Statement

10. The development shall not commence until a detailed 'Energy Statement' and relevant SAP calculations has been submitted and approved in writing by the Local Planning Authority. Submitted details will demonstrate the energy efficiency of the development and shall provide a significant reduction in total CO₂ emissions arising from the operation of a development and its services over Part L of Building Regs 2010 in line with Council and London Plan Policy. The Energy Statement should outline how the reductions are achieved through the use of Fabric Energy Efficiency performance, energy efficient fittings, and the use of renewable technologies.

The development shall be carried out strictly in accordance with the details so approved and maintained as such thereafter.

Reason: In the interest of sustainable development and to ensure that the Local Planning Authority may be satisfied that CO₂ emission reduction targets are met in accordance with Policy CP20 of the Core Strategy, Policies 5.2, 5.3, 5.7 & 5.9 of the London Plan 2011 and the NPPF.

Energy Performance Certificate

11. Following practical completion of works a final Energy Performance Certificate shall be submitted to and approved in writing by the Local Planning Authority prior to occupation of the development.

Reason: In the interest of sustainable development and to ensure that the Local Planning Authority may be satisfied that CO₂ emission reduction targets are met in

accordance with Policy CP20 of the Core Strategy, Policies 5.2, 5.3, 5.7 & 5.9 of the London Plan 2011 and the NPPF.

No Pipes

12. No pipes or vents (including gas mains and boiler flues) shall be constructed on the external elevations unless they have first been submitted to the Local Planning Authority and approved in writing. Any pipes and vents shall be installed as approved.

Reason: Such works would detract from the appearance of the building and would be detrimental to the visual amenities of the locality.

Travel Plan

13. A Sustainable Travel Plan shall be submitted to and approved in writing by the Local Planning Authority prior to the use hereby permitted commencing. The measures approved in the Travel Plan shall be implemented prior to the use hereby permitted commencing and shall be so maintained for the duration of the use, unless the prior written approval of the Local Planning Authority is obtained to any variation.

Reason: To ensure that the travel arrangements to the development are appropriate and to limit the effects of the increase in travel movements.

16/00684/LBC

3 years

1. The works approved by this Listed Building Consent must be begun not later than the expiration of three years beginning with the date of this decision notice.

Reason: To comply with S.51 of the Planning and Compulsory Purchase Act 2004.

Approved Plans

2. The development hereby permitted shall be carried out in accordance with the approved plans, as set out in the attached schedule which forms part of this notice.

Reason: For the avoidance of doubt and in the interests of proper planning.

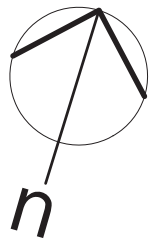
Repairs Samples

3. Unless required by any other condition, all new internal works and finishes and works of making good to the retained fabric, shall match the existing adjacent work with regard to the methods used, material, colour, unless otherwise on the drawings or other documentation hereby approved or required by any condition(s) attached to this consent.

Reason: In order to safeguard the architectural and historic interest of this Grade II Listed Building.

The Poplars

to 27







Boundary

DE BOHUN
PRIMARY SCHOOL

Playing Field

KEY

-  Proposed New Classroom Unit
-  Existing Classroom Unit
-  Existing De Bohun School
-  2 Existing Trees Removed

SITE MANAGER'S HOUSE

EXISTING PARKING

GREEN ROAD

ACCESS TO PRE-SCHOOL

ACCESS TO PLAYING FIELDS



Project
De Bohun New Classroom Block

Drawing Title
PROPOSED OVERALL SITE LAYOUT

Scale
1:500@ A3

Date
13.04.18

Drawn

Drawing No.

Revision

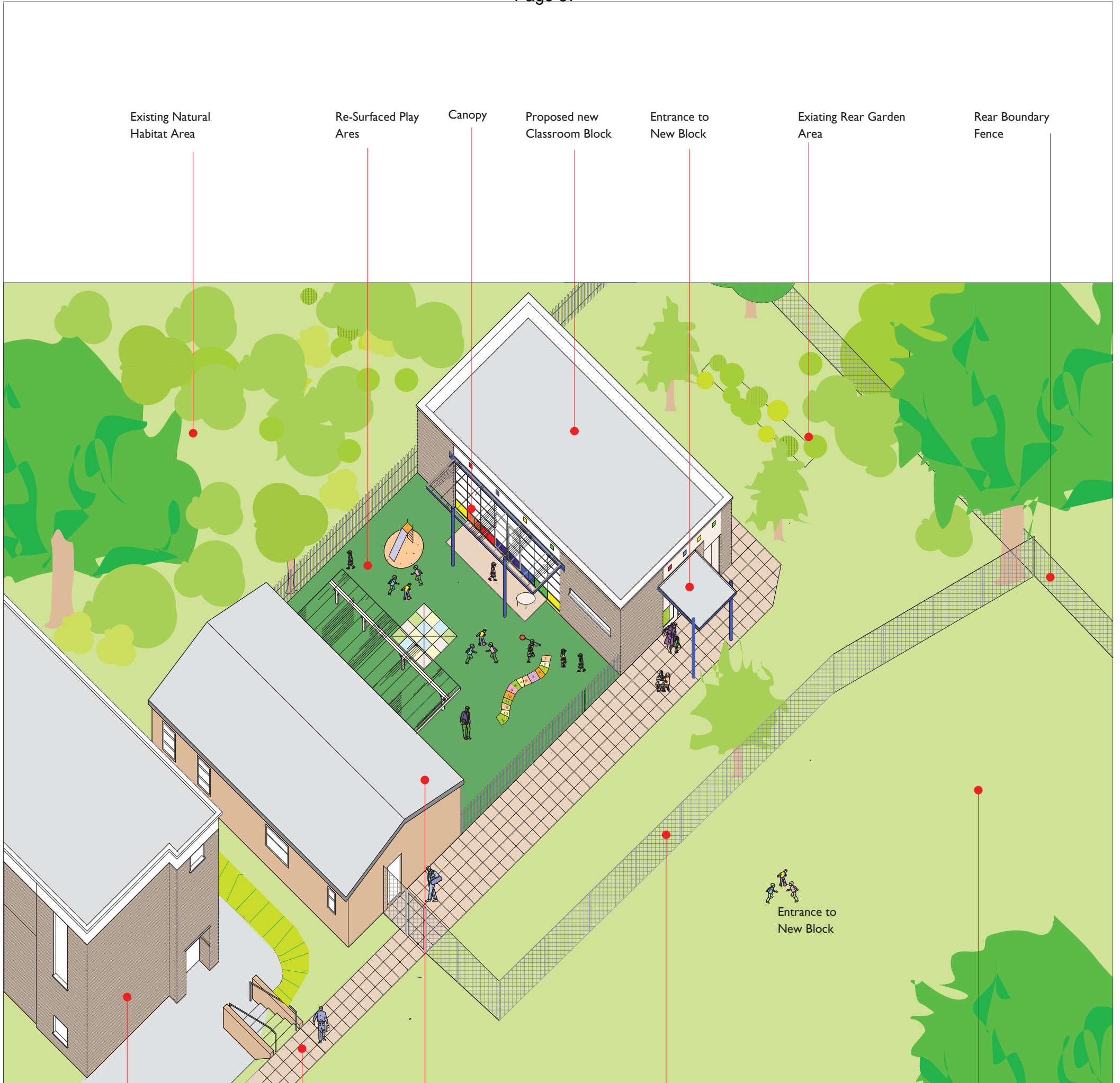
Architect I.S.O.
TN

CAD File No.

D/E222/005209

AL(0)P002

Corporate Maintenance & Construction Team
Civic Centre Enfield EN1 3XA



Existing Natural Habitat Area

Re-Surfaced Play Ares

Canopy

Proposed new Classroom Block

Entrance to New Block

Exiating Rear Garden Area

Rear Boundary Fence

Entrance to New Block

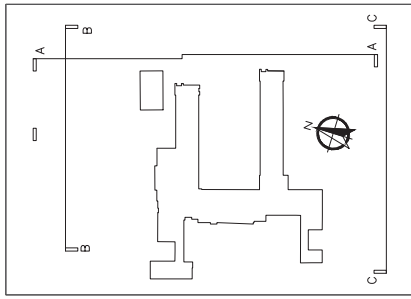
Existing Classrooms Block

Existing Access Path

Existing Prefab Classroom

Existing Chain-link Fencing

Existing Playing Field



KEY PLAN (NTS)

STATUS
Planning

CLIENT
Education

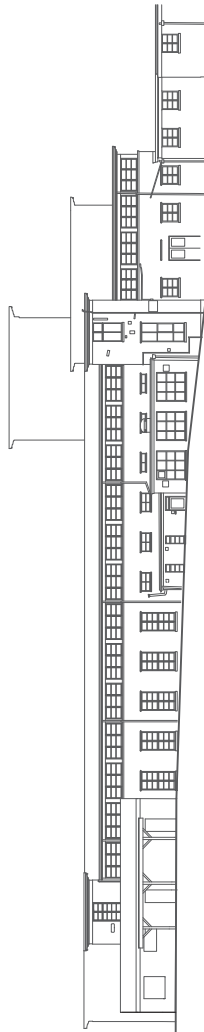
JOB TITLE
De Bohun New
Classroom Block

DRAWING TITLE
EXISTING & PROPOSED
OVERALL ELEVATION &
SECTION B-B



CORPORATE
MANAGEMENT &
CONSTRUCTION
TEAM

ARCHITECT U.S.O.	DRAWN	DATE
	T.M.	
SCALE	CHECKED	RIBA WORKSPACE
1:250 @ A0		
1:500 @ A2		
1:250 DRAWINGS		REVISION
D/E/222/005209	AL(0)P06	



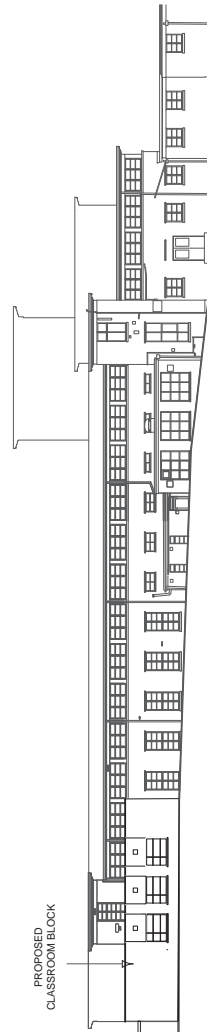
EXISTING UNUSED FENCED OFF AREA
AT NORTH END OF SCHOOL SITE
AND EXISTING MAIN BUILDING BEYOND

EXISTING MOBILE CLASSROOM

EXISTING FOOTPATH
CONNECTING
PLAYGROUND (NORTH)
TO SOUTH END OF SITE
AND GREEN ROAD

EAST PLAYING FIELD

EXISTING SECTIONAL SITE ELEVATION D (NORTH)
Scale 1:500 @A2
Scale 1:250 @A0



EXISTING UNUSED FENCED OFF AREA
AT NORTH END OF SCHOOL SITE
AND EXISTING MAIN BUILDING BEYOND

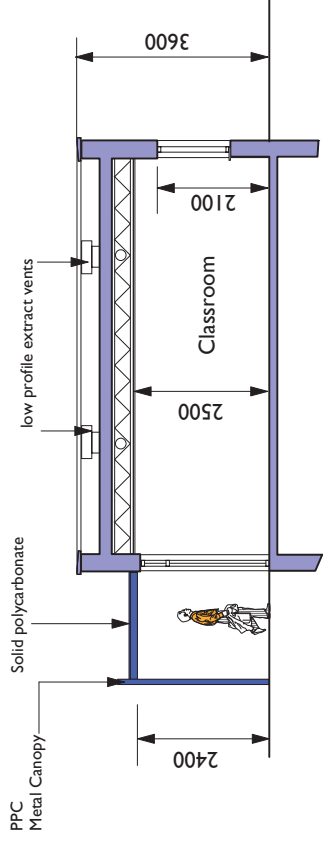
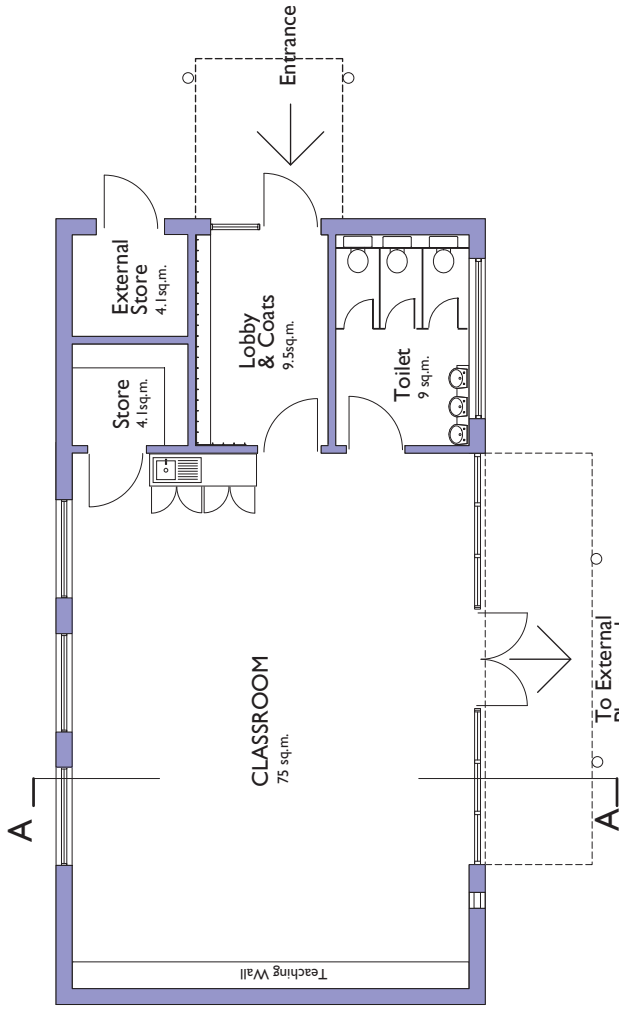
PROPOSED CLASSROOM BLOCK

FOOTPATH CONNECTING
PLAYGROUND (NORTH)
TO SOUTH END OF SITE
AND GREEN ROAD

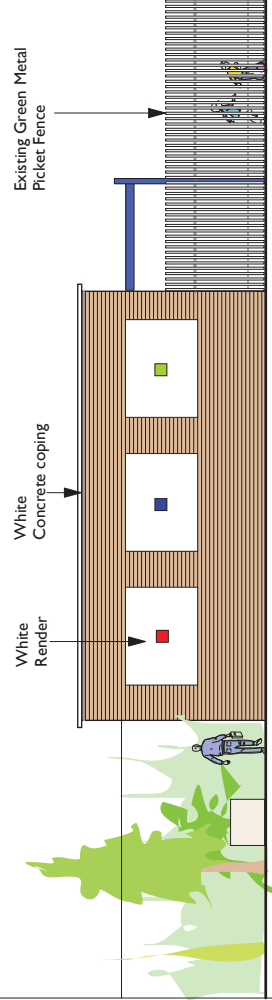
EAST PLAYING FIELD

PROPOSED SECTIONAL SITE ELEVATION D (NORTH)
Scale 1:500 @A2
Scale 1:250 @A0

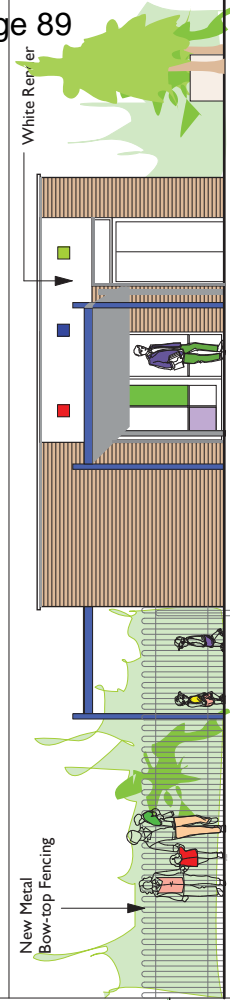




SECTION A - A



WEST ELEVATION



EAST ELEVATION

NOTES - MATERIALS

- 1) Bricks & bonding to match Existing School with selected areas of white render
- 2) White concrete Parapet Coping
- 3) White PPC finish , metal windows & doors
- 4) Colour accents in render, mosaic or tiles in PPC colour Steel & Solid polycarbonate
- 5) Entrance & External Canopies in PPC colour Steel & Solid polycarbonate

PLAN

SOUTH ELEVATION

NORTH ELEVATION

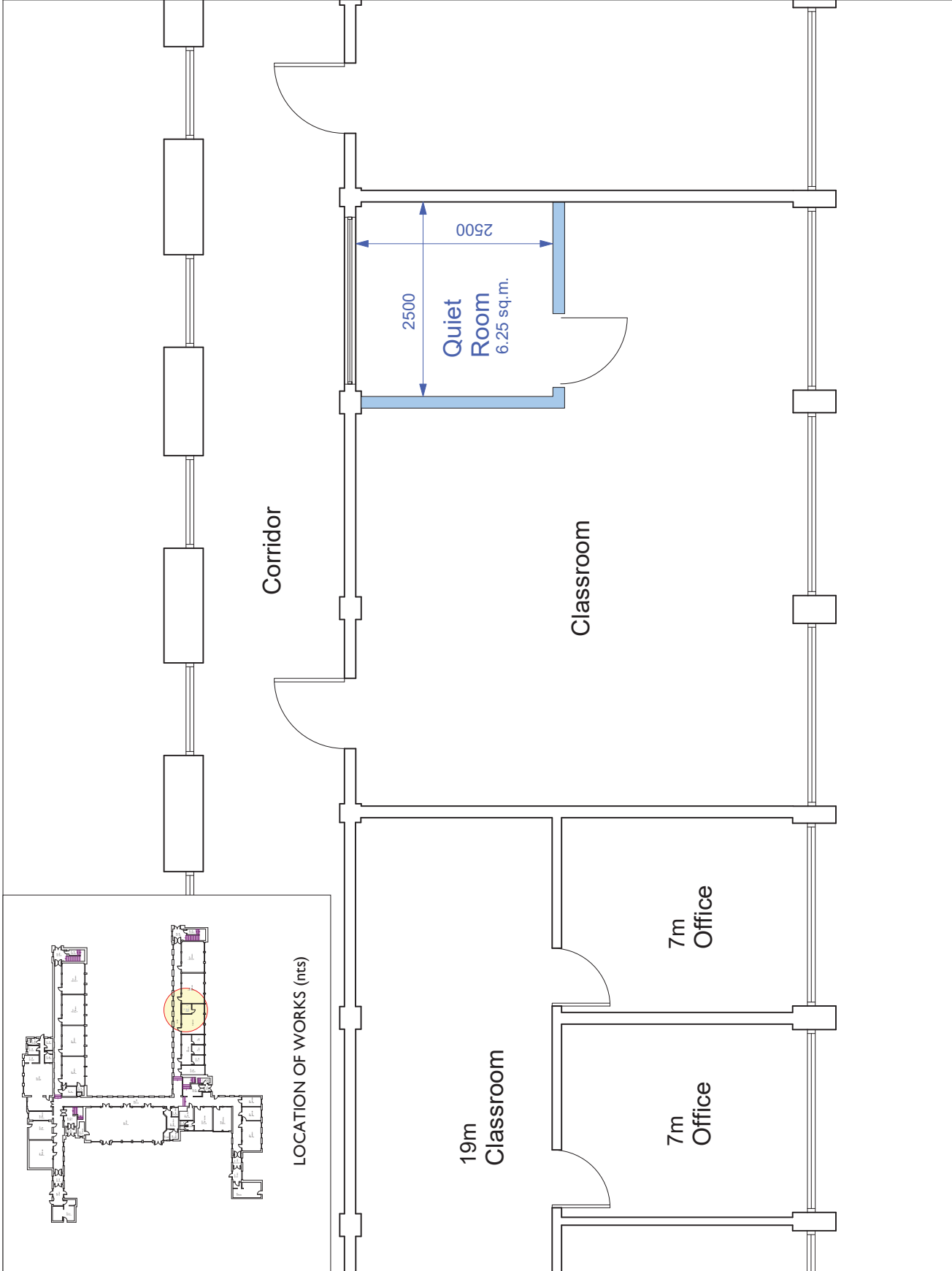
Revisions



Project De Bohun New Classroom Block		Drawing Title PLAN, SECTION & ELEVATIONS	
Scale 1:100@A3	Date 17.04.18	Drawn	Revision A
Architect I.S.O. TN	CAD File No.	Drawing No. D/E222/005209 AL(0)P005	

SCOPE OF WORKS

- 1) Corner Quite Room Added
- 2) Walls of Room constructed with metal studwork and plasterboard from floor to underside of concrete ceiling (note: all works are reversible)
- 3) Outward opening door with vision panel.

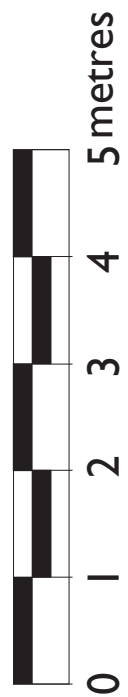


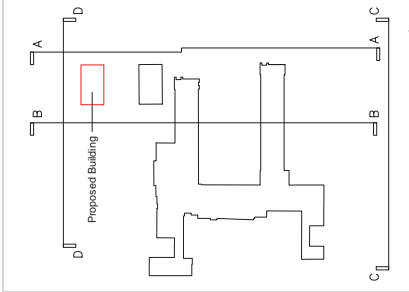
Drawing Title
ADDITION TO EXISTING SCHOOL CLASSROOM

Project
De Bohun New Classroom Block

Revision	
Drawing No.	D/E222/005209
AL(0)P013	

Scale	1:50@ A3	Drawn	
Date	19.04.18	Architect	1 S.O.
CAD File No.	TN		





STATUS
Planning

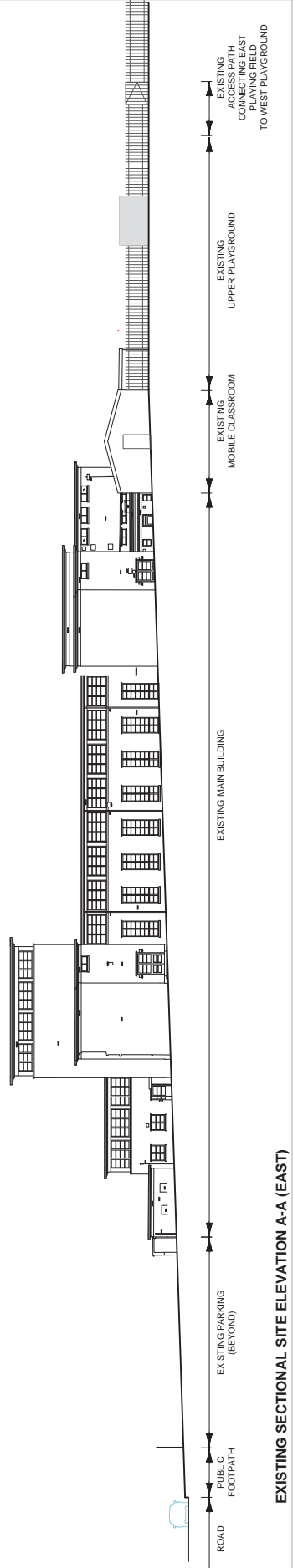
CLIENT
Education

JOB TITLE
De Bohun New Classroom Block

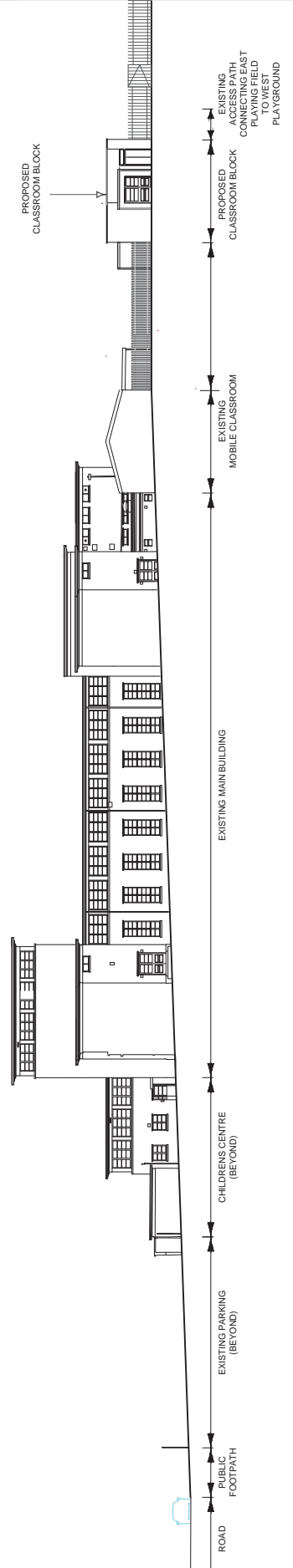
DRAWING TITLE
EXISTING & PROPOSED OVERALL SITE ELEVATION & SECTION AA



CORPORATE TEAM MAINTENANCE CONSTRUCTION		ARCHITECT NO.	DRAWN	DATE
SCALE A0 1:200 @ A2	CHECKED	RIBA WORKSHEET		REVISION
DRAWING NO. D/E222/005209		AL(0)P005		



EXISTING SECTIONAL SITE ELEVATION A-A (EAST)



PROPOSED SECTIONAL SITE ELEVATION A-A (EAST)



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LONDON BOROUGH OF ENFIELD

PLANNING COMMITTEE

Date: 20th November 2018

Report of:

Executive Director - Place

Contact Officer:

Andy Higham

Joe Aggar Tel No: 0208 379 4944

Ward:

Ponders End

Application Number: 18/02514/FUL

Category: Major

LOCATION: Ponders End Industrial Estate, East Duck Lees Lane, Enfield, EN3 7SP

PROPOSAL: Redevelopment of the site to provide a 11,761 sqm Beavertown brewery (B2 use class) involving external equipment, erection of sub station, new access and parking arrangements together with retention of 2 storey building to north of site (Phase 3).

Applicant Name & Address:

c/o Aberdeen Standard Investments

Agent Name & Address:

Planning Potential Ltd.

Magdalen House

148 Tooley Street

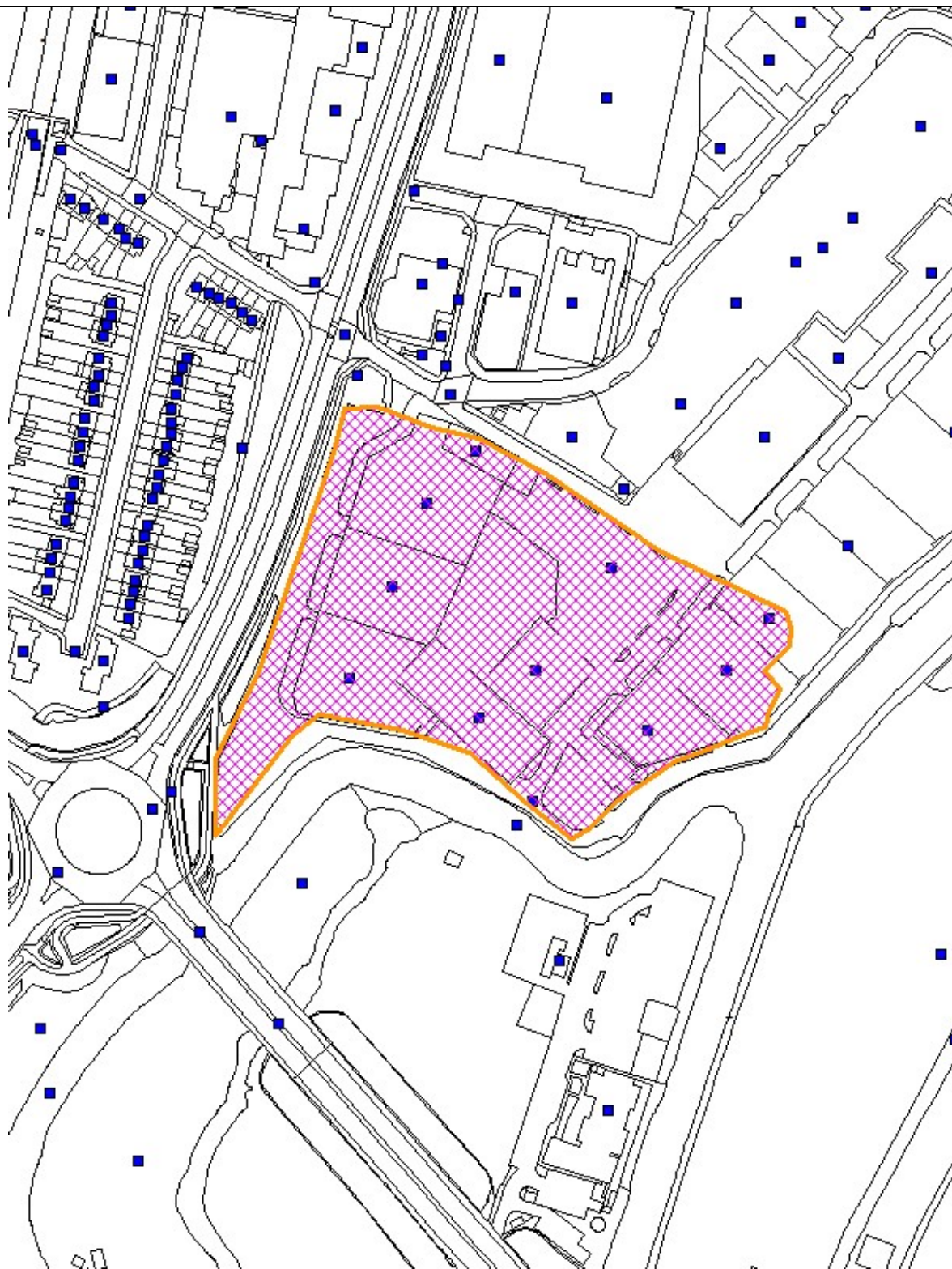
London

SE1 2TU

RECOMMENDATION: That subject to the completion of a S106 Agreement to secure the obligations as set out in the report, the Head of Development Management / the Planning Decisions Manager be authorised to **GRANT** planning permission subject to conditions.

Note for Members: Although an application of this nature would normally be determined under delegated authority, the Conservation Advisory Group has objected to the development due to the effect of the proposal on the locally listed building. In accordance with standard proceed, where officers are minded to still recommend approval, the application is reported to Planning Committee for determination.

Ref: 18/02514/FUL LOCATION: Ponders End Industrial Estate, , East Duck Lees Lane, Enfield



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Scale 1:2500

North



1. Executive Summary

- 1.1 The report seeks approval to a scheme involving the redevelopment of an existing site and construction of a new industrial building for use by Beavertown Brewery who are seeking to relocate into the Borough.
- 1.2 The reasons for recommending approval are:
- i) the proposed use is consistent with the strategic industrial land designation of the site;
 - ii) the proposed development would be consistent with the objectives of national, regional and local policy
 - iii) the development would improve the quality of the Boroughs industrial estate and infrastructure
 - iv) the development would deliver 90 jobs with the potential for an additional 110-140 jobs through future expansion.
 - v) the development would improve the local environment and deliver improved boundary treatment with the Lee Navigation
 - vi) the development would improve the public access to the River Lee Navigation corridor and contribute to the wider accessibility along the western edge.
 - vii) The acceptability of the development in terms of its size, siting, scale and design relative to its appearance in the surrounding area
 - viii) The acceptability of the development in terms of its relationship to the green belt and heritage assets including the locally listed building
 - ix) The building would be highly sustainable and energy efficient, and contribute towards local biodiversity;
 - x) the development in terms of parking, traffic generation and servicing is acceptable
- 1.3 The partial demolition of the locally listed building is acknowledged but taking account of the low historic significance of the building and the fact, a part is to remain, the benefits associated with the development outweigh any harm this would cause.

2. Site and Surroundings

- 2.1 The application site is located to the south of East Duck Lees lane and covers approximately 2.5 hectares. The site lies within the designated 'Strategic Industrial Location' (SIL) and falls within one of the Council's Regeneration areas, as identified within the North-East Enfield Area Action Plan. Within the application site there is a non-designated heritage asset comprising a two storey locally listed building associated with the Ediswan factory built circa 1890. The existing buildings were granted prior approval for demolition under application 18/01890/PADE of industrial units 10, 12, 13 and 14.
- 2.2 To the north of the site there has been substantial redevelopment comprising the demolition of the existing industrial units and erection of replacement industrial units totalling approximately 31,552sqm of floor space for B1, B2 and B8 uses.
- 2.3 The River Lee Navigation adjoins to the east and beyond lies the King George Reservoirs; designated as a Site of Special Scientific Interest (SSSI). The Navigation forms the boundary to the Lee Valley Regional Park to the east, which is also designated as Green Belt. The site is also within the vicinity of

the Lea Valley Special Protection Area (SPA) The site is also located within an Archaeological Interest area and within Flood Zone 1. To the south of the site is the Ponder End Flour Mills Conservation Area.

- 2.4 The application site straddles East Duck Lees Lane and gates are located in the highway at the north-east extent of East Duck Lees Lane.
- 2.5 No parking restrictions are in place on East Duck Lees Lane and Jeffreys Road. Northumberland Road and Alpha Road accommodate a majority of Double Yellow Line (At Any Time) Restrictions. A cycle track runs along the eastern footway of Mollison Avenue. The site is served, via Mollison Avenue, by the bus route no491 with two bus stops sited between the East Duck Lees Lane and Jeffreys Road junctions. The site has a PTAL of 2 which is categorised as poor in terms of public transport accessibility.

3. Proposal

- 3.1. The application seeks the redevelopment of the site to provide an 11,761 sqm building for use by Beavertown brewery (Class B2). The proposal would consist of a large warehouse (12m internal height) with curved roof and cladding to the exterior. Associated with the building but located externally within the proposed yard are proposed various external structures and a substation. The external structures / equipment would be of a maximum height of 19m and would include malt and grain silos plus refrigeration container, chimney and yeast vessels.
- 3.2. It is proposed that delivery / service vehicles will arrive to the northern perimeter of the site with parking is primarily proposed to the eastern part of the site together with cycle parking. New vehicular access is from East Duck Lees Lane.
- 3.3. The proposed building would include an ancillary office and visitors centre at first floor level. Fencing is proposed to the perimeter of the secure yard at 2.4m high (paladin security fencing). The proposal also includes an 8m 'strip' with walkway adjacent to the canal allowing public access through the site.
- 3.4. It is also proposed to partially retain the locally listed building on site (central bay with ground floor facades).

4. Relevant Planning History

- 4.1. 18/01890/PADE - Demolition of units 10, 12, 13, 14 and 15 was GRANTED on 11.06.2018.
- 4.2. 16/01117/RM - Submission of reserved matters approved under Ref: P14-02066PLA in respect of layout, scale, appearance and landscaping pursuant to condition 4 together with details of access, parking and turning, cycle parking, enclosure, refuse storage and cycle/pedestrian routes pursuant to conditions 5,7,8, 9,10 and 30 of outline approval for redevelopment of part of existing industrial estate involving demolition of existing units and erection of replacement industrial units totalling approx. 31,552sqm of floor space for B1, B2 and B8 uses was GRANTED on 11.03.2016.

- 4.3. 17/00003/RM - Submission of reserved matters approved under Ref: P14-02066PLA in respect of layout, scale, appearance and landscaping pursuant to condition 4 together with details of access, parking and turning, cycle parking, enclosure, refuse storage and cycle/pedestrian routes pursuant to conditions 5,7,8, 9,10 and 30 in respect of Phase 1B of the overall outline approval for redevelopment of part of existing industrial estate involving demolition of existing units and erection of replacement industrial units totalling approx. 31,552sqm of floor space for B1, B2 and B8 uses was GRANTED on 14.06.2016.
- 4.4. 17/03101/NMA - Non-material amendment to 17/00003/RM to allow changes to floorspace for B1 and B8 uses together with alterations to windows at ground floor for units 3, 4 and 5 was GRANTED on 03.08.2017.

5. Summary of Key Reasons for Recommendation

- i) The principle of this industrial development is acceptable for this location consistent with adopted policy;
- ii) The development would contribute to employment opportunities for local residents;
- iii) The development is acceptable for this location in terms of its size, siting, scale and design
- iv) The partial retention of the locally listed building addresses heritage policy;
- vii) The development provides an acceptable level of car parking and servicing while also improving access to the River Lea Navigation;
- viii) The development does not impact on neighbouring residential amenity;
- ix) Issues of sustainability, energy and SuDs are controlled by condition.

6. Consultation

Public Response:

- 6.1. Consultation letters were sent to 163 neighbouring properties. Five responses have been received and these are summarised as follows:

Locally listed building

- partial demolition of locally listed building
- greater retention of the former office façade
- any approval subject to conditions information of panel detailing
- The building should be retained
- Information boards should be present noting the history of the site

Transport

- No formal pedestrian crossing from northbound Duck Lees Lane to the site
- An additional bus stop could be placed next to the entrance from East Duck Lees Road
- Any approval subject to S106 improvement in bus stop accessibility
- There should be a walking/cycling bridge to connect the site to Camden Town Brewery and The Navigation Harvester
- Buffer strip for potential walking and cycling

Use

- The visitor centre isn't big enough to deal with the demand generated

Neighbouring Amenity

- Increased level of noise from the operation
- In particular from the outside equipment and arrival and departure of vehicles and goods outside of normal operating hours.

External Consultees:

- 6.2. Thames Water: No objection. Requested condition to be attached requiring technical details of the connection to the existing foul water network infrastructure to accommodate the needs of this development proposal.
- 6.3. MPS Designing Out Crime: No objection. Should the development be approved of a condition requiring the development to confirm attainment of Secured by Design Accreditation, is requested.
- 6.4. Historic England (GLAAS): the proposal is unlikely to have a significant effect on heritage assets of archaeological interest.
- 6.5. Lea Valley Regional Park Authority: any comments received will be reported ahead of the meeting
- 6.6. Environment Agency: No objection subject to planning conditions.
- 6.7. The Canal & Rivers Trust: No objection subject to condition.

Internal Consultees:

- 6.8. Traffic & Transportation: No objection to the proposal, subject to conditions.
- 6.9. Tree Officer: No objection subject to condition relating to tree species in landscape strategy.
- 6.10. SuDS Officer: The SuDS design currently does not meet policy as source control SuDS measures are not utilised for the whole site. As the runoff discharges into the River Lee, which means achieving greenfield runoff rates may not be applicable, there is the opportunity to reduce the size of the tanks and create a gravity drainage system. Discussions continue on this technical detail and it is considered, the developers should capture and treat the first 5mm of rainfall or the 1 in 2-year event, especially as there are opportunities for above ground green infrastructure SuDS by the riverside.

If source control SuDS measures cannot be utilised, this must be supported by a robust reason and a section 106 contribution will be sought in lieu of onsite arrangements.

- 4.11 Heritage Officer: The applicant needs to undertake an options appraisal - looking at alternative layouts that would allow the retention of the existing Locally Listed Building, in full. If this is shown not to be possible, and demolition/ part demolition is the only option, the applicant should make a financial contribution to restoring another of the Borough's heritage assets, in order to outweigh the harm.
- 4.12 Environmental Health Officer: No objection. There is unlikely to be any negative environmental impact as there are no concerns regarding air quality, noise or contaminated land. The acoustic report demonstrates that the noise

generated at the site is unlikely to cause a loss of amenity to the nearest noise sensitive receptors. There is of no contamination that poses a risk to human health in relation to the site end use and potential dust issues during construction have been addressed in the application as well.

Conservation Advisory Group

- 4.13 The Group welcomed Beavertown Brewery to Enfield noting its rapid growth over the recent years. CAG's specific interest lay in the proposal to incorporate a part of the old Ediswan factory in the new development. The factory has a long history of innovation in the electrical field and is at the forefront of Enfield's industrial heritage. The Enfield Society has been instrumental in working with the applicant to retain a portion of the factory and CAG support the Societies efforts. However, the Group felt the proposals retained too small an element of the existing building and as a result, it appeared too small and isolated with insufficient presence. The consequence is that the Group feel it would look oddly out of place in its proposed setting. The Group also feel the proposed usage as a yard office rather offices does not equate with its historical status. CAG urged the applicant to increase the scope of the retained structure and to give it a higher status (main entrance for visitors).

7. Relevant Policies

7.1. London Plan (2016)

Policy 2.7 Outer London: Economy
Policy 4.1 Developing London's Economy
Policy 4.4 Managing industrial land and premises
Policy 5.1 Climate change mitigation
Policy 5.2 Minimising carbon dioxide emissions
Policy 5.3 Sustainable design and construction
Policy 5.5 Decentralised energy networks
Policy 5.6 Decentralised energy in development proposals
Policy 5.7 Renewable energy
Policy 5.8 Innovative energy technologies
Policy 5.9 Overheating and cooling
Policy 5.10 Urban greening
Policy 5.11 Green roofs and development site environs
Policy 5.12 Flood risk management
Policy 5.13 Sustainable drainage
Policy 5.14 Water quality and wastewater infrastructure
Policy 5.15 Water use and supplies
Policy 5.16 Waste net self-sufficiency
Policy 5.18 Construction, excavation and demolition waste
Policy 5.19 Hazardous waste
Policy 5.21 Contaminated land
Policy 6.3 Assessing the effects of development on transport capacity
Policy 6.5 Funding Crossrail and other strategically important infrastructure
Policy 6.9 Cycling
Policy 6.10 Walking
Policy 6.11 Smoothing traffic flow and tackling congestion
Policy 6.12 Road network capacity
Policy 6.13 Parking
Policy 6.14 Freight

Policy 7.2 An inclusive environment
Policy 7.3 Designing out crime
Policy 7.4 Local character
Policy 7.5 Public realm
Policy 7.6 Architecture
Policy 7.8 Heritage assets and archaeology
Policy 7.14 Improving air quality
Policy 7.15 Reducing noise and enhancing soundscapes
Policy 7.16 Green Belt
Policy 7.19 Biodiversity and access to nature
Policy 7.24 Blue ribbon network
Policy 7.26 Increasing the use of the blue-ribbon network for freight transport
Policy 7.27 Blue ribbon network: supporting infrastructure and recreational use
Policy 7.28 Restoration of the blue-ribbon network
Policy 8.2 Planning obligations
Policy 8.3 Community infrastructure levy

7.2. Core Strategy

CP13: Promoting economic prosperity
CP14: Safeguarding strategic industrial locations
CP20: Sustainable energy use and energy infrastructure
CP21: Delivering sustainable water supply, drainage and sewerage infrastructure
CP22: Delivering sustainable waste management
CP24: The road network
CP25: Pedestrians and cyclists
CP26: Public transport
CP29: Flood management infrastructure
CP30: Maintaining and improving the quality of the built and open environment
CP32: Pollution
CP36: Biodiversity
CP40: North East Enfield
CP41: Ponders End
CP46: Infrastructure contributions

7.3. Development Management Document

DMD19 Strategic Industrial Locations
DMD23 New Employment Development
DMD37 Achieving High Quality Design-Led Development
DMD38 Design Process
DMD39 The Design of Business Premises
DMD40 Ground Floor Frontages
DMD41 Advertisements
DMD44 Conserving and Enhancing Heritage Assets
DMD45 Parking Standards
DMD47 New Roads, Access and Servicing
DMD48 Transport Assessments
DMD49 Sustainable Design and Construction Statements
DMD50 Environmental Assessment Methods
DMD51 Energy Efficiency Standards
DMD52 Decentralised Energy Networks

DMD53 Low and Zero Carbon Technology
DMD54 Allowable Solutions
DMD55 Use of Roof Space / Vertical Surfaces
DMD56 Heating and Cooling
DMD57 Responsible Sourcing of Materials
DMD58 Water Efficiency
DMD59 Avoiding and Reducing Flood Risk
DMD60 Assessing Flood Risk
DMD61 Managing Surface Water
DMD62 Managing Surface Water
DMD63 Protection and Improvement of Watercourses and Flood Defences
DMD64 Pollution Control and Assessment
DMD65 Air Quality
DMD66 Land Contamination and Instability
DMD68 Noise
DMD69 Light Pollution
DMD70 Water Quality
DMD75 Waterways
DMD77 Green Chains
DMD78 Nature Conservation
DMD79 Ecological Enhancements
DMD80 Trees on Development Sites
DMD81 Landscaping
DMD83 Development Adjacent to the Green Belt

7.4. Other Relevant Policy Considerations

NPPF
NPPG
LBE S106 SPD
Ponders End Flour Mills Conservation Area Appraisal
Enfield North East Area Action Plan
Enfield Characterisation Study

8. Assessment

8.1. The main issues arising from this proposal for Members to consider are:

Principle of Development
Design and Appearance (setting of Green Belt/ designated and non-designated heritage assets)
Neighbouring Amenity
Transport
Sustainability
Archaeology
Biodiversity & Ecology
Flood Risk/SuDS
Land Contamination
Planning Obligations
Community Infrastructure Levy
Other Matters

Principle of Development

- 8.2. The site's existing buildings are out of date, vacant or under-used, or in the process of being demolished. Together with the poor condition of the ground and boundary treatment, the site detracts from the visual amenity of Duck Lees Lane and the wider area. Redevelopment of the site is considered acceptable in principle, and is welcomed, as it provides an opportunity to bring a relatively large and accessible site back into use and remove the visual and aesthetic harm currently being caused.
- 8.3. In terms of land use, London Plan Policy 2.17 seeks to promote and manage the strategic industrial locations (SILs) designated in Annex 3 and illustrated in Map 2.7, as London's main reservoirs of industrial and related capacity, including general and light industrial uses, logistics, waste management and environmental industries (such as renewable energy generation), utilities, wholesale markets and some transport functions.
- 8.4. The London Plan distinguishes between two types of SIL. The application site is a Preferred Industrial Location (PIL) which are particularly suitable for general industrial, light industrial, storage and distribution, waste management, recycling, some transport related functions, utilities, wholesale markets and other industrial related activities. SILs are given strategic protection to safeguard their existence and because their scale and relatively homogenous character means they can accommodate activities which elsewhere might raise tensions with other land uses. The London Plan also recognises SILs are important in supporting the logistics system and related infrastructure which are essential to London's competitiveness.
- 8.5. The Core Strategy policies are supported by policies in the Development Management Document (DMD). Core Policy 14 states that the Council will safeguard Strategic Industrial Locations. In addition, Policy DMD 19 of the Development Management Document makes it clear that only employment related uses will be permitted in designated employment areas (PIL), and proposals involving the loss of industrial capacity will be refused. DMD23 goes on to state that industrial development will be permitted within Strategic Industrial Locations, subject to no adverse impact arising as a result of noise and disturbance, access, parking and servicing in the area, the scale, bulk and appearance of the development is compatible with the character of its surroundings, on-site servicing and space for waiting goods vehicles is provided to an adequate standard and that the accommodation provided is flexible.
- 8.6. The North East Enfield Area Action Plan (NEEAAP), of which the Brimsdown PIL forms part, also expects the north east part of the borough to deliver a significant number of new jobs over the next 15-20 years. Policy 6.1 of the NEEAAP seeks to improve existing industrial areas within this supplementary planning document, the site is identified falls within the Ponders End Waterfront which is a major opportunity area for employment led, mixed use development.
- 8.7. The proposal would be wholly consistent with this designation. Furthermore, the comprehensive redevelopment would raise the quality of the area and provide modern large floorplate accommodation. The scheme will therefore involve the redevelopment of a prominent industrial site, provide investment in the quality of the Boroughs industrial estate and wider environment as well as creating 90 jobs, potentially rising to 200 jobs.

- 8.8. It is clear therefore that in principle, the redevelopment and regeneration of the site for industrial Class B2 purposes, is in compliance with strategic regional and local policy. As such the proposal is compliant with the aims of London Plan Policy 2.17, Core Strategy Policy 14, Development Management Policies DMD 19 and DMD24 and the North East Enfield Area Action Plan.
- 8.9. The above in-principle position regarding redevelopment of the site accords with the National Planning Policy Framework's presumption in favour of sustainable development.

Design and Appearance

- 8.10. A substantial rectangular building is proposed with an overall floor area 11,761 sqm and 12m clear internal head height. The unit would have a dedicated service yard, featuring entry level loading and door access.



Figure 1: Masterplan (showing the application site relative to recent developments).

- 8.11. The proposed unit comprises a steel-framed, single storey warehouse with external cladding (grey) a curved roof which is sized to suit the operational requirements of distribution / logistics occupiers. The application seeks to provide warehousing with ancillary office accommodation and a visitor centre. The most prominent elevation is that facing the canal and this would incorporate glazing to activate this perspective. Approximately half of the curtilage of the site would be formed of forecourt for manoeuvring and loading, motor/cycle parking and soft landscaping. There would be large external equipment placed adjacent to the building and a substation in association with the proposed use (B2).
- 8.12. The proposed development is industrial in scale, design and language. The proposed building is akin in those respects to the recently completed developments to the north of Duck Lees Lane. The surrounding area is characterised by large-scale industrial units typically of 1 to 2-storeys in height. The proposed building will be of a similar typology to surrounding

developments and will therefore not be out of keeping and character and is considered appropriate in terms of townscape.

- 8.13. Taking into consideration this context, it appears that further redevelopment within the SIL of the height, scale and mass proposed, sharing the same visual and spatial context would be compatible. Overall the proposal is considered a well-designed replacement, potentially acting as a catalyst for further development.

Impact on openness of adjoining Green Belt

- 8.14. The proposal is located in close proximity to adjoining Green Belt and the Lee Valley Regional Park.
- 8.15. Policy DMD83 (“Development Adjacent to the Green Belt”) advises that there should be no increase in the visual dominance and intrusiveness of the built form by way of height, scale and massing. It also advises that development should not restrict future public access/rights of way from being provided and where possible, public access provision should be increased.
- 8.16. The site sits adjacent to Mollison Avenue and Lea Valley Road. The site faces the Kings Georges Reservoir. Lea Valley Road is elevated above open land on either side including the application site. The proposed building’s position therefore sits lower, relative to Lea Valley Road and the reservoir. Consequently, views of the site are largely restricted from views into and out of the Green Belt.
- 8.17. Several buildings are present on the site with a similar existing footprint to that of the proposed. The proposal is to rationalise the space and therefore whilst the building greater in height and a single mass, it is considered the proposal, on balance, is acceptable in terms of its townscape, surrounding character and relationship to the adjoining Green Belt.
- 8.18. DMD83 acknowledges the value of attractive landscapes close to the urban fringe, but equally, the quality of the urban fringe or interface between the Green Belt and the more urban forms varies considerable as is the case in this instance. The rural character adjoins a very urban and industrialised built environment and while the above policy aims to maintain a clear distinction between urban forms and the Green Belt, in this instance given the greening to the southern and eastern end of the site, (on the boundary with the canal), the intrusion of the built form is mitigated by existing and proposed landscaping. This it is considered will maintain an appropriate relationship between the site and the adjacent green belt.

Impact on setting of the Conservation Area

- 8.19. The National Planning Policy Framework (NPPF) recognises conservation areas as designated heritage assets and locally listed buildings as non-designated heritage assets. The site is located to the north of the Ponders End Flour Mills Conservation Area which lies to the south of Lea Valley Road.

- 8.20. Policy 31 of the Core Strategy and DMD44 of the Development Management Policies 2014, accord with the NPPF in seeking to sustain and enhance the significance of heritage assets, which include conservation areas, through development which makes a positive contribution to local character and distinctiveness.
- 8.21. Section 72(1) of the Planning (Listed Buildings and Conservation Areas) Act 1990 sets out the general duty regarding conservation areas in the exercise of planning functions: special attention shall be paid to the desirability of preserving or enhancing the character or appearance of that area. Paragraph 131 of the NPPF sets out matters which should be taken into account when determining planning applications, including sustaining and enhancing the significance of heritage assets (which include conservation area) and the desirability of new development making a positive contribution to the local character and distinctiveness.
- 8.22. The National Planning Policy Framework also makes it clear that great weight should be given to the conservation of designated heritage assets and to their setting.
- 8.23. The A110 (Lea Valley Road) runs east/west to the north of the Conservation Area and due to its elevation, provides an effective visual break between the site and the conservation area. This is recognised by the conservation area appraisal, which emphasises its 'island form'. It is clear therefore that the conservation area is visually and distinctly separated from the application site, notably by the Lea Valley Road.
- 8.24. Given the acceptability of the proposed heights and massing relative to the pattern of warehouse development that exists and improvements secured to improve the canal frontage, it is considered the development would have no impact on the setting of the conservation area.

Impact on the Locally Listed Building

- 8.25. The National Planning Policy Framework (NPPF) recognises locally listed buildings as non-designated heritage assets. The Ediswan building is locally listed. The application seeks the partial demolition of this locally listed building.
- 8.26. The former Ediswan factory is a two storey, general office block circa 1890. The building occupies part of a site associated with Sir Joseph Swan (1828-1914), inventor of the incandescent electric lamp, Sir James Dewar (1842-1923), inventor of the vacuum flask, and Professor Sir John Ambrose Fleming (1849-1945), inventor of the diode radio valve in 1904, which marks the birth of the electronic technological revolution.
- 8.27. The building is of an interesting but unadorned design which befits its former use as a late 19th-century factory general office. Stylistically, it comprises a 2 storey, rendered building of 7 bays (1-3-3-3-3-1) with flat roof and parapet over. The central bay features an exaggerated key stone over a carved inscription which denotes the building's erection in 1890. Later somewhat utilitarian 2 storey mid-20th century extensions can be seen affixed to the flank elevation, which detract from the symmetry of the late 19th-century façade. The building is considered of importance for its historic association with the area, as stated in the Enfield Local Heritage List.

- 8.28. The locally listed building holds a degree of aesthetic and evidential value. However, its historic significance is limited by the more recent internal alterations which have removed any original features and the fact of its association with these former uses: it being a building previously forming part of a wider factory complex.
- 8.29. Revised plans have been submitted which retain the ground floor facades (only) connected to the central tripartite bay. The large removal of fabric to the locally listed building is considered to result in a degree of harm. The NPPF states the effect of an application on the significance of a non-designated heritage assets should be taken into account when determining the application. In weighing applications that directly or indirectly affect non-designated heritage assets, a balanced judgement will be required having regard to the scale of any harm or loss and the significance of the heritage asset.
- 8.30. In this regard, it is noted that the new industrial building with an identified end user would deliver development and substantial beneficial regeneration within a designated industrial area where such uses should be promoted. Moreover, the proposal would deliver 90 jobs, with the potential to uplift to 200 jobs. The development would also improve the accessibility to the canal, connecting a walkway with schemes carried out to the north through to access routes near Mollison Avenue. Furthermore, a contribution of £100,000 has been agreed as part of this scheme (in conjunction with earlier approvals) to deliver a pedestrian footbridge over the canal. This would contribute to the continuations of plans to open up the western towpath improving public access.
- 8.31. While weight is given the locally listed building, its significance due to its recent alterations and its condition together with the fact its designation is largely through association than the importance derived from its actual use, it is considered its historic significance is low. When balanced against the objectives of the land designation and the employment / investment benefits associated with the proposal, the partial demolition and retention of only a visual core, it is considered the development would outweigh the harm identified to the non-designated heritage assets. No objection is therefore raised to the partial demolition of the locally listed building in this instance based on the clear and convincing justification as required by policy.
- 8.32. Where development may affect the significance of an asset, such as a locally listed building, it is noted that the Council may seek contributions. However, the building is considered of locally significance rather than statutorily listed it is considered in this instance, a commuted sum would not be reasonable for the harm to this asset for the maintenance of heritage assets, particularly those 'at Risk' is not considered commensurate or applicable in this instance.

Neighbouring Amenity

- 8.33. London Plan policy 7.6 states that buildings should not cause unacceptable harm to residential amenity, including in terms of privacy and overshadowing. DMD 6 and 8 ensure that residential developments do not prejudice the amenities enjoyed by the occupiers of neighbouring residential properties in terms of privacy, overlooking and general sense of encroachment.

Furthermore, CP30 of the Local Plan seeks to ensure that new developments have appropriate regard to their surroundings, and that they improve the environment in terms of visual and residential amenity.

- 8.34. The nearest residential properties are those located on Duck Lees Lan to the west of Mollison Avenue. The acoustic report demonstrates that the noise generated at the site (operational or otherwise) is unlikely to cause a loss of amenity to the nearest noise sensitive receptors. Consequently, given the distance to the nearest residential properties, the proposed development will have no impact on the amenities of nearby residential occupiers.

Transport Impact

- 8.35. DMD 45 relates to car parking, cycle provision and parking design. DMD 47 states that new development proposals will need to demonstrate that enough space for servicing, circulation and access to, from and through the site is provided. All developments must be fully accessible to pedestrians and cyclists and assist with general permeability within an area and the current development does not provide this. The London Plan policy 6.13, DMD policy 45 (Parking Standards and Layout) and 47 (Access, New Roads and Servicing) states that operational parking for maintenance, servicing and deliveries is required to enable a development to function.

Level of Car Parking provision

- 8.36. The parking provision of 123 spaces, compared to a max provision of 118, is therefore in excess of London Plan standards. However, in mitigation, the site is in a low PTAL area with relatively poor access to public transport. Key to this higher provision is the growth the applicant expects to achieve leading to an increase in employees and the needs of shift workers.
- 8.37. The parking provision will help to avoid parking problems if shift workers for a later shift arrive in advance of shift workers leaving the earlier shift. Further information of shift patterns and staffing details are given in the TA, which states that up to 232 staff will be employed at maximum capacity. It is anticipated that as with the previous site (where 80% are stated to use public transport / walk / cycle), that a similar target can be achieved on the new site.
- 8.38. It is noted that some parking is also required for the visitor centre / brewery tours which further supports the need for a slightly higher parking provision.
- 8.39. A lot of the spaces are located within an overflow area of the car park. It is therefore likely that these spaces will not be routinely used although they do demonstrate extra spaces can be provided without being at the expense of larger service HGVs being able to circulate within the car park.
- 8.40. Further mitigation is provided through the commitment of a Travel Plan (via S106), which would be expected to reduce car use, or as a minimum provide the council with figured on how many staff are driving, cycling etc. This has been informally agreed to be a Heads of Terms in the S106.
- 8.41. Overall the level of parking provision is considered acceptable ad no objection is raised.

Vehicle Parking Layout

- 8.42. The layout of the spaces consists of a car park to the east of the site containing 42 spaces, and more informal parking bays within the main site consisting (81 spaces). The bays in the main car park meet the minimum required dimensions and circulation space is provided. There is a concern with how the bays in the main site will be implemented, given their proximity to the servicing arrangements. However, this is an operational consideration and would not pose any issue to the local highway network. A condition to this effect is included to show the final layout with pedestrian routes.
- 8.43. Layout for the HGVs is supported through the swept path tracking. These show that operational needs can be met, and that it is unlikely vehicles will have to either park or reverse onto the highway.

Vehicular Access & Servicing

- 8.44. There will be an entrance and exit for the HGVs onto East Duck Lees Lane. Swept path confirms they can access and egress safely, and junction protection in the form of DYL (double yellow lines) on the access road ensures this can be maintained. The car park to the east of the site maintains a turning head for any vehicles not accessing the car park, via the gate being set back. Adequate space is available within the site for vehicles to move around and turn.
- 8.45. Overall the accesses do not impact on highway safety and are acceptable. It is not expected that any S278 work will be required to modify the accesses.

Traffic Generation

- 8.46. The estimated level of traffic generated by the existing site is shown in Table 2 (taken from the Transport Assessment).

Time	TRIP RATE (based on per 100sqm)	
	IN	OUT
AM 7-8	34	17
PM 5-6	16	35
DAILY	405	455

Table 2: Estimated trip rate of existing site surveys (from TA)

- 8.47. This is based on surveys from the masterplan redevelopment and was considered robust enough to be used in determining that planning application, therefore it is still considered valid for this one. However, the TA has also used the TRICS database to provide another estimation on traffic (based on selection of multiple sites). These predictions are shown in Table 3.

Time	TRIP RATE (based on per 100sqm)	
	IN	OUT
AM 7-8	47	24
PM 5-6	11	54
DAILY	395	421

Table 3: Estimated trip rate from TRICS database (from TA)

- 8.48. A comparison of the above shows that the predicted overall trips are relatively similar although there are a slightly higher number of trips in the peaks when figures are derived from the TRICS database. The difference however, is not considered significant and it is considered the site will generate a similar volume of traffic to the extant / previous usage and the junction will continue to operate within capacity.

Cycle Parking

- 8.49. The development will provide 40 spaces. This exceeds the number (short and long stay) required by policy and are secure and covered. It is noted the Canals & Rivers would prefer an alternative location away from the edge of the site with the Navigation. There is scope to accommodate this within the site and thus a condition is included to this effect.

Summary

- 8.50. Subject to conditions, the proposal is considered to not generate an unacceptable level of impact on transport or highway network or safety.

Sustainability

- 8.51. The NPPF confirms that the purpose of the planning system is to contribute to the achievement of sustainable development, and policies relevant to sustainability are set out throughout the NPPF. Further planning policies relevant to sustainability are set out in chapter 5 of the London Plan, which states that development proposals should make the fullest contribution to minimising carbon dioxide (CO₂) emissions in accordance with the following energy hierarchy:

- Be Lean: use less energy;
- Be Clean: supply energy efficiently; and
- Be Green: use renewable energy.

- 8.52. Enfield's DMD policy 49 requires the highest sustainable design and construction standards, having regard to technical feasibility and economic viability. These policies require new developments to address the causes and impacts of climate change by minimising energy use, supplying energy efficiently and using energy generated from renewable sources (Core Strategy Policy 20 and DMD51), seeking zero carbon developments (DMD50), using decentralised networks where feasible (DMD52), and providing on-site renewable energy generation to make-up any shortfall where feasible (DMD53).

- 8.53. The development of industrial buildings of this scale is expected to achieve improvements in line with the Building Regulations (Part L). Specifically, DMD Policy 50 requires non-residential development to be BREEAM 'Excellent' rating (73.71%). The applicant has submitted an Energy Statement stating the proposal will be compliant with Building Regulations Part L2A and BREEAM 'Excellent' rating. A condition is recommended to ensure the proposal is compliant in this regard.

- 8.54. The applicants Energy Statement has made a committed to meeting the Building Regulation (2013) carbon reduction. To ensure the energy

performance is in accordance with Building Regulations, a condition is attached.

Decentralised Energy Networks

- 8.55. In line with draft policy DMD52, proposals for major development should contribute to the supply of decentralised energy networks and will be expected to ensure that the site is capable of future connection to a decentralised energy network or provide a contribution towards the expansion of decentralised energy networks, or other carbon reduction measures within the borough.
- 8.56. There is no available Decentralised Energy Network available at present. However, the applicant has identified the potential locations and routes for the buried pipe systems necessary to distribute thermal energy from a future DEN to serve each development plot. The route of the pipe system will be reserved for the future installation of DEN connections into each unit as and when the demand for thermal energy arises. In addition, where CHP or CCHP is technically feasible and financially viable for the development, it will be designed to be able to connect to a future DEN. This is to be secured in the S106 Agreement.

Biodiversity

- 8.57. Through the Enfield Plan, Core Strategy (2010-2025) Policy 36 the Council commits to 'protect, enhance, restore or add to biodiversity interests within the Borough'. This is reaffirmed in the Enfield Development Management Document (2014) policies DMD 78 to 81.
- 8.58. The National Planning Policy Framework (NPPF) recognise that the planning system should aim to conserve and enhance the natural and local environment by minimising impacts on biodiversity and providing net gains in biodiversity where possible, contributing to the Government's commitment to halt the overall decline in biodiversity, including by establishing coherent ecological networks that are more resilient to current and future pressures. Paragraph 175 of the NPPF also states that opportunities to incorporate biodiversity in and around developments should be encouraged.
- 8.59. The application site is situated in a highly urbanised and industrial environment. The site is dominated by mainly disused warehouse units and parking / servicing areas. As a result, the site has little biodiversity or ecological value at present.
- 8.60. An Ecological report was submitted in conjunction with the application which took account of nesting birds, bats and water voles. No harm to these species was identified nor did statutory consultees raise or require conditions as mitigation in these respects.
- 8.61. It is considered there would be a biodiversity enhancement as part of an overall landscaping scheme which is to be conditioned. The proposal allows for the 'greening' to the perimeter of the canal.
- 8.62. In relation to the comments from the Environment Agency it is considered necessary to attach conditions regarding the removal of invasive species. Although a survey and management plan has been provided by the Applicant

a methods statement outlining how this will be put into practice has not yet been provided. Without it, avoidable damage could be caused to the nature conservation value. It is also considered necessary to attach a condition relating to light spill to ensure there is no ecological harm.

Trees

- 8.63. Policy DMD 81 states development must provide high quality landscaping that enhances the environment. There is proposed planting to the walkway perimeter. Planting to the River Lee navigation is considered to enhance the character of the area and the perimeter of the canal. No trees are to be removed and the additional planning will add to the environmental quality.

Sustainable Drainage/ Flood Risk

- 8.64. The NPPF states that inappropriate development in areas at risk of flooding should be avoided by directing development away from areas at highest risk, but where development is necessary, making it safe without increasing flood risk elsewhere (para 100). Core Policy 32 and London Plan Policy 5.21 seeks to address the risks arising from the reuse of brownfield sites to ensure its use does not result in significant harm to human health or the environment.
- 8.65. The site is in Flood Zone 1 (and may be in the 1 in 100 year plus climate change flood extent). The site is also recorded as previously being flooded.
- 8.66. DMD61 requires SuDS measures to be integral to any development and opportunities should be taken to maximise their use. For the proposed development, the developers must achieve Greenfield Runoff rates for 1 in 1 year and 1 in 100 year (plus climate change) events and utilise SuDS in accordance to the London Plan Drainage Hierarchy and the principles of a SuDS Management Train.
- 8.67. The SuDS design currently does not meet our Policy or the London Plan as source control SuDS measures are not utilised for the whole site. As mentioned previously, as the runoff discharges into the River Lee, which means that achieving greenfield runoff rates may not be applicable. Therefore, there is opportunity to reduce the size of the tanks and create a gravity drainage system. It is considered the developers should capture and treat the first 5mm of rainfall or the 1 in 2-year event, especially as there are opportunities for above ground green infrastructure SuDS by the riverside.
- 8.68. It is appropriate to deal with this issue by condition and a condition included. However, if source control SuDS measures cannot be utilised and this can be justified, a section 106 contribution will be sought for use on alternative SuDS schemes.

Contaminated Land and Other Environmental Considerations

- 8.69. The previous use of the proposed development site presents a high risk of contamination that could be mobilised during construction to pollute controlled waters. Controlled waters are particularly sensitive in this location because the proposed development site is within Source Protection Zone 1, and directly located upon Secondary Aquifer As which overly the Principal Chalk Aquifer which is abstracted for public water supply.

- 8.70. The Enfield Plan, Core Strategy (2010-2025) Policy 32 aims to “ensure that water quality will not be compromised.” This is reiterated in the Enfield Development Management Document (2014) DMD 70 which states that “new development that adversely affects water quality, including waterways, indemnified Source Protection Zones (SPZ) or Aquifer will not be permitted ... the developer must consider the risks arising to water quality from development.”
- 8.71. All risks to groundwater and surface waters from contamination need to be identified so that appropriate remedial action can be taken. As such it is considered appropriate to attach a condition to ensure that the development is not put at unacceptable risk from, or adversely affected by, unacceptable levels water pollution and to prevent further deterioration of water quality to a lower status class in the surface water body and prevent the recovery of a drinking water protected area.
- 8.72. It is therefore also considered necessary to attach conditions in relation to a remediation strategy and to require monitoring of foundations, boreholes and remediation in the event unidentified contaminated land is encountered. This is a technical requirement and appropriate to be covered by a condition which is included.

Archaeology

- 8.73. The NPPF (2018) and the London Plan (2016) emphasise that the conservation of archaeological interest is a material consideration in the planning process.
- 8.74. English Heritage’s Greater London Archaeological Advisory Service (GLAAS) has provided archaeological advice to the Council in relation to the information submitted with the application. The relevant officer has not raised concern or need for condition in this regard.

Secure by Design

- 8.75. Following consultation with the MPS Designing out Crime team, the project has the potential to achieve a Secured by Design Commercial 2015 award. The proposal is therefore considered acceptable in this regard and a condition seeking confirmation of the measures is included.

Planning Obligations.

- 8.76. The necessary Heads of Terms are:
- Riverside walk along the western bank of the River Lea for the extent of the application site of a minimum width of 8m from the top edge of the river bank;
 - A financial contribution of £100,000 towards the cost of provision of a new bridge across the River Lea;
 - Employment and skills strategy in accordance with S106 SPD;
 - Submission and adherence to a Travel Plan;
 - demonstrate feasibility for future connection to Decentralised Energy Network; and
 - Sustainable Urban Drainage.

CIL

- 8.77. This would be calculated in accordance with the Mayor's adopted Community Infrastructure Levy Charging Schedule 2012 and Enfield's adopted Community Infrastructure Levy Charging Schedule 2016. The payments would be chargeable on implementation of the commercial development.

7. Conclusion

- 8.78. Redevelopment of the site is welcomed in principle, and the application has been considered with regard to the National Planning Policy Framework (NPPF) and its presumption in favour of sustainable development.
- 8.79. The regeneration of this site for an employment led development is consistent with a number of Council corporate priorities and the prevailing Development Plan policies in the London Plan, Core Strategy and North East Enfield Area Action Plan (NEEAAP).
- 8.80. The development would make a significant and meaningful contribution to the creation of new jobs, and this is a key material planning consideration to be weighed up as part of the assessment of the application.
- 8.81. The proposal is considered acceptable in terms of the strategic industrial land designation and with regard to land use, design, neighbour amenity, transportation and servicing, sustainability and energy, subject to conditions and to an appropriate Section 106 (S106) agreement, the Heads of Terms of which have been agreed with the applicant.
- 8.82. The partial loss of the locally listed building is a consideration which has been weighted against the proposed partial retention and benefits derived from the scheme
- 8.83. In this case, the benefits of the proposed development (as amended) have been given due consideration and balanced against the negative considerations. Having regard also to the mitigation secured by the recommended conditions and Section 106 Agreement, it is considered the proposed development is acceptable when assessed against the suite of relevant planning policies and that planning permission should be granted.

9. Recommendation

- 9.1. That subject to the completion of a s106 Agreement to secure the obligations as set out in the report, the Head of Development Management / the Planning Decisions Manager be authorised to GRANT planning permission subject to the following conditions.

Time Limited Permission

1. The development shall be begun not later than three years from the date of this permission.

Reason: In order to comply with the provisions of Section 92 of the Town and Country Planning Act 1990 (as amended)

Drawing Numbers

2. The development hereby permitted shall be carried out in accordance with the approved plans:

18032 P0001 Revision H
18032 P0002 Revision D
18032 P0003 Revision D
18032 P0004 Revision G
18032 P0005 Revision E
18032 P0006 Revision A
18032 P0007 Revision F
18032 P0008 Revision F
18032 P0009 Revision G
18032 P0011 Revision E
18032 P0014 Revision B
18032 P0015 Revision D
18032 P0016 Revision L
18032 P0017 Revision D
18032 P0018 Revision F
18302 P0021 Revision A
18302 P0022
18032 P0023 Revision B
18032 P0025 Revision A
01 REV E
1879-18-01B.dwg REV B
1302-ESC-00-ZZ-DR-E-2100 Revision T1
Landscape and Biodiversity Enhancement – Technical Note
Beavertown, Construction Environmental Management Plan

JKK10098 – RPS-FIGURE 2 Revision D
Flood Risk Assessment dated June 2018
Archaeological Desk Based Assessment dated May 2018
Energy Report dated 18/06/2018
Air Quality Assessment dated 17/07/2018
Environmental and Geotechnical Site Investigation Report dated July 2018
Noise Assessment dated 02 August 2018
Report on Waterway Wall/Bank dated 22 June 2018
Arboricultural Impact Assessment
Invasive Species Site Assessment and Management Plan 2018
Preliminary Ecological Appraisal
Daytime Bat Survey dated September 2018
Water Vole Survey dated September 2018
Soft Landscape works maintenance and management proposals dated 19 September 2018
Beaver town, Construction Environmental Management Plan
Site waste Management Plan
Sustainability Statement
Transport Assessment
Travel Plan 'Framework'
Planning Statement

Reason: For the avoidance of doubt and in the interests of proper planning.

Materials

3. Prior to the above ground works, commencement of building works above ground, a sample panel and a schedule of materials to be used in all external elevations including walls, doors, windows front entrances and balconies within the development hereby permitted shall be submitted to and approved in writing by the Local Planning Authority before any building work commences and this condition shall apply notwithstanding any indications as to these matters which have been given in the application. The development shall thereafter be carried out solely in accordance with the approved details.

Reason: In order to ensure that the building has an acceptable.

Landscaping

4. Prior to the completion of the external building works, details of trees, shrubs, grass and all other soft landscaped areas of internal and external amenity spaces to be planted on the site shall be submitted to and approved in writing by the Local Planning Authority.

The planting scheme shall be carried out in accordance with the approved details in the first planting season after completion or occupation of the development whichever is the sooner. Any planting which dies, becomes severely damaged or diseased within five years of planting shall be replaced with new planting in accordance with the approved details.

Reason: To provide a satisfactory appearance and ensure that the development does not prejudice highway safety.

Refuse/Waste

5. Prior to occupation of the development, details of refuse storage facilities including facilities for the recycling of waste to be provided within the development, in accordance with the London Borough of Enfield Waste and Recycling Planning Storage Guidance ENV 08/162, shall be submitted and approved in writing by the Local Planning Authority. The facilities shall be provided in accordance with the approved details before the development is occupied or use commences.

Reason: In the interests of amenity and the recycling of waste materials in support of the Boroughs waste reduction targets.

BREEAM

6. Evidence confirming that development approved achieves a BREEAM New Construction rating of no less than 'Excellent' shall be submitted to and approved in writing by the Local Planning Authority. The evidence required shall be provided in the following formats and at the following times:
 - a. a design stage assessment, conducted by an accredited Assessor and supported by relevant BRE interim certificate, shall be submitted at pre-construction stage prior to the commencement of superstructure works on site; and,
 - b. a post construction assessment, conducted by an accredited Assessor and supported by relevant BRE accreditation certificate, shall be

submitted following the practical completion of the development and prior to the first occupation.

The development shall be carried out strictly in accordance with the details so approved, shall be maintained as such thereafter and no change there from shall take place without the prior approval of the Local Planning Authority.

Reason: In the interests of addressing climate change and to secure sustainable development.

Designing Out Crime

7. The development shall achieve a Certificate of Compliance to the relevant Secure by Design Guide or alternatively achieve Crime Prevention Standards to the satisfaction of the Metropolitan Police and the Local Planning Authority. Details of these shall be provided in writing to the Local Planning Authority prior to occupation.

Reason: In the interest of safety and Crime Prevention.

Invasive Species

8. No development is permitted until a detailed method statement for removing or the long-term management of Himalayan Balsam, Japanese Knotweed and Floating Pennywort on the site shall be submitted to and approved in writing by the Local Planning Authority. The method statement shall include measures that will be used to prevent the spread of Himalayan Balsam, Japanese Knotweed and Floating Pennywort during any operations e.g. mowing, strimming or soil movement. It shall also contain measures to ensure that any soils brought to the site are free of the seeds/ root/ stem of any invasive plant listed under the Wildlife and Countryside Act 1981, as amended. Development shall proceed in accordance with the approved method statement.

Reason: to conserve and enhance the natural and local environment by minimising impacts on biodiversity and providing net gains.

Light Spill

9. There shall be no light spill from external artificial lighting into the watercourse or adjacent river corridor habitat. To achieve this, the specifications, location and direction of external artificial lights should be such that the lighting levels within 8 metres of the top of bank of the watercourse are maintained at background levels. The Environment Agency considers background levels to be a Lux level of 0-2.

Reason: To minimise light spill from the new development into the watercourse or adjacent river corridor habitat.

Remediation Strategy

10. No development shall commence until a remediation strategy to deal with the risks associated with contamination of the site has been submitted to, and approved in writing by, the Local Planning Authority.

This strategy will include the following components:

1. A preliminary risk assessment which has identified:
 - a. all previous uses;
 - b. potential contaminants associated with those uses;
 - c. a conceptual model of the site indicating sources, pathways and receptors; and
 - d. potentially unacceptable risks arising from contamination at the site.
2. A site investigation scheme, based on (1) to provide information for a detailed assessment of the risk to all receptors that may be affected, including those off site.
3. The results of the site investigation and the detailed risk assessment referred to in (2) and, based on these, an options appraisal and remediation strategy giving full details of the remediation measures required and how they are to be undertaken.
4. A verification plan providing details of the data that will be collected in order to demonstrate that the works set out in the remediation strategy in (3) are complete and identifying any requirements for longer-term monitoring of pollutant linkages, maintenance and arrangements for contingency action.

Any changes to these components require the written consent of the Local Planning Authority. The scheme shall be implemented as approved. The remediation strategy should be carried out by a competent person in line with paragraph 121 of the NPPF. The Planning Practice Guidance defines a "Competent Person (to prepare site investigation information): A person with a recognised relevant qualification, sufficient experience in dealing with the type(s) of pollution or land instability, and membership of a relevant professional organisation.

Reason: To ensure that the development is not put at unacceptable risk from, or adversely affected by, unacceptable levels water pollution in line with paragraph 170 of the National Planning Policy Framework and to prevent further deterioration of water quality to a lower status class in the surface water body and prevent the recovery of a drinking water protected area.

Verification Report

11. Prior to each phase of development being brought into use a verification report demonstrating the completion of works set out in the approved remediation strategy and the effectiveness of the remediation shall be submitted to, and approved in writing, by the Local Planning Authority. The report shall include results of sampling and monitoring carried out in accordance with the approved verification plan to demonstrate that the site remediation criteria have been met.

Reason: To ensure that the site does not pose any further risk to human health or to the water environment and to prevent further deterioration of water quality to a lower status class in the surface water body and prevent the recovery of a drinking water protected area.

Long Term Monitoring

11. The development hereby permitted may not commence until a monitoring and maintenance plan in respect of contamination, including a timetable of monitoring and submission of reports to the Local Planning Authority, has been submitted to, and approved in writing by the Local Planning Authority. Reports as specified in the approved plan, including details of any necessary contingency action arising from the monitoring, shall be submitted to, and approved in writing by the Local Planning Authority.

Reason: To ensure that the site does not pose any further risk to human health or to the water environment by managing any ongoing contamination issues and completing all necessary long-term remediation measures.

Previously Unidentified Contamination

12. If, during development, contamination not previously identified is found to be present at the site then no further development (unless otherwise agreed in writing with the Local Planning Authority) shall be carried out until a remediation strategy detailing how this contamination will be dealt with has been submitted to and approved in writing by the Local Planning Authority. The remediation strategy shall be implemented as approved.

Reason: To ensure that the development is not put at unacceptable risk from, or adversely affected by, unacceptable levels water pollution from previously unidentified contamination sources at the development site and to prevent further deterioration of water quality to a lower status class in the surface water body and prevent the recovery of a drinking water protected area.

Piling, Deep Foundations, and Boreholes

13. Piling, other deep foundations, and investigation boreholes using penetrative methods shall not be carried out other than with the written consent of the Local Planning Authority. The development shall be carried out in accordance with the approved details.

Reason: To ensure that the proposed piling and foundations do not harm groundwater and to prevent further deterioration of water quality to a lower status class in the surface water body and prevent the recovery of a drinking water protected area.

Boreholes

14. A scheme for managing any borehole installed for the investigation of soils, groundwater or geotechnical purposes shall be submitted to and approved in writing by the Local Planning Authority. The scheme shall provide details of how redundant boreholes are to be decommissioned and how any boreholes that need to be retained, post-development, for monitoring purposes will be secured, protected and inspected. The scheme as approved shall be implemented prior to the occupation of any part of the permitted development.

Reason: To ensure that redundant boreholes are safe and secure, and do not cause groundwater pollution or loss of water supplies.

Construction Environmental Management Plan

15. Prior to the commencement of the development hereby permitted, a Construction Environmental Management Plan shall be submitted to and approved in writing by the Local Planning Authority. The Construction Environmental Management Plan shall include details of:

Proposed surface water arrangements (either via drains or surface water run-off) during the demolition/construction works, and during site occupation. Details should confirm the following:

- i. whether any vehicle washing will be carried out on site and if so, where the waste-water would drain to;
- ii. an emergency pollution control plan which considers what measures could be taken to contain fire-water on-site;
- iii. whether there are any external storage vessels containing substances that would be liquid at atmospheric temperature & pressure;
- iv. that no surface water (either via drains or surface water run-off) or extracted perched water or groundwater is allowed to be discharged into the river during the demolition/construction works i.e. at the point of surface water ingress and at the Lee Navigation outfall;
- v. that any stockpiles of soil from the site are located at a suitable distance away from the river and suitable methods are used to minimise dust emissions from the site during demolition/construction.
- vi. that any surface water drains connecting the site with the river are capped off at both ends for the duration of the demolition & construction works – i.e. at the point of surface water ingress and at the canal outfall.

Reason: To ensure, prior to any works taking place that the proposed construction works do not have any adverse impact on the safety of waterway users and the integrity of the River Lee.

Cycle Storage

16. That development shall not be occupied until details of the siting and design of secure covered cycle parking facilities in accordance with the standards adopted by the Local Planning Authority have been submitted to and approved in writing by the Local Planning Authority. The cycle parking facilities shall be provided in accordance with the approved details prior to occupation of any part of the development within that phase and thereafter permanently maintained for cycle parking.

Reason: To ensure the provision of cycle parking spaces in line with the Council's adopted standards.

Subdivision

17. That no building approved pursuant to this permission shall be occupied by more than one business at any one time and shall not be subdivided unless otherwise agreed in writing by the Local Planning Authority.

Reason: To ensure that the development complies with the adopted parking and servicing standards.

Nesting Boxes

18. Prior to occupation of development details of bird and/or bat nesting boxes/bricks have been submitted to and approved in writing by the Local Planning Authority.

The nesting boxes/bricks shall be provided and the details shall include the exact location, number, specification and design of the habitats.

The development shall be carried out strictly in accordance with the details so approved and maintained as such thereafter.

Reason: To minimise the impact of the development on the ecological value of the area and to ensure the development provides the maximum possible provision towards the creation of habitats and valuable areas for biodiversity.

Sewerage

19. No properties shall be occupied until confirmation has been provided that either: - all wastewater network upgrades required to accommodate the additional flows from the development have been completed; or- a housing and infrastructure phasing plan has been agreed with Thames Water to allow additional properties to be occupied. Where a housing and infrastructure phasing plan is agreed no occupation shall take place other than in accordance with the agreed housing and infrastructure phasing plan.

Reason: The development may lead to sewage flooding and network reinforcement works are anticipated to be necessary to ensure that sufficient capacity is made available to accommodate additional flows anticipated from the new development. Any necessary reinforcement works will be necessary in order to avoid sewer flooding and/or potential pollution incidents.

Number / location / design of electric vehicle charging points

20. Prior to any works commencing in relation to the provision of parking / turning facilities, typical details, including siting and design of plugs, of electric vehicular charging points to be provided in accordance with London Plan standards (minimum 20% of spaces to be provided with electric charging points and a further 20% passive provision for electric vehicles in the future) shall be provided to the Local Planning Authority for approval in writing.

All electric charging points shall be installed in accordance with the approved details prior to first occupation of the relevant phase of the development approved and permanently maintained and retained.

Reason: To ensure that the development complies with the sustainable development policy requirements of the London Plan.

Carbon Reduction

21. The development shall provide for no less than 20% total CO2 emissions arising from the operation of a development and its services reduction over a Part L of Building Regs 2013.

Reason: In the interest of sustainable development and to ensure that the Local Planning Authority may be satisfied that CO2 emission reduction targets are met.

Energy Performance

22. Following practical completion of works, a final Energy Performance Certificate accompanied by an as built energy performance assessment shall be submitted to an approved in writing by the Local Planning Authority. Where applicable, a Display Energy Certificate shall be submitted within 18 months following first occupation of any individual building.

Reason: In the interest of sustainable development and to ensure that the Local Planning Authority may be satisfied that CO2 emission reduction targets are met.

23. No infiltration of surface water drainage into the ground at this site is permitted other than with the written consent of the Local Planning Authority. The development shall be carried out in accordance with the approved details.

Reason: to prevent an unacceptable risk from, or be adversely affected by, unacceptable levels of water pollution.

Surfacing Materials

24. Prior to the completion of the external building works, details and design of the surfacing materials to be used within the development including footpaths, shared surfaces, access roads, parking areas, road markings and all other hard surfacing shall be submitted to and approved in writing by the Local Planning Authority. The surfacing shall be carried out in accordance with the approved detail before the development is occupied or use commences.

Reason: To ensure that the development does not prejudice highway safety and a satisfactory appearance.

Pedestrian Pathways

25. Prior to occupation of the building details of the pedestrian pathways within the curtilage of the site shall be submitted to and approved in writing by the Local Planning Authority

The details shall be carried out in accordance with the approved details and maintained as such.

Reason: To ensure that the development does not prejudice highway safety and a satisfactory appearance.

26. The development shall not commence until a Sustainable Drainage Strategy has been submitted to and approved in writing by the Local Planning Authority. The details shall be based on the disposal of surface water by means of a sustainable drainage system in accordance with the principles as set out in the Technical Guidance to the National Planning Policy Framework and should be in line with our DMD Policy SuDS Requirements:

- a) Shall include source control SuDS measures to capture and treat the first 5mm of rainfall

- b) Follow the SuDS management train and London Plan Drainage Hierarchy by providing a number of treatment phases corresponding to their pollution potential
- c) Should maximise opportunities for sustainable development, improve water quality, biodiversity, local amenity and recreation value
- d) The system must be designed to allow for flows that exceed the design capacity to be stored on site or conveyed off-site with minimum impact. The fluvial and surface water overland flow route on the western boundary should be kept in tact
- e) Clear ownership, management and maintenance arrangements must be established
- f) The details submitted shall include levels, sizing, cross sections and specifications for all drainage features.

Reason: To ensure the sustainable management of water, minimise flood risk, minimise discharge of surface water outside of the curtilage of the property and ensure that the drainage system will remain functional throughout the lifetime of the development.

27. Prior to occupation of the development, a Verification Report demonstrating that the approved drainage / SuDS measures have been fully implemented shall be submitted to the Local Planning Authority for approval in writing. This report must include:

- As built drawings of the sustainable drainage systems including level information (if appropriate)
- Photographs of the completed sustainable drainage systems
- Any relevant certificates from manufacturers/ suppliers of any drainage features
- A confirmation statement of the above signed by a chartered engineer

Reason: To ensure the sustainable management of water, minimise flood risk, minimise discharge of surface water outside of the curtilage of the property and ensure that the drainage system will remain functional throughout the lifetime of the development.

JOB NO: 1879/18

PROJECT: **Enfield Distribution Park, Phase 3**

DATE: 18/10/18

TECHNICAL NOTE

Landscape & Biodiversity Enhancement

Refer to the 'Landscape Proposals' drawing no. 1879/18-01D.

Southern Boundary – River Lee Navigation

Existing Boundary – There is a concrete access road running around the length of this boundary, with parked vehicles and warehousing open to view. A narrow margin of vegetation has established in places along this boundary, where space is available.

Proposed Landscape – The planting proposals along this boundary have been designed to present a continuous riverside landscape to the watercourse and Lee Valley Regional Park.

- The natural soft banks of the river and the existing vegetation they support have been retained within the landscape proposals. A three metre wide foot / cycle path is proposed along the length of this boundary, starting from the access road in the eastern corner of the site. This foot/cycle way will allow access to that section of the river which was previously unobtainable.
- The outer edge of the path is set in from the top of bank by 500mm, so as not to disturb the river edges. Following installation of the path edging, the top of bank area will be cultivated, sown with grass seed and managed to encourage the establishment of species rich grassland margin.
- Between the path and Unit 1 boundary, native shrub planting is proposed, composed of a mixture of species chosen for their appropriateness for this riverside location. This mix will either be planted on a one metre grid, or as a double staggered row, depending on location.
- Within the shrub mix there will be planted native tree species, located to co-ordinate with the existing riverside trees and proposed building.
- Within the Unit 1 plot and in front of the visitor centre/offices, low growing ornamental shrubs have been proposed to create an attractive setting for visitors and staff.
- Bird nesting boxes and Bat boxes will be installed in existing mature trees and on the proposed building as part of the landscape scheme, to provide additional opportunities for these species.
- The existing invasive species which have been identified on the site will be removed before construction begins, allowing valuable native trees and shrubs to be planted instead.

Northern Boundary – Access Road

Existing Boundary – The access road leading from East Duck Lees Lane down to the River Lee is currently bordered by paved footways, car parking and buildings, with no provision for landscape.

Proposed Landscape – The planting proposals along this boundary have been designed to provide a green margin between the Unit 1 plot and the highway kerb.

- Low growing ornamental shrub species are proposed around the substation to help soften the structure and within the car park to provide an attractive setting.
- Along the frontage with East Duck Lees Lane and the eastern end of the service yard, there is proposed a clipped Beech hedgerow; a species which is native to the UK and therefore valuable to wildlife.
- Sections of this boundary also have a mown grass verge between the service yard fence and access road.
- At the junction of East Duck Lees Lane and the access road, there is proposed a small swale, which will be sown with grass seed and managed as species rich grassland.
- Five native trees are proposed along this boundary to provide some screening of the eastern end of the service yard and provide a landscape feature in the car park.

Western Boundary – Mollison Avenue

Existing Boundary – Along this boundary with the highway there is a linear group of vegetation ranging in height from 10 to 12 metres.

Proposed Landscape – This linear group of trees has been surveyed as part of the Arboricultural Impact Assessment (produced by RPS), with the aim of retaining the existing mature vegetation within the proposed scheme.

- In the south-western corner of the service yard there is proposed small swale as part of the drainage strategy. This feature will be sown with grass seed and managed as species rich grassland.

Landscape Maintenance

Refer to the separate document '*Soft Landscape Works Maintenance & Management Proposals*' reference no. 1879/18-RP01B, for details of on-going maintenance. This document contains the aims and objections of the future maintenance and management, plus a schedule of works for year one and a table of maintenance operations for the initial five year period.

Dimensions are in millimeters, unless stated otherwise.
 Scaling of this drawing is not recommended.
 It is the recipient's responsibility to print this document to the correct scale.
 All relevant drawings and specifications should be read in conjunction with this drawing.



Location Plan - Scale 1:2500

SCHEDULE OF ACCOMMODATION

Proposed Unit	
Warehouse Area	- 108,000 sq ft (10,033m ²)
GF Office Core	- 600 sq ft (56m ²)
First Floor Office	- 6,000 sq ft (557m ²)
GF Staff Welfare + Core	- 6,000 sq ft (557m ²)
First Floor Visitor Centre	- 6,000 sq ft (557m ²)
First Floor Bridge Link	- 3,070 sq ft (285m ²)
Total Area	- 129,670 sq ft (12,045m²)
Level Access	- 6
Car Parking	- 42
Motorcycle Parking	- 2
Plot Area	- 6.82 Acres (2.88 Hectares)
Site Coverage	- 44.9%



Site Plan
Scale 1:500



Enfield Distribution Park,
Phase 3
Site Plan

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Drawing Status:	Flawing
Drawn / Checked:	HJ / JBR
Date:	07/02/2018
Scale:	1:500 A1
Drawing no.:	Revision:
18032 P001	H



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Masterplan
 Scale 1:1000



rev: amendments by: dtd: date:
**Enfield Distribution Park,
 Phase 3
 Masterplan**

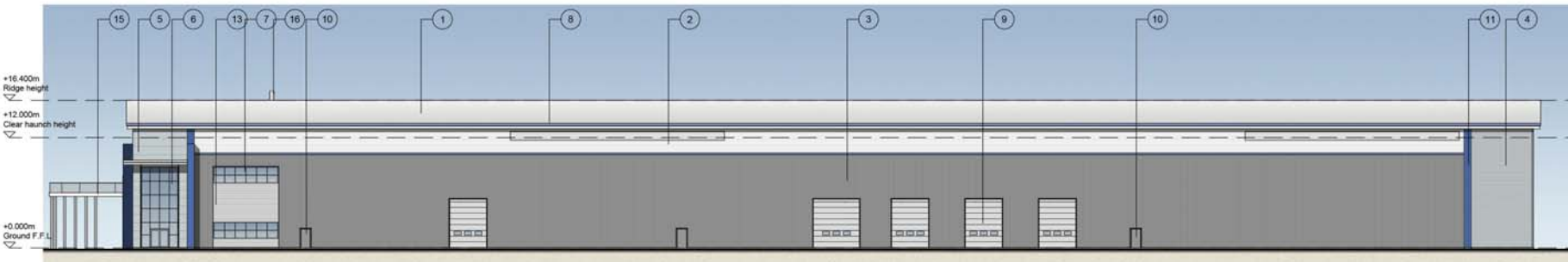
Network Services Innovation Centre, California Way, Newark, Nottinghamshire NG24 2JH
 +44 (0)1509 453027 +44 (0)1509 453010 info@umcarchitects.com

Drawing Status:	Flavouring
Drawn / Checked:	HI / J3M
Date:	07/03/2018
Scale:	1:1000 AI
Drawing no:	Revision:
18032 P0004	G

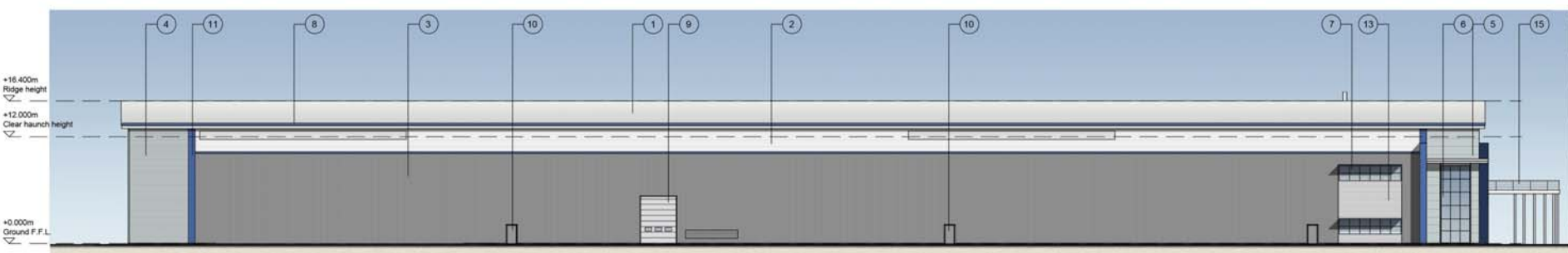
PLANNING

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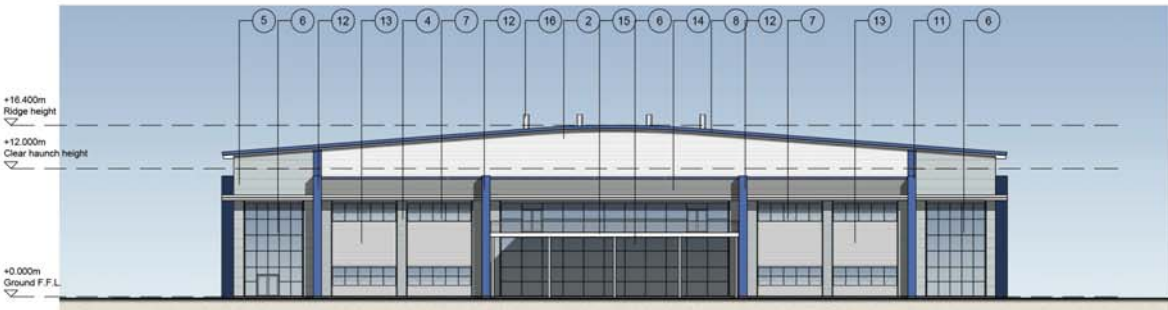
- KEY:**
- 1 Colour-coated profiled roof cladding.
Finish: Colorcoat HPS200 Ultra 7038 Goosewing Grey
 - 2 Colour-coated profiled metal cladding laid horizontally.
Finish: Colorcoat HPS200 Ultra RAL 9010 White
 - 3 Colour-coated profiled metal cladding laid vertically.
Finish: Colorcoat Prisma RAL 7012 Slate Grey
 - 4 Colour-coated profiled metal cladding laid horizontally.
Finish: Colorcoat HPS200 Ultra Albatross
 - 5 Colour-coated metal Microrib cladding laid horizontally.
Finish: Colorcoat Prisma Albatross
 - 6 Schuco or similar polyester powder coated aluminium curtain walling with sealed unit, clear, toughened double glazing.
Finish: Frames to be RAL 7012 Slate Grey
 - 7 Polyester powder coated aluminium windows with sealed unit, clear, toughened double glazing.
Brise soleil to ground and first floor windows on South West elevation
Finish: Frames to be RAL 7012 Slate Grey
 - 8 Polyester powder coated aluminium trims and flashings
Finish: Colorcoat HPS200 Ultra Sargasso (top colour)
Finish: Colorcoat Prisma White (bottom colour)
 - 9 Composite roller shutter doors with micro rib profile
Finish: RAL 9006 Sirius
 - 10 Personnel Doors
Finish: Colour to match adjacent cladding
 - 11 Colour-coated profiled metal cladding laid horizontally.
Finish: Colorcoat HPS200 Ultra Sargasso
 - 12 Steel Flashing
Finish: Colorcoat HPS200 Ultra Sargasso
 - 13 Colour-coated profiled metal cladding laid horizontally.
Finish: Colorcoat Prisma RAL 9006 Sirius
 - 14 Colour-coated metal Microrib cladding laid horizontally.
Finish: Colorcoat Prisma RAL 7012 Slate Grey
 - 15 Balcony at first floor (+6.0m) with ucrete floor and perimeter balustrade
 - 16 4no. 500mm Ø chimneys penetrating roof up 1000mm above ridge level



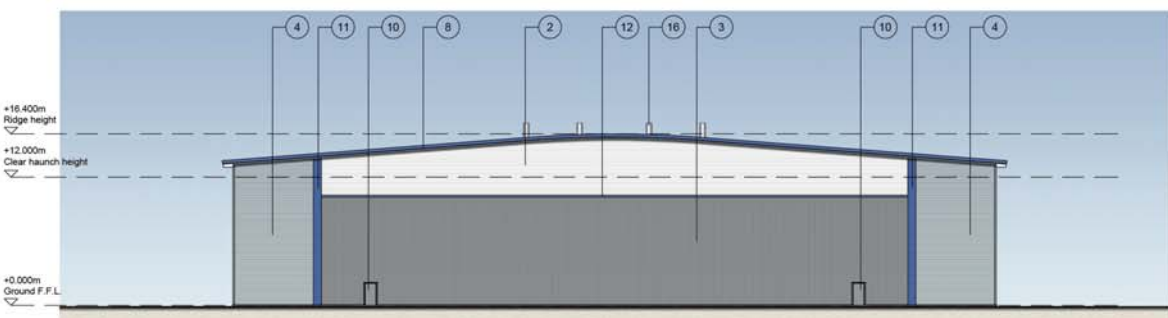
North East Elevation - Scale 1:250



South West Elevation - Scale 1:250



South East Elevation - Scale 1:250



North West Elevation - Scale 1:250

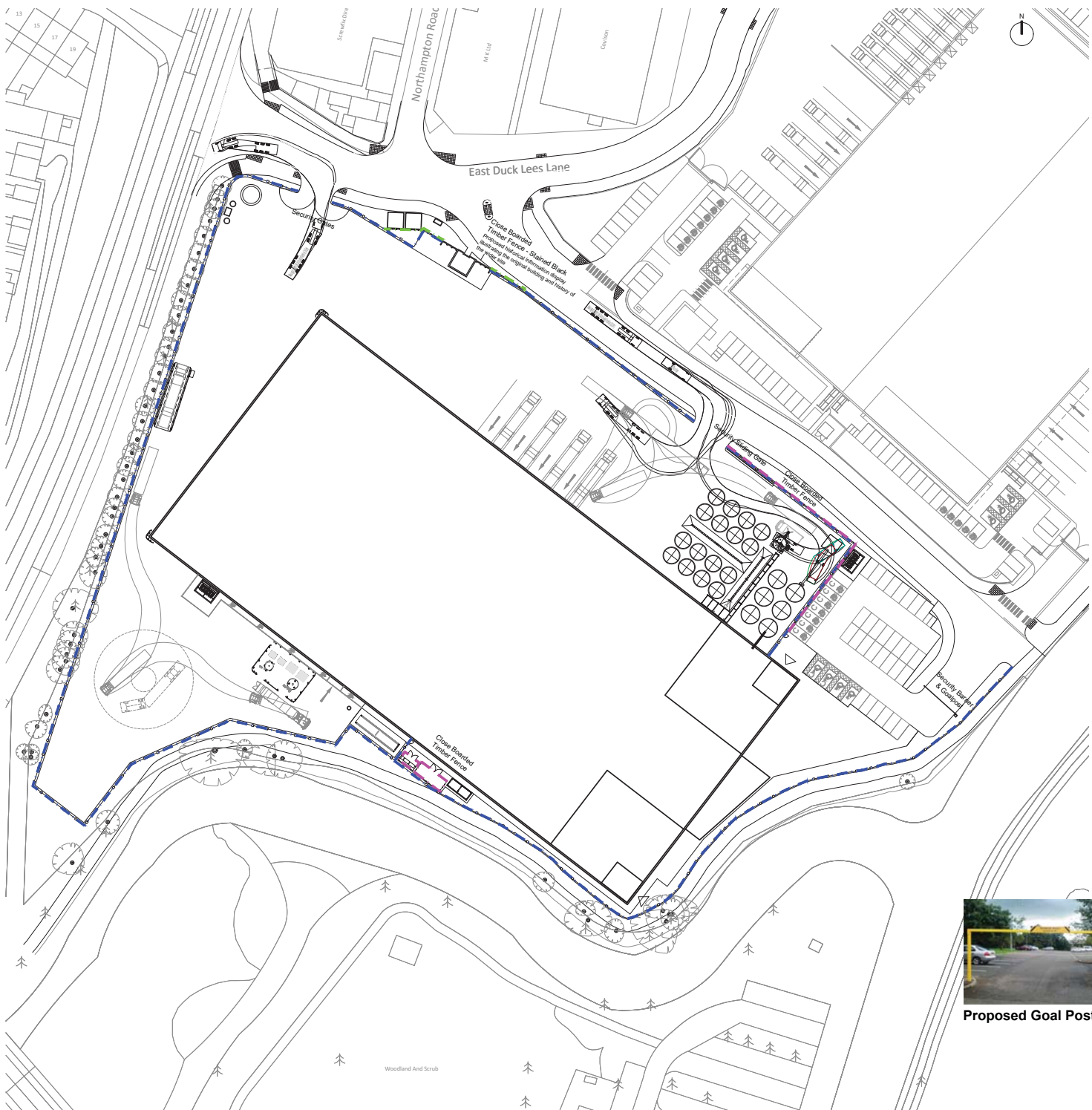
Page 128



rev amendments by dtd date
Enfield Distribution Park, Phase 3
 Elevations

Drawing Status	Planning
Drawn / Checked	HJ / JBN
Date	21/03/2018
Scale	1:250 A1
Drawing no	Revision
18032 P0005	E

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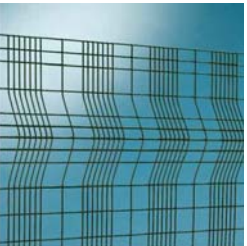
Close boarded timber fence type



Proposed paladin security sliding gate - to yard entrance



Proposed paladin security gates - to yard egress



Proposed paladin fence type



Proposed Goal Post barrier



Proposed security access barrier

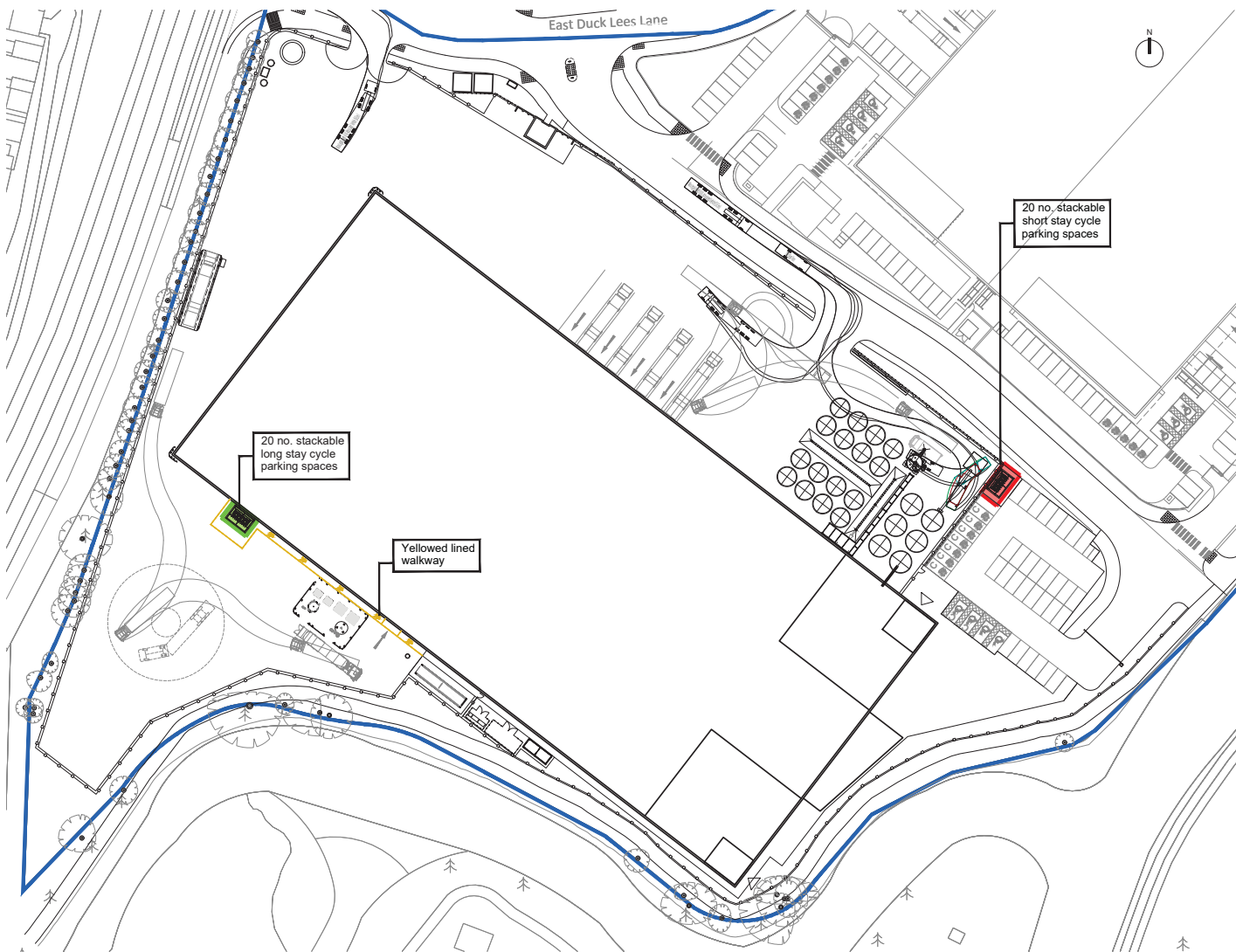
rev amendments by dkd date
 Enfield Distribution Park,
 Phase 3
 Fencing Details

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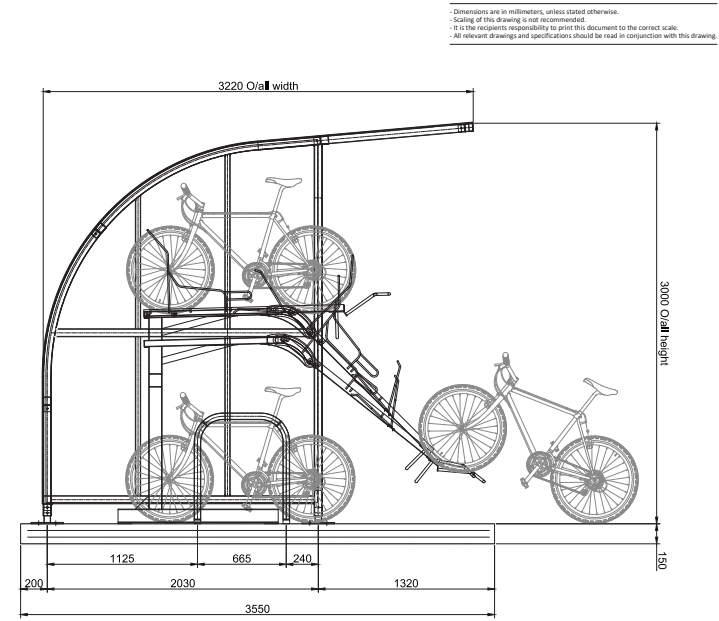
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PLANNING

Drawing Status:	Planning
Drawn / Checked:	HJ / JIM
Date:	08/03/2018
Scale:	1:500 A1
Drawing no:	18032 PO007
Revision:	F



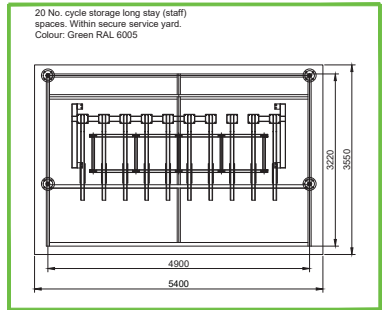
Site Plan Scale 1:500



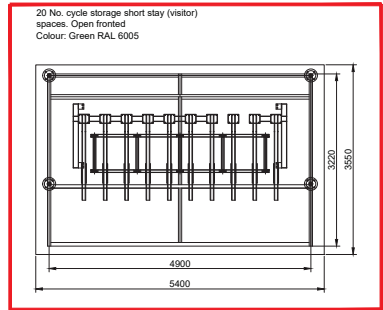
Double Decker Shelter Elevation scale 1:20



NOTE: Cycle shelters to be finished in Green (RAL 6005)



Cycle Shelter Plan Scale 1:50



Cycle Shelter Plan Scale 1:50

rev	amendments	by	chk	date

Enfield Distribution Park,
Phase 3
Cycle Shelter Details

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Date:	08/03/2018
Scale:	1:50 A1
Drawing no:	Revision:
18032	PO008 F



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MATERIALS KEY

- Proposed Bitumen Macadam Road Surfacing
- Proposed 80mm Concrete Block Paving
- Proposed 60mm Concrete Block Paving
- Proposed Bitumen Macadam to Footway & Cycleway
- Proposed Soft Landscape
- Proposed Concrete Service Yard
- Proposed Breerton Gravel



Site Plan
 Scale 1:500

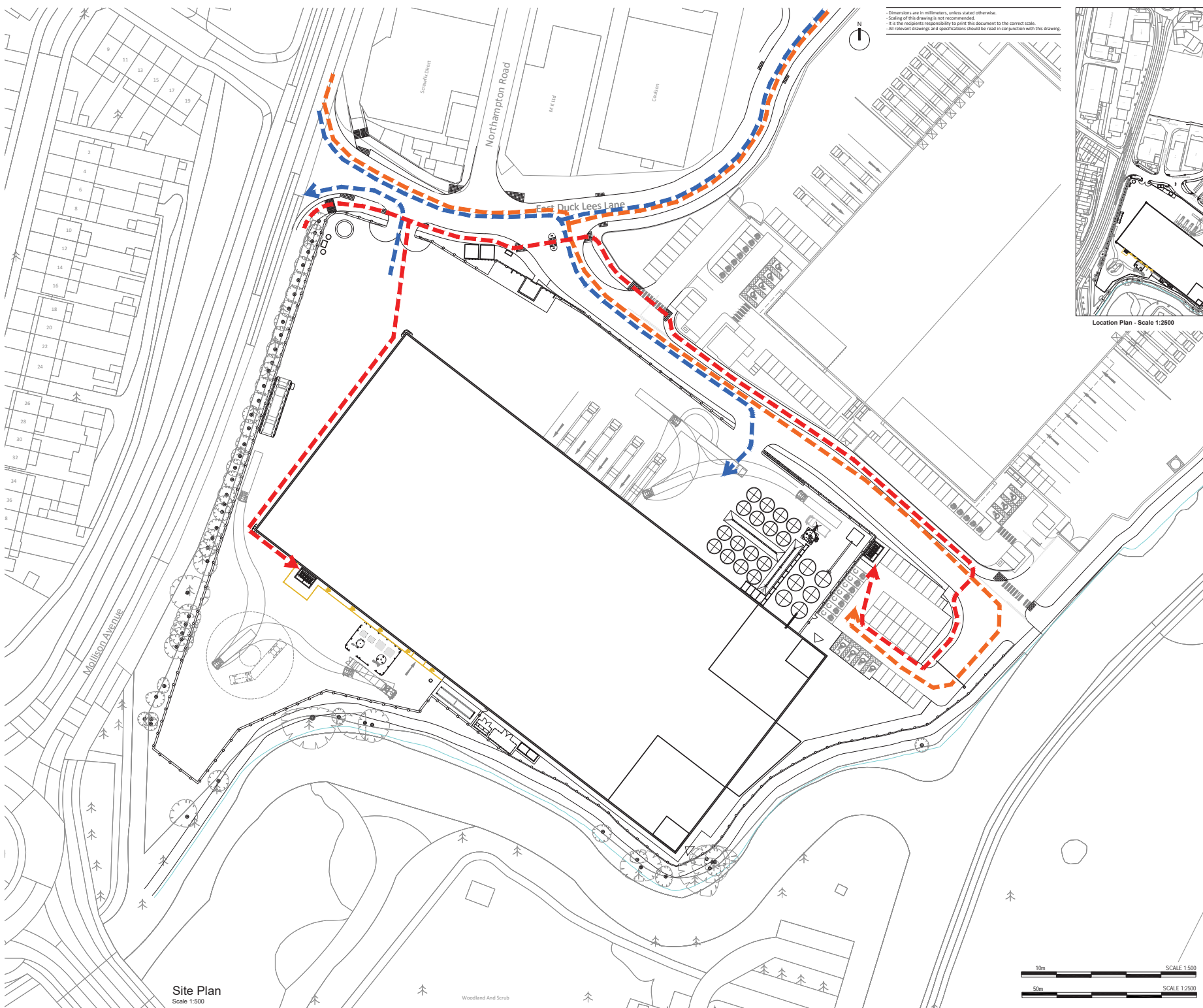
PLANNING

rev amendments by chd date
Enfield Distribution Park, Phase 3
 Proposed External Surface Materials

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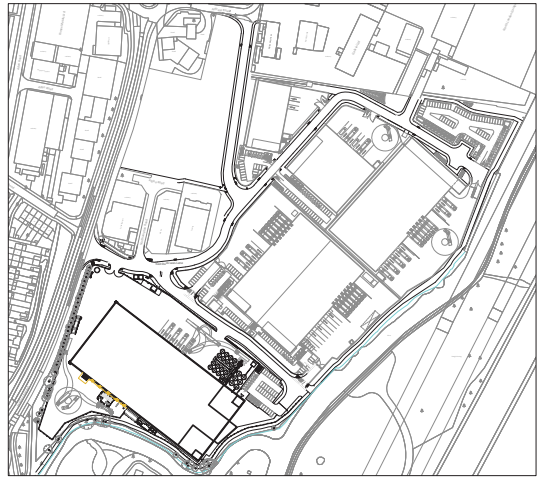
Drawing Status:	Planning
Drawn / Checked:	HJ / JHM
Date:	09/03/2018
Scale:	1:500 A1
Drawing no:	Revision
18032 P0009	G



Site Plan
Scale 1:500

Woodland And Scrub

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Location Plan - Scale 1:2500

- 3m Wide Cycle & Pedestrian Access
- HGV / Van Access
- Car Access



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Enfield Distribution Park, Phase 3
 Movement Plan

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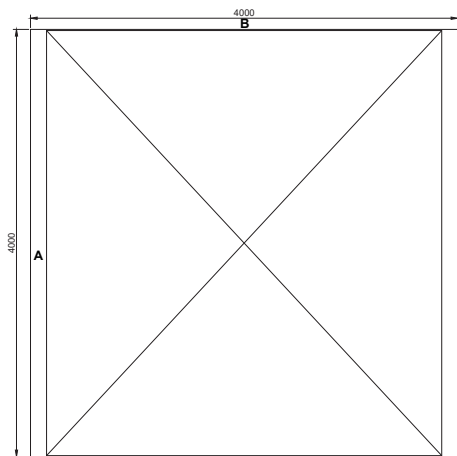
PLANNING

Drawing Status:	Planning
Drawn / Checked:	HJ / JJM
Date:	07/03/2018
Scale:	1:500 A1
Drawing no:	Revision:
18032	P0015
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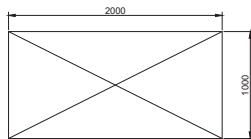
Ring-main Unit/ HV Enclosure



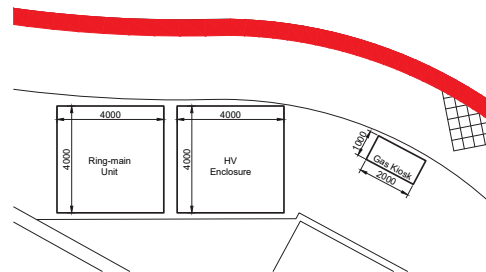
Ring-main Unit/ HV Enclosure Plan Scale 1:25



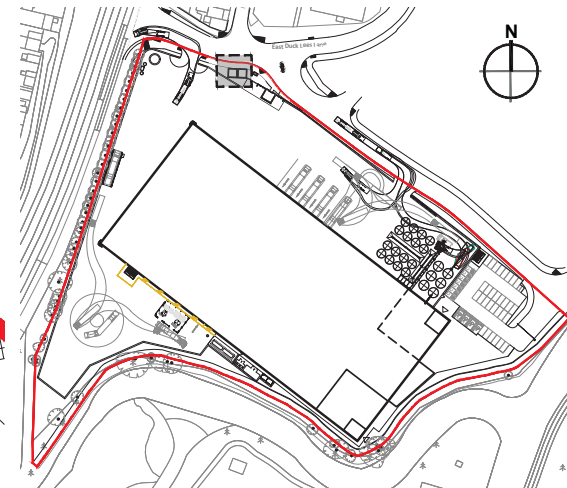
Industrial Gas Kiosk Housing



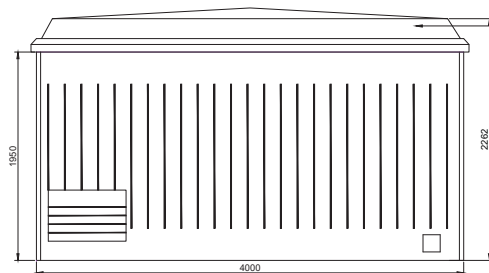
Gas Kiosk Plan Scale 1:25



Extract Plan Scale 1:100

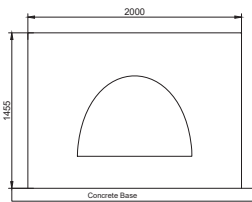


Location Plan Scale 1:1250

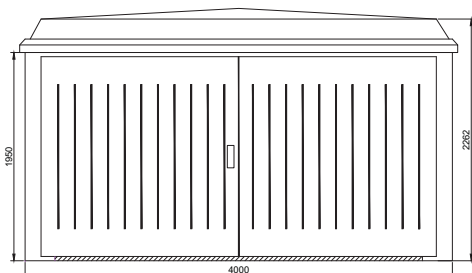


Ring-main Unit/ HV Enclosure Side Elevation A Scale 1:25

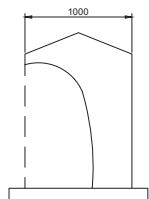
GRP Substation
 Colour - Grey RAL 7012
 or similar



Gas Kiosk Front Elevation Scale 1:25



Ring-main Unit/ HV Enclosure Front Elevation B Scale 1:25



Gas Kiosk Side Elevation Scale 1:25

rev amendments by ckd date
 Enfield Distribution Park,
 Phase 3
 Substation & Gas Kiosk

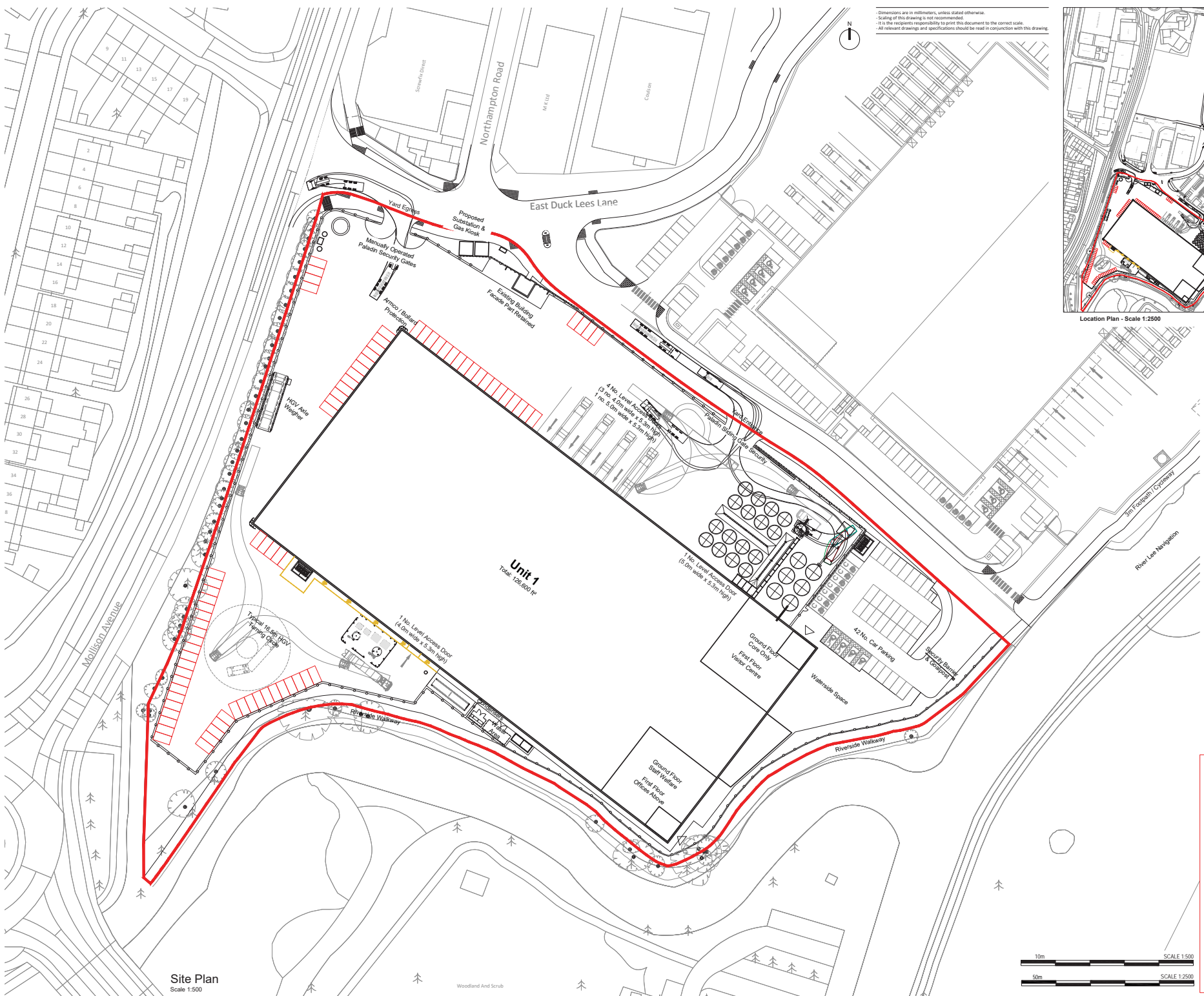


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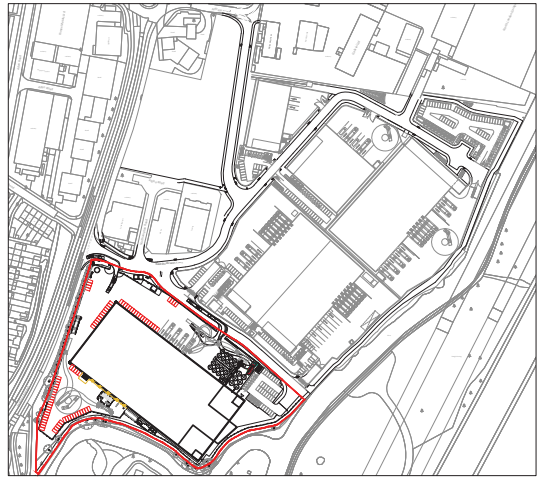
Drawing Status:	Planning
Drawn / Checked:	HJ / JIM
Date:	05/07/2018
Scale:	1:25 A1
Drawing no:	18032 P0018
Revision:	F

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SCHEDULE OF ACCOMMODATION

Car Parking	42
Motorcycle Parking	2
80 Additional Parking Spaces	

Site Plan
Scale 1:500



rev | amendments | by | dtd | date

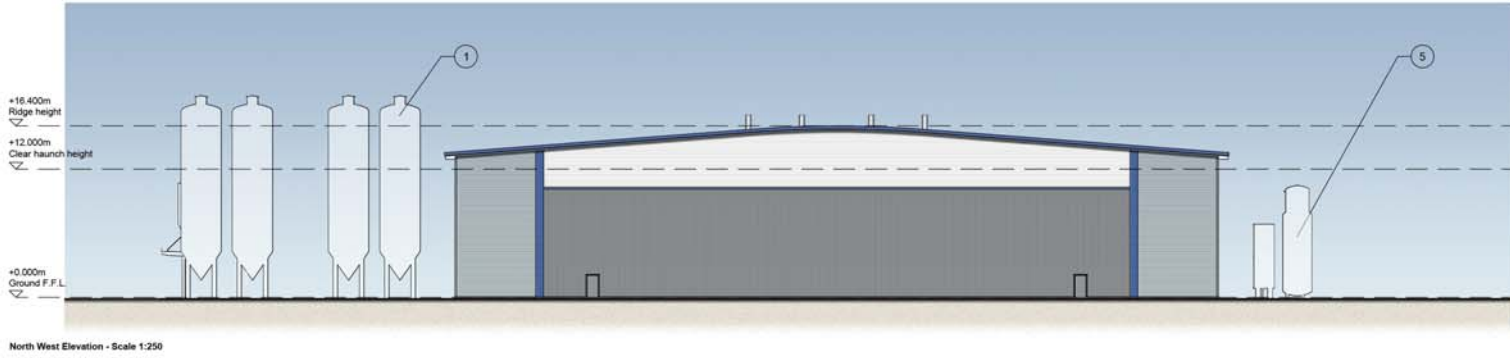
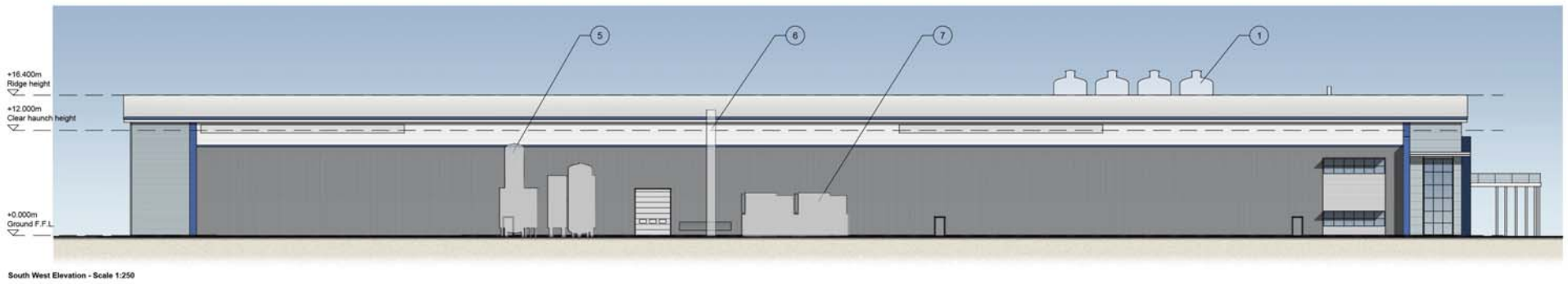
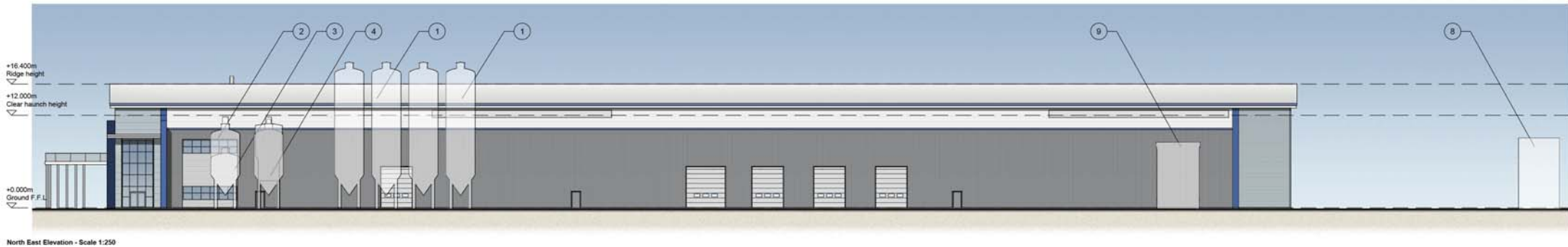
Enfield Distribution Park, Phase 3
Site Plan Showing Potential Additional Parking

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Drawn / Checked:	HJ / JIM
Date:	11/09/2018
Scale:	1:500 A1
Drawing no:	Revision:
18032 PO023	B

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- KEY:**
- ① CCT's 16 No. Total
19m High
 - ② Malt Silos 6 No. Total
11.1m High
 - ③ Yeast Vessel 1 No. Total
8m High
 - ④ Spent Grain Silo 1 No. Total
12m High
 - ⑤ BDC Nitrogen 1 No. Total - 10.65m High
BDC CO₂ 1 No. Total - 8.34m High
 - ⑥ Chimney 1 No. Total
14.5m High
 - ⑦ Refridge 40' Container & 2 No.
Condensers 5.02m High
 - ⑧ Balance Tank 1 No. Total
9.2m High
 - ⑨ Extent of existing facade to be retained
8.6m High



rev amendments by (sk) date
**Enfield Distribution Park,
 Phase 3**
 Elevations - External Equipment

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Drawing Status	Planning
Drawn / Checked	HO / JHM
Date	21/03/2018
Scale	1:250 A1
Drawing no.	Revision
18032 P0011	E

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LONDON BOROUGH OF ENFIELD

PLANNING COMMITTEE

Date : 20th November 2018

Report of

Executive Director - Place

Contact Officer:

Andy Higham
Tel No: 020 8379 3848

Ward: Enfield Town

Ref: 18/03009/FUL

Category: Major

LOCATION: Genotin Road Car Park, Genotin Road, Enfield, EN1 2AG

PROPOSAL: Erection of a five storey block of offices (B1a), ground floor business café (B1a/A3) and conference space (B1a/D1), with basement level, ground floor car parking, landscaping and ancillary works.

Applicant Name & Address:

Stoford Properties Limited and Metaswitch
Networks Limited
Lancaster House
67 Newhall Street
Birmingham

Agent Name & Address:

Joanne Russell
c/o Turleys
9 Colmore Row
Birmingham

RECOMMENDATION:

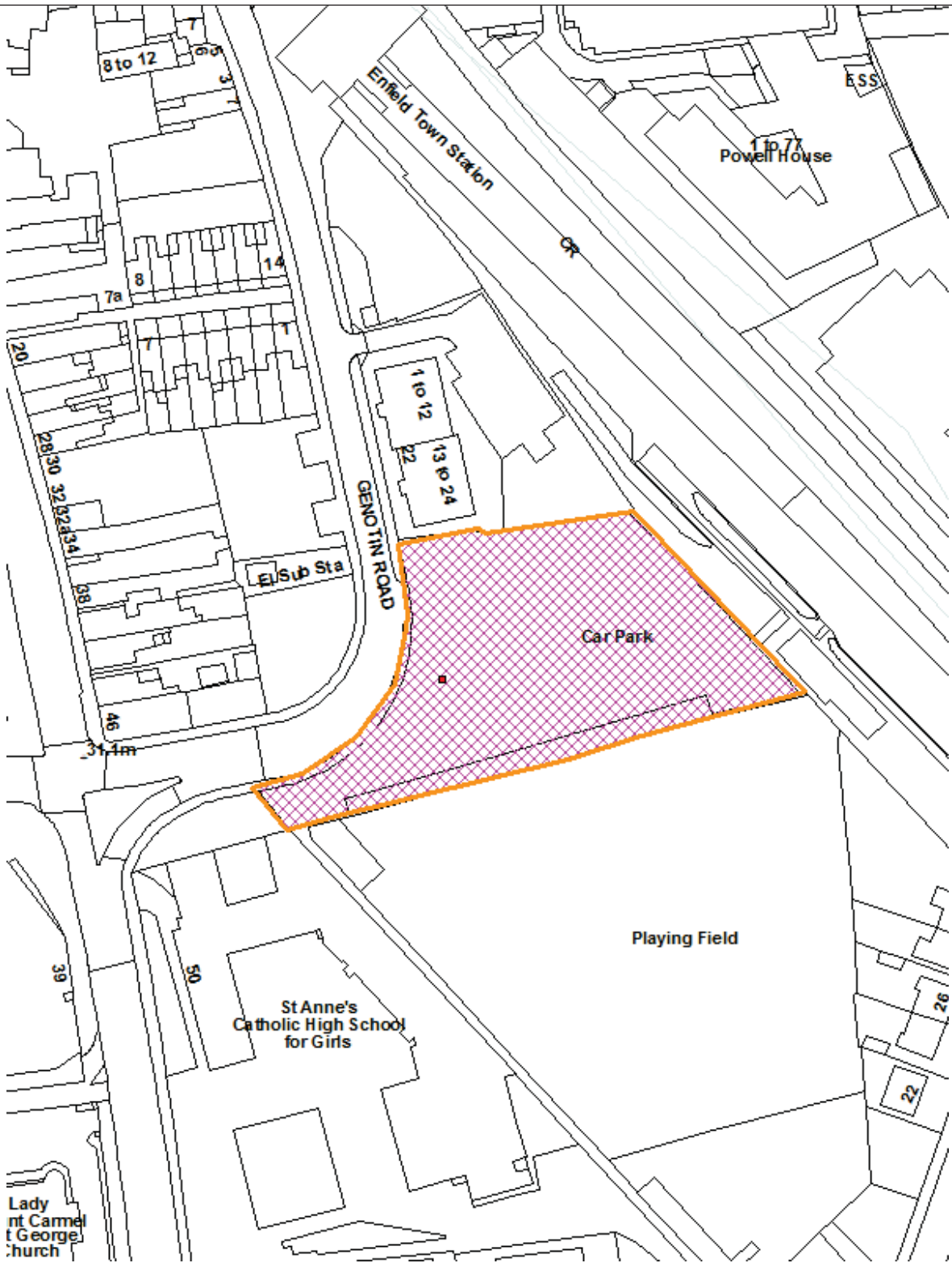
That subject to the completion of the S106 agreement the Head of Development Management be authorised to GRANT planning permission subject to the following conditions:

Note for Members

At Planning Committee on 25th September 2018, Members resolved to defer the application to enable officers to provide further clarification on a number of points and concerns raised at the meeting.

Revised plans and further information has now been received and the report has been updated in light of this in support of the recommendation to grant conditional planning permission

Ref: 18/03009/FUL LOCATION: Genotin Road Car Park, Genotin Road, Enfield, EN1 2AG



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Scale 1:1250

North



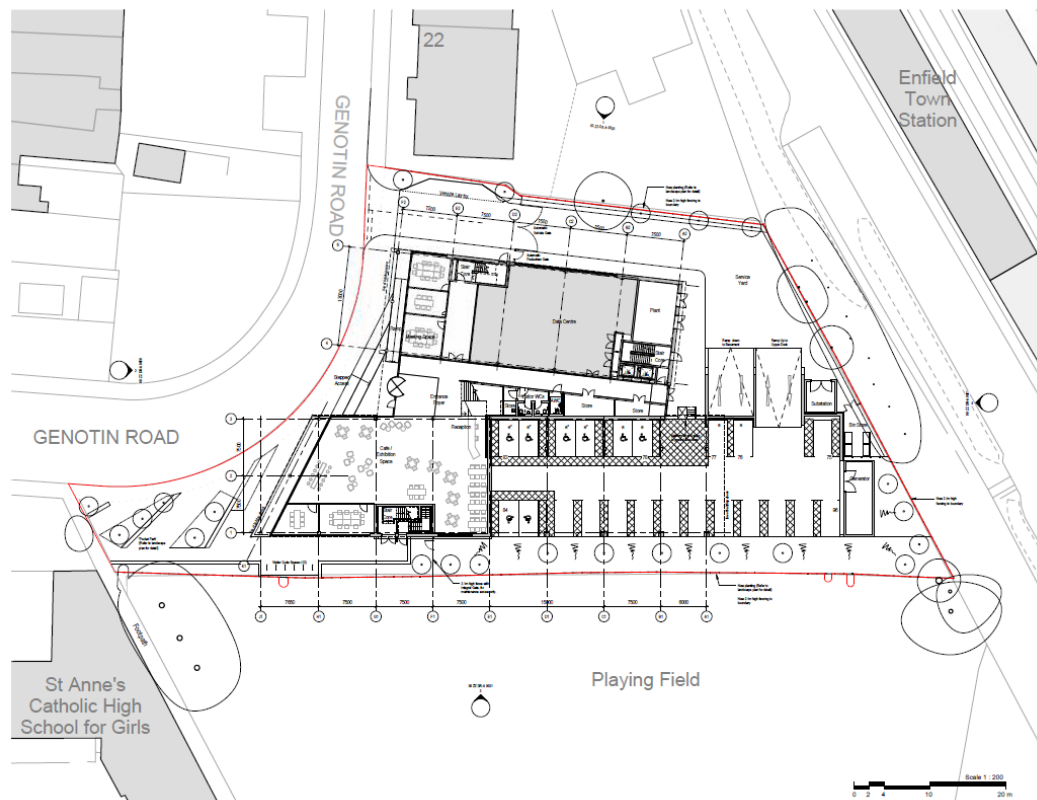
1. Executive Summary

- 1.1 The report seeks approval to a proposed office led mixed use development of the Genotin Road car park. Metaswitch are an existing local company employing 360 people (with a potential to increase to circa 500) who are wishing to consolidate a number of existing office locations across Enfield Town into a single location through this development and continue to grow their business.
- 1.2 The application is recommended for approval, for the following key reasons:
 1. it would make a positive contribution to increasing London's supply of offices and support Enfield Town Centre and the Outer London economy, and is consistent with policies in the London Plan, the Enfield Development Management Document 2014 and the Enfield Town Framework Masterplan 2018.
 2. due to its design, size, scale and siting, the proposed development responds appropriately to the character and appearance of the street scene and surrounding area
 3. the proposed development does not significantly impact on the existing amenities of the occupiers of nearby properties in terms of loss of light, outlook or privacy.
 4. the proposed development makes appropriate provision for servicing, access, parking, including cycle parking and visibility splays, and in this respect complies with adopted and emerging local, regional and national policy.
 5. The proposed development, will be BREEAM Excellent and will contribute to the mitigation of and adaptation to climate change, having regard to adopted local, regional and national policies.
- 1.3 There are differing opinions on the form and appearance of the proposed development but all support the principle of the development and its benefits to the town centre.
- 1.4 Revisions to the scheme have been made since its submission and following the meeting of the Planning Committee on 25th September. These revisions include:
 - i) the introduction of a café at ground floor level that offers opportunities for business networking and creates an active frontage to the street;
 - ii) the introduction within the 4th floor of a flexible space available for external meetings, and conferences;
 - iii) revision to the elevational treatment to the southern façade;
 - iv) enhancements to the sustainable drainage scheme;
 - v) enhancement to the secure by design strategy
- 1.5 These changes strengthen the design quality of the scheme. It remains the opinion that the proposed development is acceptable when considered against the various policies which promote development in town centres.
- 1.6 The main issues raised by this application which Members of the Planning Committee must consider are:
 - i) Principle of land use

- ii) Compatibility with Enfield Town Framework Masterplan and the ability of adjoining sites to come forward for redevelopment
- iii) Design and relationship to the Character and Appearance of the surrounding area;
- iv) The relationship of the development to neighbouring sites including 22 Genotin Road;
- v) Design and the relationship of the development to the Enfield Town Conservation Area;
- vi) Transport including parking and access
- vii) Landscaping & Trees
- viii) Sustainability, Energy and Ecology.

2. Site and Surroundings

- 2.1 The application site comprises the existing Council owned Genotin Road Car Park (126 spaces). It measures 0.37ha in size and is located within Enfield Town. The site lies within the Enfield Town Framework Masterplan area and is located on the edge of, but outside, Enfield Town Conservation Area which lies to the west. Access into the site is from Genotin Road.
- 2.2 To the east of the site is the railway line serving Enfield Town Station managed by Network Rail/Transport for London. To the south of the site there are currently playing fields, used by St Anne's School and owned by the Sisters of the Holy Family of Nazareth - Good Shepherd Province, who have a Convent on London Road. The adjacent school shares a boundary with the playing fields, and also Genotin Road.
- 2.3 To the west on the opposite side of Genotin Road is an open, green space adjoining to the rear of properties on London Road which form part of the Enfield Town Conservation Area.
- 2.4 To the north of the application site is a residential building, 22 Genotin Road. This is occupied as 24 flats. The building is three storeys with a pitched roof, and the elevation facing the application site includes 12 windows that serve residential units within the building. To the rear of this residential block is an area of communal amenity space.
- 2.5 Whilst there are a number of trees to the northern, southern and eastern boundaries, there are no Tree Preservation Orders on the site or on adjoining land.



3.0 Proposal

- 3.1 The development involves the redevelopment of the existing 122 space car park for a five-storey office led mixed use development incorporating café and conference / meeting space together with a ground and basement car park to accommodate 96 car parking spaces and cycle provision with associated works.
- 3.2 The proposed office (B1a) use would be distributed across the ground to the fourth floor, with car parking located at basement and ground floor level. The front of the building would also include at ground floor, a café and exhibition space that will activate the street frontage. The ground floor reception and foyer areas will be located here, in addition to a series of meeting rooms and informal break out spaces together with a business lounge for employees and visitors to use. These areas will be visible from the pavement outside of the building helping to provide a sense of activity at street level, beyond the glazing.
- 3.3 The central atrium that connects the two wings of the building together will be a strong feature allowing light to centrally spill down through the core. This feature will also create a legible entrance when viewed from Genotin Road. Vehicle and pedestrian accesses from Genotin Road, with servicing and cycle parking to the rear are also proposed. Amendments to the proposal have been secured during pre-application discussions with the applicant and subsequent to the Planning Committee meeting on 25th September. These improvements address the design and form of the building, external appearance, the use of high quality materials, vehicular and pedestrian access, the western buffer zone and SuDs.
- 3.4 The overall floor space provided within the development is 8,946 sqm (GEA). The building will have 4 office floors and a further level of mechanical plan on the roof, although the building varies in height to respond to the site and its surroundings. The maximum height however will be 23 metres with the building height stepping down to 20.5 metres at its northern elevation and 20 metres at its western elevation with Genotin Road.

- 3.5 The access to the site providing both vehicular and service access is from Genotin Road close to the boundary with No 22 Genotin Road. This is in approximately the same location as the current car park access.
- 3.6 The appearance of the building is contemporary and modern with the intention of creating a landmark for Enfield Town Centre. The building has strong eastern and western facades, that optimise the use of glazing and detailing with solid panel, metal, and vertical fins and the accent use of materials found within the adjacent Conservation Area.
- 3.7 Stone has been selected as the reference material linking back to the conservation area. A stone plinth forming a base to the building, that continues into the reception space, with stone benches, low walls and other ground plane materials drawn from historic context have been proposed.



The Applicant

- 3.8 Metaswitch Networks Ltd (hereafter, Metaswitch) are the world's leading cloud based native communications software company. Established over 30 years ago, Metaswitch have an enviable history of providing high performance hardware-independent software to the communications industry, whilst solving its most difficult problems.
- 3.9 Having been established in the Borough for almost 40 years, Metaswitch has built strong relationships with businesses in the town and in addition the company's employees directly contribute to the local economy.
- 3.10 The total annual wage roll for Enfield staff is £17.4m. Metaswitch also buy all the produce and groceries for the canteen and tea-rooms locally (around £250,000 per annum), plus the following local expenditure has been identified by the Applicant:
- i) Pubs/restaurants – Metaswitch frequently pays for staff morale events and most employees have their Christmas lunch locally - £60,000 per annum
 - ii) Local hotels – Metaswitch bring a lot of employees from other offices (mainly USA) to Enfield and they stay in local hotels and eat in local restaurants - £20,000 per annum
 - iii) Taxis – Metaswitch employ local taxi firms to take staff to and from airports - £3,000 per annum
 - iv) Furniture – Metaswitch use local suppliers to buy our office furniture - £25,000
 - v) The Metaswitch budget for local donations is £40,000 per annum
 - vi) Metaswitch pays for employees to have regular eye-tests from opticians in Enfield.
- 3.11 In terms of recruitment and training Metaswitch work with schools, universities and professional bodies

- 3.12 The need to consider a new office development for Metaswitch has arisen following discussions regarding the potential redevelopment of one of their existing buildings within Enfield Town (Ross House). In order for the company to remain local, which is their preferred option, an alternative facility needed to be identified especially if it brought about the opportunity to consolidate all their Enfield Town sites into one location. After an extensive search both within Enfield Town and across the Borough, the site of Genotin Road car park already highlighted within the then emerging Enfield Town Framework Masterplan, was identified.

4.0 **Planning History**

- 4.1 There is no relevant planning history for this application site.

5.0 **Summary of Key Reasons for Recommendation**

- i) The principle of this office led mixed use development is acceptable for this location;
- ii) The development would contribute to the vitality and viability of Enfield Town;
- iii) The development provides an acceptable level of car parking that meets both the needs of the applicant and the town centre;
- iv) The development is fully consistent with the objectives of the adopted Enfield Town Framework Masterplan;
- v) The proposal does not prejudice the ability of adjacent development sites identified in the Enfield Town Framework Masterplan to be developed;
- vi) The acceptability of the development in terms of its siting, scale and design relative to the character and appearance of the Enfield Town Conservation Area;
- vii) The development does not prejudice the amenities of the neighbouring residential properties at No 22 Genotin Road;
- viii) Issues of sustainability, energy and SuDs are controlled by condition.

6.0 **Consultation Responses**

Public

- 6.1 Consultation letters were sent to 426 neighbouring and nearby properties. In addition a notice was displayed at the site (16/08/18) and in the local press (15/8/2018). Representations have been received from 25 individuals: 17 raising points against and 11 in support. These are summarised below:

6.2 Against:

- Development too high, too big and overdevelopment of the site.
- Obstructed views of other buildings
- Loss of parking
- General dislike for the proposal
- More open space required
- Strain of existing parking facilities
- Loss of the existing alleyway to the catholic school
- Loss of privacy, light, overshadowing and out of character
- Contrary to the Mayors Transport Strategy
- Resulting pollution due to traffic and noise
- Affect the businesses in town due to the loss in the car park
- Unsightly
- The multi storey car parks would not accommodate for disabled persons

6.3 Support:

- Thriving opportunity for businesses and locals of Enfield Town
- Would keep existing businesses going
- Do not understand why people are concerned about the loss of car park
- Bring in more employment
- Already enough parking spaces within the borough
- Net benefit to the town and community is far greater than a short term inconvenience.

6.4 Following the meeting of the Planning Committee on 25th September, revisions have been incorporated into the scheme:

- i) the introduction of a café at ground floor level that offers opportunities for business networking and creates an active frontage to the street;
- ii) the introduction within the 4th floor of a flexible space available for external meetings, and conferences;
- iii) revision to the elevational treatment to the southern façade;
- iv) enhancements to the sustainable drainage scheme;
- v) enhancement to the secure by design strategy

No further comments from residents have been received.

External

6.5 Metropolitan Police: there is no objection to the development based on its current design and use. Following discussions between the Metropolitan Police, Applicant and the Council, since the previous meeting, the Secure by Design Officer has confirmed that mitigation will be incorporated into the design relating to all the areas of concern we raised.

Officer Response: A standard condition is proposed to ensure the development achieves a Secure by Design accreditation - see Conditions 23 & 24

6.6 Enfield Town Conservation Area Study Group:

The Group are a non-statutory consultee but have been notified of the development due to its proximity relative to the Enfield Town Conservation Area.

From the Groups perspective the most important aspects are the west façade, pointing directly down into Cecil Road and the Conservation Area; and the north elevation facing along Genotin Road towards the Town Park Station which acts as a “gateway” to the Conservation Area.

The Character Appraisal notes that “the gateways to the Conservation Area are important” and that the “approach from Enfield Town rail station is unattractive” whilst Genotin Road has a “poor environment” “dominated by the multi-storey residential development to the north-east of the railway station”, save for the “small scale urban space of Genotin Terrace” which is specifically mentioned. The proposed north elevation in the opinion of the Group disappointingly shows an unbroken, bulky and flat facade towards the train station and Genotin Road. The Group feel this would not enhance the poor environment of Genotin Road nor attempt to integrate the area to the north of it into the small scale urban space of Genotin Terrace and the rest of the Conservation Area. The Group are of the opinion that the harm thus caused to the setting of the Conservation Area would need to be given considerable importance and weight in deciding whether the

design is acceptable and the Group would hope that an improvement making the elevation less monolithic and more interesting can be achieved.

The Group does however consider the west elevation facing Genotin Road to be acceptable.

The Group also acknowledges that improvements to material and landscaping have been made and request a condition to ensure the quality of appearance / public realm is maintained.

Although not directly facing or affecting the setting of the Conservation Area, the Group considers the east elevation to be hugely important as it will be visible to those arriving to the Town by train and recommended that this elevation should be interesting and should not have any service elements visible. Similarly, the Group consider the southern elevation needs to be a good and positive neighbour to any future re-development which it faces as well as the users of the proposed bridge.

Careful consideration will also need to be given to the need for access to, proper maintenance and irrigation and drainage of, to ensure the appearance of the building is not harmed.

Officer Response: Revisions to the design of the scheme have emphasised the central atrium as a feature which creates articulation in the northern elevation to address the concern identified by the Group from this perspective. The positive comments on the western elevation are welcomed. Conditions 9, 10, 11, 12, & 13 deal with the approval of materials, external finishes and landscaping. The scheme does involve a green roof and the approach to its maintenance forms part of the strategy – see Condition 28

6.7 Network Rail for London (RfL):

Rail for London (RfL) has reviewed the application from an Infrastructure Protection perspective given the relationship of the site to the railway line. No objections are raised to the proposed development.

Standard conditions have been requested to safeguard the network infrastructure during construction and these will cover , the submission and approval of a management plan, risk assessment and method statement for construction work and in particular, should specify that cranes should not be erected or dismantled until RfL's approval has been obtained, a that adequate measures will be introduced to ensure that the debris/equipment cannot fall or be blown onto the railway and that the development is designed to militate against operational noise and vibration from an existing railway.

Officer Response: The items identified by RfL are addressed at Conditions 4, 5 & 6

6.8 Transport for London (TfL):

Transport for London raises no objection. TfL note current and emerging policy but based on the circumstances of the applicant and the relationship to the viability of the town centre, has confirmed the redevelopment of the car park and re-provision of 96 spaces for the use of the Applicant and the public during evenings and weekends is acceptable..

TfL understands that 54 long stay spaces and 12 short stay cycle spaces are to be provided; these meet the standards of the draft London plan which TfL welcome. TfL recommend sufficient signage should be provided for the short stay cycle spaces for visitors and welcomes the provision of shower and locker facilities

TfL consider the measures to promote sustainable travel are acceptable but request a Travel Plan to manage this process. Conditions are also requested to cover the provision

and the technical specification for the agreed cycle parking, a Construction Logistics Plan, A Delivery and Servicing Plan and a requirement to commit to a Considerate Contractor Scheme

Officer Response: the items identified by TfL in support of their confirmation of no objection to the development are covered in the legal agreement (Travel Plan) and at Conditions 19 & 20.

Internal Consultation

6.9 Environmental Health Officer:

No objection raised as there are no perceived environmental impacts. In particular there are no concerns regarding air quality or contaminated land.

There is however a need for a condition as the acoustic report submitted as part of the application was unable to assess plant noise impact on local residents because the specification for the plant to be used has not been selected.

Response: The requirement for an acoustic report is set out at Condition 28

6.10 Public Health Officer:

From a public health perspective, alternatives to the provision of car parking spaces would be welcome as it is felt, there are significant benefits to air quality and public health by encouraging model shift, If this level of parking is needed, measures should be put in place through the travel plan to reduce reliance. Home working could form part of this approach.

Officer Response: A Travel Plan is to be secured by the S106 agreement which will encourage model shift towards non-car modes.

6.11 Urban Design Officer:

The benefits of circa 500 jobs in the town centre are acknowledged. However, the site has potential for a greater mix of uses and ideally the scheme would have less parking. The circumstances of the Applicant are acknowledged and the public use of the parking area at evenings/weekends can offset this. There is concern over the proportion of the ground floor taken up with parking as this could present an inactive and unattractive façade to the playing fields (especially as parking is included at ground and above ground level).

The proposal ensures that the north-south access route to the St Anne's development site (proposed in the Enfield Town Masterplan) is protected and an adequate width would be available upon the development of the adjoining land.

The land between the building and the pavement should be integrated into the public realm to avoid a sudden change in materials.

The inclusion of an atrium is supported as a positive enhancement of the building's design. Furthermore, although the building will be positioned in close proximity to existing windows of 22 Genotin Road, mitigation of any overlooking can if necessary be managed through the façade treatment (by using obscured glazing opposite the residential windows).

Landscaping to the front of the building will enhance the public realm and will enable the frontage landscaping to be altered to accommodate an access to the St Anne's site as shown in the recently adopted Enfield Town Masterplan.

Response: The level of car parking is considered appropriate given the requirements and circumstances of the applicant and the public use secured in support of the viability of the town centre. The visual relationship to the playing field will be addressed through landscaping along this southern boundary and the introduction of a café addresses the concern around a lack of mixed use.

6.12 Traffic and Transportation Officer:

Following discussions with the Applicant, the Traffic & Transportation officer confirms that the proposal in terms of the overall parking provision, the design and layout of the car park together with servicing arrangements, is acceptable. The following elements are also considered to be acceptable:

- Trip generation.
- Pedestrian access.
- Vehicle access including for a standard London Fire Brigade pump vehicle (at 7.9m these are shorter than the tracking for a refuse vehicle which has been provided).
- Car parking levels including electric vehicle provision and disabled parking.
- Cycle parking and access.
- Draft travel plan including monitoring via TRICS compliant surveys.

Items to be secured by way of legal agreement:

- Travel plan and monitoring fee (£3,900).
- Traffic order costs (£5,000).
- Cycle Enfield contribution (£22,205.40).
- Highway works via a S278 agreement.
- Commitment to covering the cost of variable messaging signage changes.
- Strategic highway works contribution (£25,000) agreed.
- Variable signage.

6.13 SuDS Officer:

No objection to the revised planning application. Further to the Planning Committee meeting on 25th September, the SuDs officer has confirmed that discussions with the Applicant have resulted in amendments to the scheme which means that the development is fully compliant with the Council's adopted SuDs policy. An off-site financial contribution is no longer required.

Response: A condition securing the implementation of the agreed SuDs strategy is included – see Conditions 7 & 8

6.14 Tree Officer

No objection. There are no significant tree constraints or trees covered by a tree preservation order within or adjacent to the proposed development site.

There are several large trees located off-site in the School playing field to the south and the development proposes to remove some of these trees to facilitate the development. No planning consent is required for their removal because they are not subject to any protective designation but is considered necessary to remove them due to the proximity of the building and the fact there will be continual issues with regard to shade, seasonal debris and tree and building maintenance. It should also be noted that these trees would have to be removed to facilitate the development identified in the adopted Master Plan

There is limited opportunity for landscaping within the development but the landscaped areas are considered adequate. The proposed landscape corridor on the south of the site will also provide an opportunity for landscaping.

Response: the landscaping condition is set out at Condition 13

6.15 Conservation Advisory Group

The Conservation Advisory Group comment that they are unanimously supportive of the proposed development and although they recognise elements of the design could be improved on balance, the design and scale is considered acceptable as are the benefits of the development to the wider town centre. No objection is therefore raised.

However, the Group do request that careful screening of plant and equipment, usually positioned at roof level, will be vital in maintaining a clean image (the roof of the Dugdale Centre, Cecil Road is a nearby example where little thought was given to plant screening)

The Group also wish to be consulted on materials / external finishes as the design develops.

Response: The development incorporates appropriate screening to the roof top plant. The approval of materials is covered by Condition 9 & 10

6.16 Conservation Officer

No objection on heritage grounds is raised. The proposals will conserve the setting of the Enfield Town Conservation Area. If minded to approve, it is advised conditions to confirm the quality and specification of materials, external finishes and other external details should be imposed. In addition, to safeguard archaeological interests, a condition requiring a watching brief during construction is recommended.

Response: The conditions referred to are Conditions 9, 10, 11, 12 & 13 of the schedule of Conditions at Section 14.

6.17 Place & Design Review Panel

The Panel an independent advisory body, reviewed the application in June 2018.

The Panel recognised the importance of Metaswitch to Enfield Town and strongly supported their desire to remain in Enfield as a critical business and employer for the town.

The opportunity for this project to form the first phase of the wider Enfield Town Framework Master Plan was acknowledged as an exciting prospect.

However, the Panel had a number of comments regarding the proposed development:

- Level access needs to be addressed, avoid a ramp or elevating the building as this would ruin the appearance.
- The roof form at the top whilst it may have a logical function behind it, it is an alien feature which does not contribute positively to the building
- The client needs to view the development in a more forward-thinking manner, i.e. having a restaurant at the top with the use of lifts, lack of sustainable roofs, excessive parking spaces within a location which is walking distance to the station are all elements which need to be reconsidered.
- The site has the potential to have a building of greater height than what has been proposed here.

- The design of the building should be of a high standard which would form a gateway into the conservation area (but this is not currently reflected) and the building also should be a statement in itself. At the moment the details of the building do not provide any justification or connection to the conservation area or the surrounding area of Enfield.
- Connectivity and permeability in and out of the site needs a good level of security, good lighting and usable permeable space in servicing area.
- Landscaping treatment adjacent to the existing residential apartments should be considered.
- The client needs to understand the overall contribution the development would be making to the wider area, in terms of massing and urban grain, but also to bring about a proposal which ties well into the wider context of the surrounding.

Officer Response: The Panel did not object to the principle of development recognising the benefits for Enfield Town Centre. Since the Panel's consideration of the scheme, there have been amendments to the proposed development with revisions to the use, materials, building design and landscaping. The revised proposal will result in well designed scheme that relates appropriately to its setting and the wider town centre. In addition, it is confirmed additional height could be added if circumstances permit in the future. It is recognised there are differing opinions on the scheme, but the benefits in terms of employment and the wider town centre outweigh these points.

7.0 Pre-Submission Consultation

- 7.1 The proposals have evolved over a period of engagement covering more than 12 months. This engagement has been at a number of levels from meeting with officers and residents, through to a public consultation event in May 2018. Details of the engagement process are summarised below.

Pre-Application Meetings

- 7.2 Four meetings with officers were held to discuss the principles of the planning application the initial two of which were outside of the formal pre application process. The first was held in May 2017. The principle of development was supported by officers albeit with strong preference expressed for the proposal to include a mix of uses, including residential development and for the building to be taller overall. The clear brief from Metaswitch is for a building that is fit for purpose as a company headquarters, without compromising their business strategy, their on-site security or overall design. Furthermore, Metaswitch and their representatives are of the opinion contends that at 4-5 commercial storeys high, the building is sufficiently significant in height, given its edge of Conservation Area location. The Applicants also points to feedback from local residents which echoed this view.
- 7.3 Officers pre application advice also sought to influence the positioning of the building within the site advising that an 10-11m distance from 22 Genotin Road was likely to be acceptable (subject to final design / fenestration); that the setting the building back from the edge of Genotin Road to achieve an improve public realm was recommended; that safeguarding potential access to the playing fields site to the south; and safeguarding an area of land for a potential footbridge connection towards the Tesco site was necessary. In later meetings, comments were raised relating to the level of car parking with officers requesting a reduction in the level of provision. The principle of car parking and the number of spaces has been a matter that Metaswitch consider they cannot reduce further given feedback from existing employees and where the staff are commuting from). In recognition of this concern however, the number of car parking spaces for staff has been reduced to 96 from the 126 originally proposed. Other detailed comments relating to sustainable urban drainage and energy efficiency were also raised by officers.

Meetings with adjacent landowners

- 7.4 Stoford Developments (who are retained to construct the development) on behalf of Metaswitch met with representatives from Chalkwell Park Residents Association; St Anne's School; The Diocese of Westminster and a representative from the Sisters of the Holy Family of Nazareth - Good Shepherd Province convent ('the convent'). Early iterations of the proposals were shared with those attending the meetings. Further invitations were extended to local Members, and local Residents Associations too.
- 7.5 With the exception of the Chalkwell Park Residents Association, the other meetings focussed on how the proposals might potentially relate to their landholdings, and to explore with those landowners their proposals too, given the future development proposed in the Enfield Town Framework Masterplan (EFM). None of the adjacent landowners were able to advise on any timescales for the redevelopment of their sites.
- 7.6 It has been indicated by Stofords that the meeting with Chalkwell Park Residents Association was positive.
- 7.7 A meeting with St Anne's school and the Diocese of Westminster raised key concerns about pupil safety which were addressed at the meeting. The proposed development will seek to erect a secure boundary fence on the southern boundary with the playing fields to ensure that until such a time as a potential footbridge link comes forwards/and/or the playing fields are developed, the playing fields are secured from any intrusion that could occur from the northern boundary. Secondly, Genotin Road is a key route for those pupils walking to school or alighting from the bus. Stoford contractors are appointed on the basis of a Considerate Constructors Scheme, and code of conduct features heavily within that.

Public Consultation Event

- 7.8 Stoford Developments held a public consultation event on the afternoon of 24th May, between 2pm and 8pm at the Dugdale Centre. A press release was issued, key stakeholders including residents groups, members, and other interest groups advised by the Council, were invited to the event. A postcard invitation was also mailed to over 800 local addresses within the immediate neighbourhood.
- 7.9 The event displayed draft proposals and comments were invited. Almost 100 responses were received in total. There was a majority support for the proposals, focussed around retaining Metaswitch in the town centre, the growth of jobs, and the design of the building. The key issue raised by those concerned was the loss of the surface car park.

8.0 Relevant Policy

- 8.1 In accordance with the provision of Section 38(6) of the Planning and Compulsory Purchase Act 2004, this application is to be considered against the provisions of the adopted Development Plan, unless material considerations indicate otherwise. In this case the Development Plan comprises the Adopted Core Strategy and the Adopted Development Management Document (2014) and the relevant policies of the London Plan, and associated SPDs. Other material planning policy considerations include the NPPF and NPPG. The relevant policy is summarised below:

8.2 London Plan (2016)

Policy 2.7	Outer London: economy
Policy 2.15	Town Centres
Policy 3.9	Mixed and balanced communities
Policy 4.1	Developing London's economy
Policy 4.2	Offices

Policy 4.3	Mixed use development and offices
Policy 4.7	Retail and town centre development
Policy 5.1	Climate change mitigation
Policy 5.2	Minimising carbon dioxide emissions
Policy 5.3	Sustainable design and construction
Policy 5.5	Decentralised energy networks
Policy 5.6	Decentralised energy in development proposals
Policy 5.7	Renewable energy
Policy 5.8	Innovative energy technologies
Policy 5.9	Overheating and cooling
Policy 5.10	Urban greening
Policy 5.11	Green roofs and development site environs
Policy 5.13	Sustainable drainage
Policy 5.14	Water quality and wastewater infrastructure
Policy 6.3	Assessing the effects of development on transport capacity
Policy 6.9	Cycling
Policy 6.12	Road network capacity
Policy 6.13	Parking
Policy 7.1	Lifetime neighbourhoods
Policy 7.2	An inclusive environment
Policy 7.3	Designing out crime
Policy 7.4	Local character
Policy 7.5	Public realm
Policy 7.6	Architecture
Policy 7.7	Location and design of tall and large buildings
Policy 7.8	Heritage assets and archaeology
Policy 7.9	Heritage-led regeneration
Policy 7.14	Improving air quality
Policy 7.15	Reducing noise and enhancing soundscapes
Policy 7.18	Protecting open space and addressing deficiency
Policy 7.19	Biodiversity and access to nature

8.3 The London Plan (Draft 2018)

Policy GG1	Building Strong and Inclusive communities
Policy GG2	Making the best use of land
Policy GG5	Growing a good economy
Policy GG6	Increasing efficiency and resilience
Policy SD6	Town centres
Policy SD8	Town Centres: Development Principles & Development Plan Documents
Policy D1	London's form and characteristics
Policy D2	Delivering good design
Policy D3	Inclusive design
Policy D7	Public realm
Policy D10	Safety, Security and resilience to emergency
Policy D11	Fire Safety
Policy E1	Offices
Policy G7	Trees and woodlands
Policy SI2	Minimising greenhouse gas emissions
Policy SI4	Managing Heat
Policy SI5	Water infrastructure
Policy SI7	Reducing waste
Policy SI12	Flood risk management
Policy SI13	Sustainable drainage
Policy T1	Strategic approach to transport
Policy TR2	Healthy Streets
Policy T3	Transport capacity, connectivity and safeguarding
Policy T4	Assessing and mitigating transport impacts

Policy T5	Cycling
Policy T6	Car Parking
Policy T6.2	Office Parking

8.4 Enfield Core Strategy (2010)

Paragraph 2.58 of the Core Strategy states that:

“In the short term, Enfield needs to respond effectively to the economic downturn but also plan and coordinate sustainable growth for the years beyond. The ESP’s Skills and Employment Strategy (2008) has three key objectives looking towards 2011 - to support inward investment and business growth; increase skills and employability of the Borough’s population; and ensure better coordination and information sharing.”

Core Policy 1	Strategic Growth Areas
Core Policy 13	Promote Economic Prosperity
Core Policy 17	Town Centres
Core Policy 19	Office
Core Policy 20	Sustainable Energy Use and Energy Infrastructure.
Core Policy 21	Delivering sustainable water supply, drainage and sewerage infrastructure
Core Policy 22	Delivering sustainable waste management
Core Policy 25	Pedestrians and cyclists
Core Policy 28	Managing flood risk through development
Core Policy 30	Maintaining and improving the quality of the built and open environment
Core Policy 31	Built and Landscape Heritage
Core Policy 42	Enfield Town
Core Policy 43	Wider Enfield Town Area
Core Policy 46	Infrastructure Contributions

8.5 Development Management Document (November 2014)

The DMD provides detailed policies for the assessment of planning applications alongside the Core Strategy. The Document policies specific to this planning application relate to Town Centres, Design and Heritage, Transport and Parking, Tackling Climate Change, Environmental Protection and Green Infrastructure.

The policies are summarised below:

DMD10	Distancing
DMD 25	Locations for Office Development
DMD37	Achieving High Quality Design-Led Development
DMD38	Design Process
DMD 39	Design of Business Premises
DMD44	Preserving and Enhancing Heritage Assets
DMD45	Parking Standards
DMD47	New Roads, Access and Servicing
DMD48	Transport Assessments
DMD49	Sustainable Design and Construction Statements
DMD50	Environmental Assessment Methods
DMD51	Energy Efficiency Standards
DMD53	Low and Zero Carbon Technology
DMD54	Allowable Solutions
DMD55	Use of Roof Space / Vertical Surfaces
DMD56	Heating and Cooling
DMD57	Responsible Sourcing of Materials
DMD58	Water Efficiency
DMD59	Avoiding and Reducing Flood Risk

DMD60 Assessing Flood Risk
 DMD61 Managing Surface Water
 DMD65 Air Quality
 DMD68 Noise
 DMD69 Light Pollution
 DMD70 Water Quality
 DMD72 Open Space Provision
 DMD73 Children's Play Space
 DMD78 Nature Conservation
 DMD79 Ecological Enhancements
 DMD 80 Trees on Development Sites
 DMD81 Landscaping

Enfield Town Framework Masterplan (2018)

- 8.6 The Masterplan is the area action plan referenced in the Core Strategy for Enfield Town and has recently been adopted. The application site is identified within the Masterplan as 'Site 2 Genotin Road Car Park wherein:
- Redevelopment is supported
 - A co-ordinated approach should be taken with adjacent sites
 - Design to respect the adjacent Conservation Area
 - Contribution of the public realm
 - The benefits of the site being a short term development opportunity to strengthen the economy of the town
- 8.7 The Masterplan advises on the form of development which could prove acceptable:
- High density development is appropriate, with taller buildings potentially being acceptable being subject to any impacts on the Conservation Area;
 - Land should be set aside for the provision of a route through to a potential new link over the railway line to the Tesco site;
 - New development should respond positively to the development sites around it to ensure the contribution these sites could make to the town centre is not compromised;
 - New development should directly address and clearly define existing streets and any new routes formed;
 - Where possible, parking management changes should be put in place to help compensate for any temporary loss of parking capacity.
 - Development should also make provision for a footbridge link between the Genotin Road car park and Tesco sites aligned with the southern boundary of the car park. It should allow for the possible relocation of the Enfield Town Station alongside this footbridge.
- 8.8 Land to the north (No 22 Genotin Road) and south (St Anne's Playing Fields) is also identified within the Masterplan for redevelopment opportunities. The latter is required to co-ordinate with the application site to ensure a comprehensive approach is taken and it is noted that the Masterplan states that 'particular attention should be paid to the northern frontage of the site where a new pedestrian route and bridge may come forward in the future.'

National Planning Policy Framework

- 8.9 The NPPF sets out a clear presumption in favour of sustainable development which, for the purpose of decision making, is explained as meaning:
- Approving development proposals that accord with an up to date development plan without delay; or
 - Where there are no relevant development plan policies which are most important for determining the application are out of date, granting planning permission unless:

- a) the application of policies in this Framework that protect areas or assets of particular importance provides a clear reason for refusing the development proposed, or;
- b) any adverse impacts of doing so would so significantly and demonstrably outweigh the benefits when assessed against the policies in this Framework as a whole; or;
- c) specific policies in this Framework indicate development should be restricted

- 8.10 Paragraph 8 of the Framework identifies three dimensions to sustainable development; economic, social and environmental. In essence, the Framework is to be considered as a whole (paragraph 3) and conflict with one or more of its policies does not mean that a development is not sustainable.
- 8.11 The policy direction expressed within the NPPF follows a number of broad themes which help to ensure that development is sustainable. The key themes relevant to this application are 'building a strong, competitive economy' and 'ensuring the vitality of town centres'. Other themes are naturally interwoven into the scheme, including good design, conserving and enhancing the historic environment and so forth.
- 8.12 The clear support for securing economic growth (paragraph 80) is evident, and notes that 'significant weight should be placed on the need to support economic growth and productivity, taking into account both local business needs and wider opportunities'.
- 8.13 Paragraph 85 notes that 'meeting anticipated needs for retail, leisure, office and other main town centre uses over this [plan] period should not be compromised by limited site availability'.
- 8.14 Paragraph 131 states that 'in determining applications, great weight should be given to outstanding or innovative designs which promote high levels of sustainability or help raise the standard of design more generally in an area, so long as they fit in with the overall form and layout of their surroundings'.
- 8.15 The Framework, at paragraph 104 advises that Planning Policies should 'identify and protect, where there is robust evidence, sites and routes which could be critical in developing infrastructure to widen transport choice and realise opportunities for large scale development'.
- 8.16 Finally, 'planning obligations should only be sought where they are necessary to make the development acceptable in planning terms; directly related to the development; and fairly and reasonably related in scale and kind to the development' (paragraph 56).
- 8.17 Other Relevant Policy Considerations

National Planning Practice Guidance
LBE S106 SPD (2016)
Community Infrastructure Levy Regulations 2010
Enfield Town Conservation Area Character Appraisal (2015)
Enfield Town Conservation Area Management Proposals (2015)
Enfield Characterisation Study
Enfield Council Tall Buildings Study

9. Planning Analysis

- 9.1 The main issues raised by this application which Members of the Planning Committee must consider are:
- i) Principle of land use

- ii) Compatibility with Enfield Town Framework Masterplan and the ability of adjoining sites to come forward for redevelopment
- iii) Design and relationship to the Character and Appearance of the surrounding area;
- iv) The relationship of the development to neighbouring sites including 22 Genotin Road;
- v) Design and the relationship of the development to the Enfield Town Conservation Area;
- vi) Transport including parking and access
- vii) Landscaping & Trees
- viii) Sustainability, Energy and Ecology

Principle of Development

- 9.2 Adopted planning policy at national, London and local levels all support the principle of an office led mixed use development incorporating café / meeting space facilities on this town centre site.
- 9.3 In particular, Core Strategy CP1 identifies Enfield Town as a Strategic Growth Area while CP43 specifically focuses on the growth opportunities for Enfield Town and the area around the Station. These emphasise the development potential of the site and the opportunities to support the creation of integrated development around the station, including the Genotin Road car park, involving a “high quality new urban environment and gateway to the town, which complements its historic core”, and “retail-led mixed use development”.
- 9.4 Core Policies 17 and 19 accept that Enfield Town (along with the Boroughs other town centres) should be the main destination for office development. The proposal complies with this policy as the development would retain a key employer within the Borough, consistent with the Council’s local plan policies.

Enfield Town Framework Master Plan

- 9.5 The Enfield Town Framework Master plan was adopted in March 2018 and supersedes previous iterations of area based policies for Enfield Town. As a Supplementary Planning Document it seeks to provide a framework for a strong and deliverable vision for the town whilst not being overly prescriptive to accommodate the necessary flexibility to adapt to emerging pressures and opportunities.
- 9.6 The purpose of the master plan framework is to preserve and enhance its historic market town identity while helping to develop a town centre that meets the future needs of a growing London borough and enable the town to become a distinctive centre for residents, one that can meet the demands of diverse employment, better connections, living spaces and cultural activities.
- 9.7 The Master plan divides the Town into a number of “Character Areas”. The Genotin Road car park lies within the “Enfield Town Character Area” and the car park is identified as a future development opportunity.

SITE 2 - GENOTIN ROAD CAR PARK



- 9.8 The master plan outlines some key principles for the development of this site and these are replicated here:

Key principles and land uses

- This site falls just beyond the boundary of the Enfield Town Conservation Area and whilst it should accommodate a high density scheme, development proposals should be carefully designed as they are likely to affect the setting of the conservation area.
- Redevelopment of the car park and surrounding public realm could make a significant and important contribution to the regeneration of the area and connection to the town centre.
- The Genotin Road car park site presents a short term development opportunity for mixed use development to strengthen the economy of the town.
- Appropriate uses include offices, residential and car parking.
- A co-ordinated approach should be taken with adjacent and other sites within the character area to help promote comprehensive change.
- Redevelopment of the car park site should only proceed once alternative public parking arrangements are confirmed and in place.

Form of development

- High density development is appropriate in this sustainable and accessible location with taller buildings potentially being appropriate subject to any impacts on the setting of the adjacent conservation area.
- Land should be set aside in any development proposals for the provision of a route through to a potential new link over the railway line to the Tesco site.

- New development should respond positively to the development sites around it to ensure the contribution these sites could make to the town centre is not compromised.
- New development should directly address and clearly define existing streets and any new routes formed.

Access and movement

- Where possible, parking management changes should be put in place to help compensate for any temporary loss of parking capacity during development construction - particularly in relation to the operation in the evening of the Dugdale Centre.
- Development should make provision for a footbridge link between the Genotin Road car park and Tesco sites aligned with the southern boundary of the car park.
- It should also allow for possible future relocation of Enfield Town Station alongside this footbridge.

Public realm

- Provision should be made to a longer term green foot bridge and cycle connection across the railway.
- Access to this route from Genotin Road should be safe, generously proportioned and benefit from being addressed by active frontages.

9.9 Against these broad objectives, it is considered:

- The development has been carefully designed to be sympathetic to and safeguard the Conservation Area;
- The development would make a positive contribution to the town centre;
- The development has been carefully designed to not prejudice the ability of other sites to developed
- Sufficient alternative parking is available to the town centre
- The height of the building is appropriate having regard to the Conservation Area and No 22 Genotin Road.
- The development facilitates the future pedestrian link to the east and the potential for Enfield Town station to be relocated

9.10 The proposed development meets the aspirations for this site and the wider area. It must also be recognised that the location of Metaswitch within Enfield Town and the employment it provides, delivers economic benefit to the town which is another key objective of the Enfield Town Framework and would be in keeping with Core Policy 13 (Promoting Economic Prosperity), Core Policy 17 (Town centres).

9.11 The focus of current policy is on the optimisation of development especially in town centres with emphasis on maximising height where appropriate. The mix of uses and current height is appropriate due to the relationship to the Conservation Area and No 22 Genotin Road. The absence of any residential component is not a reason for refusal in itself and weight has been given to the particular requirements of the Applicant who wish to provide a bespoke headquarters building amalgamating a number of office locations across Enfield Town. This would fulfil the objective of the Enfield Town Framework Masterplan to make a positive contribution to the town centre and to ensure the height of the building is appropriate having regard to the Conservation Area and No 22 Genotin Road.

9.12 The Applicant has confirmed that additional floors could structurally be provided which enables future residential development should circumstances arise. However in the interim, it is considered the benefits of retaining the Applicant and the 360 existing jobs

(with the potential to rise to 500) within Enfield Town outweigh this aspect of the development.

- 9.13 The Enfield Town Framework Masterplan states that redevelopment of the car park site should only proceed once alternative public parking arrangements are confirmed and in place. The loss of the existing well used car park is obviously a key consideration but the development provides 96 spaces for public use during evening and weekend periods. Analysis done to support the Enfield Town Framework Masterplan demonstrates that the number of available parking spaces to serve the town is adequate.
- 9.14 Taking the above factors into account, it is considered that the underlying principle of the development and its compatibility with the objectives of the Enfield Town Framework Masterplan is acceptable.

Character and Appearance

- 9.15 This is a prominent site within Enfield Town and the first of the development sites identified in the adopted Enfield Town Framework Masterplan, to come forward. The Masterplan highlights the site as an opportunity to introduce a landmark building whilst respecting the setting of the Conservation Area and the surrounding area.

Height

- 9.16 Current and emerging policy encourages the optimisation of development on town centre sites. This has been reflected in the comments of officers and was also highlighted in the comments from the Council's Place & Design Review Panel. The intent of the Applicant is to create a single headquarters building primarily, for its sole purpose. The quantum of development therefore reflects this requirement. The proposed 4-5 storey building is built to commercial standards and thus is equivalent to a 6-7 metre high residential block. The option for an additional 3 floors to be constructed in the future has been confirmed, a building of this height is not unacceptable and would respect both the conservation area, the Dugdale Centre and the height of No 22 Genotin Road.
- 9.17 It is also recognised that the proposed height provides a more integrated relationship to the existing site circumstances and particularly, the 3 storey form of 22 Genotin Road. Accordingly, the height of the current development is considered acceptable.

Design / Form / Massing

- 9.18 The objective of the NPPF and adopted policy is to achieve high quality design in all developments with schemes being design led from the outset. The proposed development has sought to respond to the opportunities and constraints of this site. The comments of the Place & Design Review Panel and the Applicants response are attached as Appendix 1 to this report. Design changes have occurred to the scheme in response to these comments however, the specific requirements of the Applicant have precluded significant revision. However, importantly, the width of the future pedestrian link along the southern boundary has been increased from 3.45 metres to 5 metres along its entire length. This involved significant internal redesign / reconfiguration to meet this key masterplan objective. As a result, the development now provides an acceptable relationship to the southern boundary and the potential future development of the St Anne's playing field: a key objective of the Enfield Town Framework Masterplan.
- 9.19 The proposed development occupies a significant proportion of the developable area of the site with the exception of that required for access, servicing and parking. This is a product of the operational requirement for large internal office floor plates to support Metaswitch's preference for an open plan / close working environment. It is this client side requirement that has largely dictated the form of the proposed development.

- 9.20 The proposed building is divided into three main elements that relate to the intended internal functions of the building: two office elements linked by a glazed atrium. This latter feature is predominantly glazed so as to activate the building and both “office” elements contain substantial glazing to allow views into the active ground floor uses, the atrium and rooftop café.
- 9.21 The building is designed with a “floating” roof element which projects over the atrium and extends forwards over the main entrance to the building.. This accentuates the entrance and provides greater visual interest.
- 9.22 In response to the curve in Genotin Road, the proposed building steps out on the southern edge. In so doing, the built form responds to the road layout and provides greater articular and interest for the building.
- 9.23 A 5 metre wide strip along the southern boundary has now been identified and will be secured through the legal agreement. This is a key objective of the Enfield Town Framework Masterplan along the southern boundary and would be replicated should the St Anne’s playing field come forward for redevelopment. This would then facilitate a minimum 10 metre wide pedestrian east / west link between the Town and land to the east of the railway identified as a potential development site i.e. the Tesco site.
- 9.24 The front four/five storey building (top storey recessed) would not be out of scale with the varying heights of buildings in close proximity to the site, such as the Dugdale Centre. As a gateway building into the conservation area, the development is of good quality and the use of high quality materials and finishes (to be secured by Conditions 9 & 10) would enable the buildings to blend effectively with and enhance the appearance of the area. In conclusion, the design of the scheme is such that it has significant merit and the proposal is considered to accord with the design guidelines set out with the Enfield Town Framework Masterplan and complies with London Plan policies 7.4 and 7.6 and with DMD Policy 37.

Relationship to the railway

- 9.25 The application site shares the eastern boundary with land owned by Network Rail/Transport for London. The existing boundary treatment will be unaffected by the proposal.
- 9.26 The building will be visible by those entering Enfield Town Station by train and the building design has improved in response to officer’s guidance with the submission of revised plans to ensure that the eastern elevation is strong and interesting. Moreover, with the use of glazing and solid panels, this elevation would have visual interest.
- 9.27 RfL guidelines have also informed the approach to ground levels within the site and the areas where excavation to form the basement car parking level, could occur to ensure surface water and ground water do not run off and affect the railway.

Materials

- 9.28 In response to comments on the specification for materials, officers have been in discussions to ensure the materials uses are appropriate. As a result, a palette of materials has been identified which will deliver a contemporary building whilst respecting relationship with the adjacent Conservation Area.
- 9.29 Condition 10 secures the approval of samples to be viewed on site.

Secure by Design

- 9.30 In response to concerns raised by Members at the last meeting, the Applicant has met with Secure by Design Officer at the Metropolitan Police and provided additional information and clarification regarding the mitigation to be incorporated into the design. In the light of this, the Secure by Design officer has confirmed there is no objection on this ground subject to a condition confirming the attainment of a Secured by Design accreditation
- 9.31 Conditions 22 & 23 in the schedule of conditions are Section 14 addresses this requirement.

Relationship to 22 Genotin Road

- 9.32 The side elevation of 22 Genotin Road has 12 windows across three floors. These windows are mainly non-habitable rooms. However, where they do serve a habitable room e.g. a living room, the window is secondary and not the main light source. The internal layout of the proposed development has been revised to reduce the amount of glazing on the northern elevation facing 22 Genotin Road by locating the stairwell (used principally for escape in a fire) and meeting rooms opposite the side elevation of No 22. This means that the use of the internal areas limits the degree to which overlooking could occur. These arrangements will acceptably minimise the any impact for the occupiers living at 22 Genotin Road.
- 9.33 The proposal only extends to four storeys on the northern side (as opposed to five on the southern side), thus allowing for an increased amount of light to reach 22 Genotin Road's side elevation with the site. The acceptability of this relationship is supported by a Daylight and Sunlight Analysis which concludes outlook and light levels are appropriate. In respect of outlook from No 22 Genotin Road the proposal demonstrates a very high degree of compliance. and in terms of the VSC (Vertical Sky Component), only 6 of the 12 windows facing the site fell below the guidelines and of these, 4 serve small kitchens, which can be given less weight in accordance with BRE guidelines.
- 9.34 The APSH (Annual Probable Sunlight Hours) assessment shows that 16 out of the 20 windows meet the BRE guidance. In terms of the 4 windows which fall below the guidance the relevant kitchens have other windows that supplement the sunlight level received by the properties to produce results that exceed the BRE guidelines.
- 9.35 The other neighbouring sites, 2 Genotin Terrace, 40-46 London Road, St Anne's Catholic High School given their distance from the development, also meet the required guidelines based on the summaries set out the applicant's daylight and sunlight report.
- 9.36 It is concluded that the proposed development is satisfactory in terms of its daylight and sunlight impacts or on the amenities of these residential properties.

Relationship to Other Nearby Properties

- 9.37 The elevation is separated from the rear of properties on Chalkwell Park Avenue by the playing fields used by St Anne's School. The properties on Chalkwell Park Avenue are also circa 90 metres away. No impact on residential amenity is considered to arise.

Heritage Considerations

Statutory background

- 9.38 Sections 66 and 72 of the Planning (Listed Buildings and Conservation Areas) Act 1990 (“Listed Buildings Act”) confirm that special attention shall be paid to preserving or enhancing the character or appearance of that area (s.72). As confirmed by the Court of Appeal (Civil Division), the decision in *Barnwell Manor Wind Energy Ltd v East Northamptonshire District Council* [2014] EWCA Civ 137, it was concluded that where an authority finds that a development proposal would harm the character and appearance of a conservation area, it must give that harm “considerable importance and weight”.

National Guidance

- 9.39 Section 16 of the revised National Planning Policy Framework (“Conserving and enhancing the historic environment”) advises Local Planning Authorities to recognise heritage assets as an “irreplaceable resource” and to “conserve them in a manner appropriate to their significance” (paras.189/190). The “Framework” advises that LPAs need to consider whether a proposed development will lead to substantial harm to or total loss of significance of a designated heritage asset. Proposals that lead to substantial harm to or a total loss of significance of a designated heritage asset should be refused unless it can be demonstrated that the substantial harm or loss is necessary to achieve substantial public benefits that outweigh the harm or loss, or it meets with the test identified at paragraph 195. Where a development will lead to less than substantial harm, the harm is to be weighed against the public benefits of the proposal, including securing its optimum viable use (Para 196).
- 9.40 London Plan policy 7.8 (“Heritage Assets and Archaeology”) advises what boroughs should do at a strategic level to identify, preserve, and enhance London’s heritage assets. Policy CP31 (“Built and Landscape Heritage”) of the Core Strategy sets out a requirement that development should conserve and enhance designated and non-designated heritage assets. Policy DMD44 (“Conserving and Enhancing Heritage Assets”) states that development which fails to conserve and enhance the special interest, significance or setting of a heritage asset will be refused. The design, materials and detailing of development affecting heritage assets or their setting should conserve the asset in a manner appropriate to its significance.
- 9.41 The heritage assets upon which the impact of the development should be considered against are the Enfield Town Conservation Area. What must therefore be determined is whether any of the elements proposed will harm the significance of the Conservation Area having regard to the statutory requirement to give special attention to the desirability of preserving or enhancing its character or appearance. If any harm is identified, great weight must be given to that harm. Further to this, as advised above, if substantial harm or total loss to significance is identified, it would need to be established whether there are any substantial public benefits that would outweigh the identified harm or loss or the tests identified at para.200 of the NPPF are met. If there is less than substantial harm, the harm is to be weighed against the public benefits of the proposal, a balanced judgement must be made having regard to the scale of any harm or loss and the significance of the heritage asset. It should be noted that benefits are not limited to heritage benefits but to all material planning benefits capable of meeting the policy tests.

Analysis

- 9.42 The site lies outside of but immediately adjacent to the boundary with Enfield Town Conservation Area. The nearest buildings are those located at the southern end of London Road. These are identified as making a positive contribution to the character of the conservation area. However, the proposed development faces to the rear of these properties, the elevations of which are of less quality. They are also some distance from the rear of these buildings and it is noted the Character Appraisal does not identify any key views which would be affected by the development.

- 9.43 Taking into account therefore the relationship of the site and the proposed development to the Conservation Area associated with the development, it is considered the proposed building would have less than substantial harm to the character, appearance and setting of the Conservation Area. However given the designation in the adopted Enfield Town Framework Masterplan and the economics and business benefits for viability of Enfield Town it is considered there are significant public benefits to the proposed development which outweigh this less than substantial harm.

SuDS

- 9.44 Revised details have now been submitted and considered by the SuDs team. As result, the SuDs officer has confirmed there is no objection and that the proposals now comply with the Council's policy in terms of addressing surface water run off within the application site.

Condition 7 & 8 require the agreed strategy to be implemented.

9.45 **Transport**

Loss of Existing Surface Car Park

- 9.46 While the development would result in the loss of the existing 122 space open car park , the proposal involves the re-provision of 96 spaces. Other multi storey car parks are still available within the town centre and the Council's own analysis show they meet expected demand levels. In addition, the town centres benefits from good accessibility to a range of public transport options (PTAL 5). It should also be noted that car parking spaces would still be available to the public during evening and weekend periods. As a result, it is considered there would be no adverse impact on the vitality and viability of the town centre

Proposed Re-provision / Level of Car Parking

- 9.47 Parking provision would consist of 96 spaces of which 22 spaces would be located on ground level and 74 located at basement level.
- 9.48 This equates to 31 car parking spaces more than would normally be considered necessary in accordance with London Plan policy. However, TfL have confirmed that have no objection to this level of provision in light of their policy. Consequently taking an overall perspective, it is considered there are exceptional and specific circumstances in this case to justify the policy exception. In particular, the amalgamation of existing offices within the town centre is noted and weight has been given to the current levels of car use within the existing workforce which the applicant identifies as being important to the retention of key staff. Furthermore, Metaswitch have provided details of commuting methods by staff within their existing offices and concluded the number of spaces proposed as a minimum requirement to retain those staff.
- 9.49 It should also be noted that Policy T6.2 of the emerging London Plan accepts that within outer London's boroughs, more generous standards can be applied where this can be justified and where future measures through a travel plan seek to limit the use of car parking. A Travel Plan forms part of the package of measures including cycle parking and electric charging points is proposed by the Applicant and no additional car parking is proposed when the number of employees expands. It should also be noted that should be appropriate at some point in the future, the car park area could be converted into alternative commercial / leisure uses.

- 9.50 Eight disabled spaces have been proposed at ground level (which is 3 more than required by policy , with disability lift access. In addition, 20 electric vehicle charging points from the outset would be provided and a further 10 bays will be provided
- 9.51 On balance it is considered the approach to car parking is appropriate.

Cycle Provision

- 9.52 Fifty four cycle spaces have been proposed across two separate levels . The cycle storage within the basement is secure and will provide a safe environment with direct lift access to the showers and changing facilities within the offices.
- 9.53 For cyclists who choose not to use the basement car park ramps – lift accesses are provided from the secondary entrance at the rear.

Highways and servicing

- 9.54 Diagrams have been submitted which demonstrate access width and the servicing area layout are adequate for larger vehicle manoeuvring, the service bay sited near the entrance of the site is useable still allowing and that the bin storage arrangements are practical and appropriate. A Delivery and Services Strategy is recommended as a planning condition – see Condition 19 in the schedule of conditions at Section 14.
- 9.55 For emergency services, adequate turning would be available for a fire engine to turn and move in a forward gear direction. Internal details would need to meet the requirements of Building Control standards which include sprinklers but this is a separate process which would follow the approval of any planning scheme. No condition is necessary.

Sustainable Construction & Energy

- 9.56 An Energy and Sustainability Statement is submitted with this planning application and provides details regarding the credentials of the building in terms of its sustainability performance. The proposed development will be BREEAM Excellent and will comply with relevant polices.
- 9.57 Energy efficiency is built into the design via passive design and efficiency measures, as detailed within section 5.2 of the Energy and Sustainability Statement. Policy GG6, G7, SI4, SI5, of the London Plan 2018, Core Strategy Policy 20 are satisfied here in addition to the Development Management Document Policies on ‘tackling climate change.’
- 9.58 Sustainability has been a consideration in the design process, from the layout and orientation, (and use of materials to reduce solar gain), through to the inclusion of a roof zone (where plant is not positioned) to accommodate potential photovoltaic panels and/or a ‘green’ roof.
- 9.59 The location of the site lends itself to the principles of sustainability too- being centrally located to minimise travel for existing Enfield residents who are employed by Metaswitch, and being located within walking distance of Enfield Town rail station and the bus route that is located on Genotin Road. The site is also previously developed land. In addition, in terms of the sustainable management of waste, a Site Waste Management Plan accompanies this planning application.

Ecology / Trees

- 9.60 The trees sited on the southern elevation sited on the neighbouring site of the playing fields, do not have a Tree Preservation Order nor are they within the Conservation Area. No consent is therefore required for their removal. It should also be noted the playing field

is also identified as a development site. Consequently, no objection is raised on this ground.

- 9.61 Opportunities for landscaping within the site and along the southern boundary have been maximised including proposals to improve the public realm. As a result the scheme is considered acceptable on this ground.

10. Environmental Impact Assessment

- 10.1 Having considered the proposed development within the context of the Town and Country Planning (Environmental Impact Assessment) Regulations 2017, it is considered that the planning application is not an EIA development. The proposals are not a Schedule 1 project with regards to the relevant criteria and threshold under the Regulations. As an urban area project, it is considered within Schedule 2. The site area of the proposed development does not exceed 0.5 ha which is the threshold in Column 2 of Schedule 2 for considering whether such projects are EIA. It is stated within the Planning Practice Guidance that only a very small proportion of Schedule 2 development will require EIA. The development is not within a Sensitive Area as defined under the 'Interpretation' (Regulation 2) of the Regulations.

11.0 Planning Obligations

- 11.1 A draft S106 legal agreement is being prepared and will include the following Draft Heads of Terms:
- Skills & Training
 - Future Availability of Car Park for Public Use
 - Safeguarding of land to the south of the building within the application site – to be annotated on a drawing within the Section 106 Agreement, and subject to a no build zone
 - Future Access to development the Playing Fields – to be annotated on a drawing within the Section 106 Agreement, and subject to a no build zone
 - Off Site Highway Improvements to be discussed, agreed and to be secured through a S278 Agreement

12.0 Community Infrastructure Levy (CIL)

- 12.1 The Mayoral CIL is collected by the Council on behalf of the Mayor of London. The amount that is sought for the scheme is calculated on the net increase of gross internal floor area multiplied by an Outer London weighting (£20/sqm) and a monthly indexation figure. The development is CIL liable for the construction of 6,552 sqm of new commercial floor space.
- 12.2 The Council introduced its own CIL on 1 April 2016. The money collected from the levy (Regulation 123 Infrastructure List) will fund rail and causeway infrastructure for Meridian Water. For CIL, the site falls within the "All Other Uses", which specifically includes offices, for which the Council CIL is charged at £0/sqm.

13.0 Conclusion

- 13.1 The application proposal requires an evaluation of the impact of the net reduction of 26 in car parking spaces and availability within Enfield Town, and the introduction of a significant built mass on the edge of a conservation area against the positives of retaining a significant local employer within the town and creating a modern office facility for Enfield Town Centre within a contemporary building that helps to complete the built fabric that is consistent with the growth agenda.

13.2 Officers have given significant weight to:

- i) the economic benefits to the local economy linked to the retention of Metaswitch in the town centre;
- ii) the provision of an appropriate level of car parking that meets both the needs of the applicant and the town centre;
- iii) the compatibility of the proposed mixed use scheme with the objectives of the Enfield Town Framework Masterplan;
- iv) the scheme not prejudicing the ability of adjacent development sites identified in the Enfield Town Framework Masterplan from coming forward at some future date;
- v) the acceptability of the development in terms of its siting, scale and design relative to the character and appearance of the Enfield Town Conservation Area;
- vi) the acceptability of the development in terms of its siting, scale and design to safeguard the amenities of the properties in the adjacent residential block of 22 Genotin Road.

13.3 It must be acknowledged that there are differing opinions on this scheme but it is important to balance all planning considerations when determining its acceptability. It is considered that in this case any dis-benefits are outweighed by the positives and with this in mind, Members should note:

- i) The principle of this office led mixed use development is acceptable for this location;
- ii) The development would contribute to the vitality and viability of Enfield Town;
- iii) The development provides an acceptable level of car parking that meets both the needs of the applicant and the town centre;
- iv) The development is fully consistent with the objectives of the adopted Enfield Town Framework Masterplan;
- v) The proposal does not prejudice the ability of adjacent development sites identified in the Enfield Town Framework Masterplan to be developed;
- vi) The acceptability of the development in terms of its siting, scale and design relative to the character and appearance of the Enfield Town Conservation Area;
- vii) The development does not prejudice the amenities of the neighbouring residential properties at No 22 Genotin Road;
- viii) Issues of sustainability, energy and SuDs are controlled by condition.

13.4 Having regard to all of the above, on balance, it is considered that the proposal constitutes a well designed regeneration scheme with significant benefits for the town centre and wider Borough:

14.0 **Recommendation**

14.1 That subject to the completion of a S106 Agreement to secure the obligations as set out in Section 8.60 of this report, the Head of Development Management / the Planning Decisions Manager be authorised to GRANT planning permission subject to the conditions as set out below:

3 Years

1. The development to which this permission relates must be begun not later than the expiration of three years beginning with the date of the decision notice.

Reason: To comply with the provisions of S.51 of the Planning & Compulsory Purchase Act 2004.

Approved Plans

2. The development hereby permitted shall be carried out in accordance with the approved plans:

7486-BDP-00-XX-DR-A-0001 S2 P03 (Existing Site Location Plan); 7486-BDP-00-XX-DR-A-0003 S2 P04 (Existing Site Plan); 7486-BDP-00-XX-DR-A-0005 S2 P04 (revised) (Proposed Site Plan); 7486-BDP-00-00-DR-A-0010 S2 P08 (revised) (Proposed Ground Floor Plan); 7486-BDP-00-01-DR-A-0011 S2 P07 (Proposed First Floor Plan); 7486-BDP-00-02-DR-A-0012 S2 P07 (Proposed Second Floor Plan); 7486-BDP-00-03-DR-A-0013 S2 P07 (Proposed Third Floor Plan); 7486-BDP-00-04-DR-A-0014 S2 P07 (Proposed Fourth Floor Plan); 7486-BDP-00-05-DR-A-0015 S2 P07 (Proposed Roof Plan); 7486-BDP-00-B1-DR-A-0009 S2 P08 (revised) (Proposed Basement Plan); 7486-BDP-00-XX-DR-A-0007 S2 P03 (Tree Removal Plan); 7486-BDP-00-ZZ-DR-A-0020 S2 P08 (revised) (Proposed GA Elevations Sheet 1); 7486-BDP-00-ZZ-DR-A-0020 S2 P08 (revised) (Proposed GA Elevations Sheet 2); 7486-BDP-00-ZZ-DR-A-0027 S2 P03 (Proposed GA Sections); 7486-BDP-00-ZZ-DR-A-0025 S2 P04 (Proposed GA Sections Sheet 1); 7486-BDP-00-ZZ-DR-A-0025 S2 P04 (Proposed GA Sections Sheet 2); AVR3 dated 16 July 2018 (Accurate Visual Representations); 661 P03 Rev B (Proposed Landscape Schematic); CCL 10003 Rev 4 (Tree Protection Plan); GRE-BWB-GEN-01-DR-TR-100 S2 P4 (Proposed Junction General Arrangements); GRE-BWB-GEN-01-DR-TR-110 S2 P4 (Swept Path Analysis: Refuse Vehicle Access & Egress); GRE-BWB-GEN-01-DR-TR-111 S2 P4 (Swept Path Analysis: Large Car Two-way Movement); 16-6912-SK005-P1 (Revised) Drainage Areas; 16-6912-SK004-P3 (Revised) (Access Road Design); S2 P01 (Design and Access Statement); 16-6912-FRA Issue 4 (Flood Risk Assessment and Drainage Strategy); 16-6912-DMP Issue 1 (Drainage Maintenance Plan); BSA 1821_1c 260718 (Archaeology and Heritage Statement); Energy and Sustainability Statement (Rev 06 dated 26 July 2018); BREEAM Pre-Assessment (Rev 04 dated 26 July 2018); ESS0142 Rev A (Preliminary Ecological Appraisal); Arboricultural Report (10003 dated 24 July 2018); Tree Schedule (10003); GRE-BWB-GEN-XX-RP-TR-001 S2 P3 (Transport Assessment); GRE-BWB-GEN-XX-RP-TR-002 S2 P4 (Revised) (Travel Plan); STOQ3007 July 2018 (Site Waste Management Plan); REP-1010453-05-AM-20180514 Rev 3 (dated 23 May 2018) (Noise Assessment); 16-16152-20180726 (Environmental Lighting Assessment); Planning Statement (July 2018); Ground Conditions Report (27039 dated 6 July 2018); Landscape Report (Dated 11th July 2018); Daylight / Sunlight report (ROL 00027 dated 6 July 2018); REV A – 06.09.2018 (Revised) (Signage options).

Reason: For the avoidance of doubt and in the interests of proper planning.

Construction Management Plan

3. That development shall not commence until a construction methodology has been submitted to and approved in writing by the Local Planning Authority. The construction methodology shall contain:
- a. Arrangements for wheel cleaning;
 - b. Arrangements for the storage of materials;
 - c. Hours of work;
 - d. Arrangements for the securing of the site during construction;
 - e. The arrangement for the parking of contractors' vehicles clear of the highway;

- f. The siting and design of any ancillary structures;
- g. Arrangements for the loading and unloading of plant and materials;
- h. Scheme for recycling/disposing of waste resulting from demolition and construction works;
- i. Enclosure hoarding details; and
- j. Measures that will be taken to control dust, noise and other environmental impacts of the development in accordance with 'London Best Practice Guidance: The control of dust and emission from construction and demolition'.

The development shall be carried out in accordance with the approved construction methodology unless otherwise agreed in writing by the Local Planning Authority.

Reason: To ensure the implementation of the development does not lead to damage to the existing highway and to minimise disruption to neighbouring properties and the environment.

Contamination

- 4. Prior to the commencement of building works, a scheme to deal with the risks associated with contamination of the site shall be submitted to and approved, in writing, by the local planning authority. That scheme shall include all of the following elements unless specifically excluded, in writing, by the Local Planning Authority and the development shall then proceed in strict accordance with the measures approved.
 - a. A desk study identifying: all previous uses; potential contaminants associated with those uses; a conceptual model of the site indicating sources, pathways and receptors; potentially unacceptable risks arising from contamination at the site;
 - b. Site investigation scheme, based on (1) to provide information for an assessment of the risk to all receptors that may be affected, including those off site;
 - c. The results of the site investigation and risk assessment (2) and a method statement based on those results giving full details of the remediation measures required and how they are to be undertaken and
 - d. A verification report on completion of the works set out in (3) confirming the remediation measures that have been undertaken in accordance with the method statement and setting out measures for maintenance, further monitoring and reporting.

Any changes to these agreed elements require the express consent of the Local Planning Authority.

Reason: In order to ensure that the development does not pose an unacceptable risk to the quality of the groundwater.

Piling

- 5. No piling shall take place until a piling method statement (detailing the depth and type of piling to be undertaken and the methodology by which such piling will be carried out, including measures to prevent and minimise the potential for damage to subsurface sewerage infrastructure, and the programme for the works) has been submitted to and approved in writing by the local planning

authority in consultation with Thames Water. Any piling must be undertaken in accordance with the terms of the approved piling method statement.

Reason: The proposed works will be in close proximity to underground sewerage utility infrastructure. Piling has the potential to impact on local underground sewerage utility infrastructure.

Crane lifting management plan

6. Prior to the erection or dismantling of any cranes and other lifting equipment required during the construction of the development, details shall be submitted to and approved in writing by the Local Planning Authority. The details shall include a crane /lifting management plan including crane base design (and certification), Risk Assessment and Method Statement for siting, erection, lifting arrangements, operational procedure (including any radio communications), jacking up and/or, de-rigging in addition to plans for elevation, loads, radius, slew restrictions and collapse radius.

No cranes shall be erected or dismantled until RfL's approval has been obtained in writing. Once this has been approved by Rail for London this would need to be submitted to the Local Planning Authority.

Reason: To ensure that the lifting operations are carried out safely in compliance with BSI standards, and to prevent anything falling on to the adjacent railway, compromising the safety of the railway.

Sustainable Drainage Strategy

- 7 Notwithstanding the details set out in the submitted Preliminary Drainage Strategy (Drawings A2071-1500 – 1503 P3, and A2071-1504 P1), September 2016, prior to the commencement of any construction work, completion of the building frame, details of the Sustainable Drainage Strategy shall be submitted to and approved in writing by the Local Planning Authority and must conform with the Landscaping Strategy. The details shall include:

- a) Sizes, storage volumes, cross-sections, long-sections (where appropriate) and specifications of all the source control SuDS measures including, green roofs, swale and permeable paving;
- b) Final sizes, storage volumes, invert levels, cross-sections and specifications of all site control SuDS measures including underground tanks.
- c) Include calculations demonstrating functionality where relevant
- d) Demonstration that the system is designed to allow for flows that exceed the design capacity to be stored on site or conveyed off-site with minimum impact
- e) A management plan for future maintenance

Reason: To ensure the sustainable management of water, minimise flood risk, minimise discharge of surface water outside of the curtilage of the property and ensure that the drainage system will remain functional throughout the lifetime of the development in accordance with Policy CP28 of the Core Strategy and Policies 5.12 & 5.13 of the London Plan and the NPPF and to maximise opportunities for sustainable development, improve water quality, biodiversity, local amenity and recreation value.

SuDs

8. Prior to occupation of the development, a Verification Report demonstrating that the approved drainage/SuDS measures have been fully implemented shall be submitted to the Local Planning Authority for approval in writing. This report must include:
- a. As built drawings of the sustainable drainage systems;
 - b. Level surveys of completed works;
 - c. Photographs of the completed sustainable drainage systems;
 - d. Any relevant certificates from manufacturers/ suppliers of any drainage features;
 - e. A confirmation statement of the above signed by a chartered engineer.

Reason: To ensure the sustainable management of water, minimise flood risk, minimise discharge of surface water outside of the curtilage of the property and ensure that the drainage system will remain functional throughout the lifetime of the development in accordance with Policy CP28 of the Core Strategy and Policies 5.12 & 5.13 of the London Plan and the NPPF.

Detailed Drawings

9. Prior to completion of the building frame, detailed drawings to a scale of 1:20 to confirm the detailed design and materials of the:
- a. Schedule and sample of materials used in all elevations;
 - b. Construction details of all external elements at 1:20 scale (including sections). This should include: entrances and exits, glazing, masonry, weathering and flashings, balustrades and parapets, roof, plant and plant screening, health and safety systems;

Shall be submitted to and approved in writing by the Local Planning Authority prior to the commencement of development above ground hereby permitted. The development shall thereafter be carried out solely in accordance with the approved details.

Reason: To safeguard and enhance the visual amenities of the locality.

Samples and Materials

10. Prior to the commencement of building works above ground, a sample panel and a schedule of materials to be used in all external elevations including walls, doors, windows front entrances and balconies within the development hereby permitted shall be submitted to and approved in writing by the Local Planning Authority before any building work commences and this condition shall apply notwithstanding any indications as to these matters which have been given in the application. The development shall thereafter be carried out solely in accordance with the approved details.

Reason: In order to ensure that the building has an acceptable external appearance and preserves the character and appearance of the conservation area.

Surfacing Materials

11. Prior to the completion of the external building works, details and design of the surfacing materials to be used within the development including footpaths, shared surfaces, access roads, parking areas, road markings and all other hard surfacing shall be submitted to and approved in writing by the Local Planning Authority. The surfacing shall be carried out in accordance with the approved detail before the development is occupied or use commences.

Reason: To ensure that the development does not prejudice highway safety and a satisfactory appearance.

Public Realm

12. Prior to the completion of the external building works, a Landscape and Public Realm Strategy for all external public realm areas within the curtilage of the site hereby approved shall be submitted to and approved by the Local Planning Authority. This Strategy is to include, amongst other things, details of proposed plant and tree maintenance, paving materials, pedestrian priority materials and shared surface treatments, plant species, ground levels, green roofs, green walls, boundary treatments and water features. The development shall be in accordance with the approved details, unless otherwise agreed in writing by the Local Planning Authority.

Reason: To ensure that the proposed landscaping areas are of a high quality and for consistent treatment of the public realm.

Soft Landscaping

13. Prior to the completion of the external building works, details of trees, shrubs, grass and all other soft landscaped areas of internal and external amenity spaces to be planted on the site shall be submitted to and approved in writing by the Local Planning Authority. The planting scheme shall be carried out in accordance with the approved details in the first planting season after completion or occupation of the development whichever is the sooner. Any planting which dies, becomes severely damaged or diseased within five years of planting shall be replaced with new planting in accordance with the approved details.

Reason: To provide a satisfactory appearance and ensure that the development does not prejudice highway safety.

Refuse Storage

14. Prior to occupation of each phase of the development, details of refuse storage facilities including facilities for the recycling of waste to be provided within the development, in accordance with the London Borough of Enfield Waste and Recycling Planning Storage Guidance ENV 08/162, shall be submitted and approved in writing by the Local Planning Authority. The facilities shall be provided in accordance with the approved details before the development is occupied or use commences.

Reason: In the interests of amenity and the recycling of waste materials in support of the Boroughs waste reduction targets.

Cycle Parking

15. Prior to occupation of each phase of the development, details of the siting, number and design of secure/covered cycle parking spaces shall be submitted and approved in writing by the Local Planning Authority. The

approved details shall thereafter be installed and permanently retained for cycle parking.

Reason: To ensure the provision of cycle parking spaces in line with the Council's adopted standards.

Energy Statement

16. The development hereby approved shall be constructed in accordance with the approved detailed 'Energy Statement' and relevant SAP calculations dated July 2018.

Reason: In the interest of sustainable development and to ensure that the Local Planning Authority may be satisfied that CO2 emission reduction targets are met in accordance with Policy CP20 of the Core Strategy, Policies 5.2, 5.3, 5.7 & 5.9 of the London Plan 2011 and the NPPF.

Energy Performance Certificate

- 17 Following practical completion of the development, a final Energy Performance Certificate shall be submitted to an approved in writing by the Local Planning Authority prior to occupation of the development.

Reason: In the interest of sustainable development and to ensure that the Local Planning Authority may be satisfied that CO2 emission reduction targets are met in accordance with Policy CP20 of the Core Strategy, Policies 5.2, 5.3, 5.7 & 5.9 of the London Plan 2011 and the NPPF.

No Pipes

18. No pipes or vents (including gas mains and boiler flues) shall be constructed on the external elevations unless they have first been submitted to the Local Planning Authority and approved in writing. Any pipes and vents shall be installed as approved.

Reason: Such works would detract from the appearance of the building and would be detrimental to the visual amenities of the locality.

Servicing Management Plan

19. Prior to occupation of the development, full details of a servicing management strategy for the management of deliveries and servicing of the development, shall be submitted to and approved by the Local Planning Authority. Servicing shall thereafter be carried out solely in accordance with the approved details.

Reason: In order that the Local Planning Authority and Transport for London may be satisfied as to the effects of the scheme on the adjacent road network so as to avoid hazard or obstruction to the public highways.

Travel Plan

20. The measures approved in the approved Travel Plan (July 2018) shall be implemented within 3 months of occupation of the commercial use hereby permitted and shall be so maintained for the duration of the use, unless the prior written approval of the Local Planning Authority is obtained to any variation.

Reason: To ensure that the travel arrangements to the residential development are appropriate and to limit the effects of the increase in travel movements.

Hours of Operation

21. The commercial (B1a) premises, conference facility (D1) and café (A3) hereby approved shall not be open to the public except between the hours of 08.00 to 23.00 Monday to Saturday and between 10.00 and 17.00 hours on Sundays and Bank or Public Holidays. The premises shall not be open at any other time except with the prior agreement in writing of the Local Planning Authority.

Reason: To safeguard the amenities of neighbouring residential occupiers.

Crime Prevention Strategy

22. Notwithstanding the details of the development, hereby approved, a detailed crime prevention management and maintenance strategy detailing how the development will minimise opportunities for crime including details of a controlled access system, CCTV and external lighting shall be submitted to and approved in writing by the Local Planning Authority prior to the first occupation of the development.

Reason: To ensure that the development protects community safety.

23. The building shall obtain a 'Secured by Design' accreditation within three months of occupation. This accreditation certificate shall be copied to the local planning authority and the development shall only be carried out in accordance with the approved details and maintained permanently thereafter.

Reason: To ensure that the development protects community safety.

24. Prior to the public use of the car park area hereby permitted, the car park will retain a Park Mark accreditation, and;

Reason: To ensure that the development protects community safety.

Car Park Management Plan

25. A Car Park Management Plan shall be submitted to and approved in writing by the Local Authority

Reason: To ensure that the development protects community safety.

External Lighting

26. Prior to the completion of the external building works of development, details of any external lighting proposed shall be submitted and approved in writing by the Local Planning Authority. The approved external lighting shall be provided before the development is occupied.

Reason: To ensure that the development does not prejudice the amenities of adjoining occupiers and / or the visual amenities of the surrounding area.

Parking Strategy

27. Prior to the completion of the external building works, details of a Parking strategy to include Layouts, Temporary bays and Electric Vehicle Charging Points in accordance with Draft/London Plan and standards adopted by the Local Planning Authority shall be submitted to and approved in writing by the Local Planning Authority. The facilities shall be constructed in accordance with the approved details before the development is occupied and shall be maintained for this purpose.

Reason: To ensure that the development complies with Development Plan Policies and does not prejudice conditions of safety or traffic flow on adjoining highways.

Biodiverse roofs

28. Details of the proposed biodiverse roofs shall be provided to the Local Planning Authority for approval in writing have been provided to the Local Planning Authority for approval in writing demonstrating the feasibility or otherwise of providing a biodiverse green / brown roof. The submitted detail shall include location, design, substrate (extensive substrate base with a minimum depth 80-150mm), vegetation mix and density, and a cross-section of the proposed roof.

The biodiverse roof shall not be used for any recreational purpose and access shall only be for the purposes of the maintenance and repair or means of emergency escape.

The biodiverse roof shall be implemented in accordance with the approved details prior to first occupation and maintained as such thereafter. Photographic evidence of installation is to be submitted and approved in writing by the council.

Reason: To assist in flood attenuation and to ensure the development provides the maximum possible provision towards the creation of habitats and valuable areas for biodiversity in accordance with adopted Policy.

Details of a mechanical ventilation strategy

28. Prior to completion of the building frame details of a mechanical ventilation strategy shall be submitted to and approved in writing by the Local Planning Authority. All accepted recommendations must be fully implemented within the relevant phase prior to first occupation and maintained thereafter.

Reason: To ensure adequate ventilation whilst not unduly impacting on the amenity of residents.

Noise attenuation for plant

29. Prior to completion of the building frame an acoustic report shall be submitted to the Local Planning Authority for approval in writing detailing the sound level generated from the kitchen extraction system and state the noise control measures to be employed to ensure the noise from the system does not exceed a level of 10dB(A) below the typical background noise level measured as L(A)₉₀ 15 minutes during operational hours, at the façade of the nearest residential property.

Reason: To protect residents from noise and disturbance in accordance with adopted policy.

Number / location / design of electric vehicle charging points

30. Prior to the completion of the building frame typical details, including siting and design of plugs, of electric vehicular charging points to be provided in accordance with London Plan standards (minimum 20% of spaces to be provided with electric charging points and a further 20% passive provision for electric vehicles in the future) shall be provided to the Local Planning Authority for approval in writing.

All electric charging points shall be installed in accordance with the approved details prior to first occupation of the relevant phase of the development approved and permanently maintained and retained.

Reason: To ensure that the development complies with the sustainable development policy requirements of the London Plan.

Details for communal satellite dish / TV antenna provision

34. Prior to the completion of the building frame, details for the provision of communal television systems/satellite dishes shall be submitted to an approved in writing by the Local Planning Authority. The development shall only be undertaken in accordance with the approved detail and implemented prior to first occupation of the relevant phase of the development and permanently maintained.

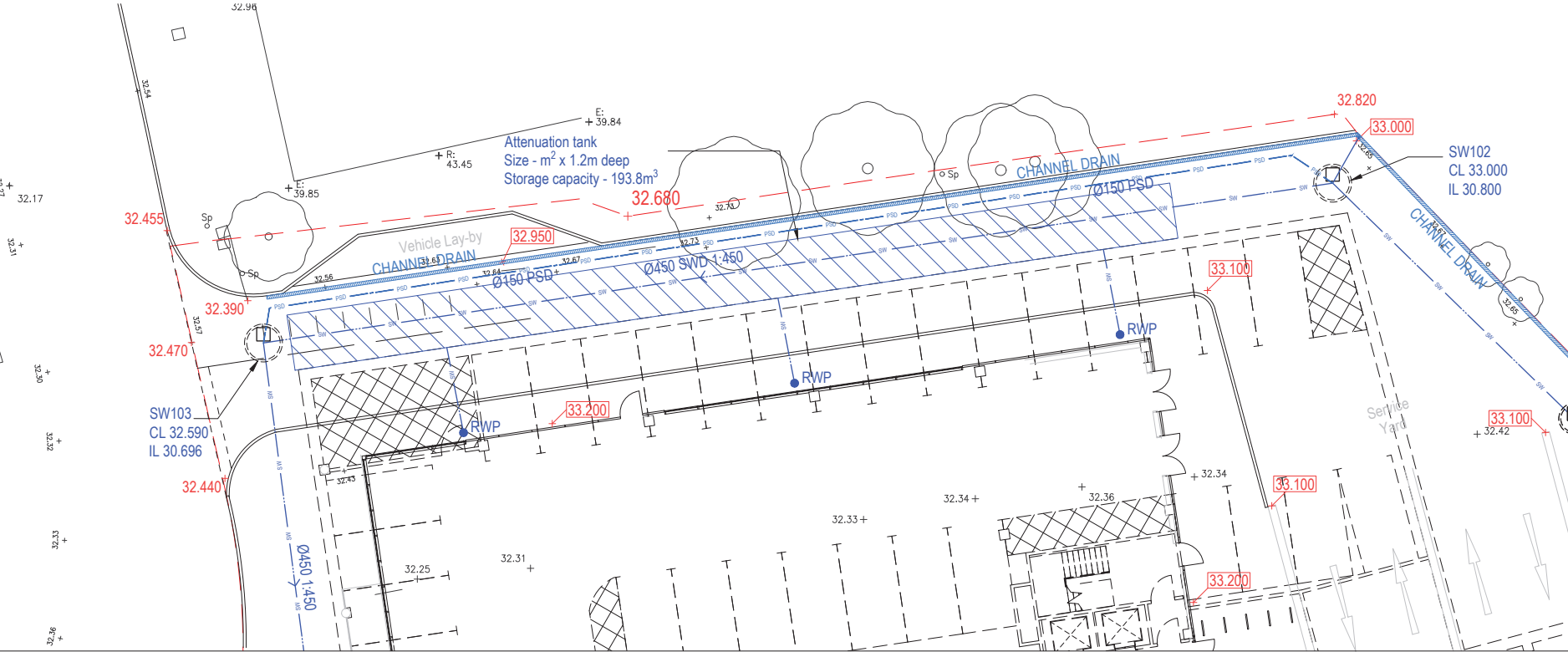
Reason: In order to mitigate the possibility of numerous satellite dishes being installed on the buildings hereby approved in the interests of the visual appearance of the development, having regard to its location adjacent to the Enfield Town Conservation Area.

35. No demolition or development shall take place until a methodology for a archaeological watching brief has been submitted to and approved by the local planning authority in writing to include the programme and methodology of site investigation and recording and the nomination of a competent person(s) or organisation to undertake the agreed works

Reason: Heritage assets of archaeological interest may survive on the site. The planning authority wishes to secure the provision of appropriate archaeological investigation, including the publication of results, in accordance with Section 12 of the NPPF.

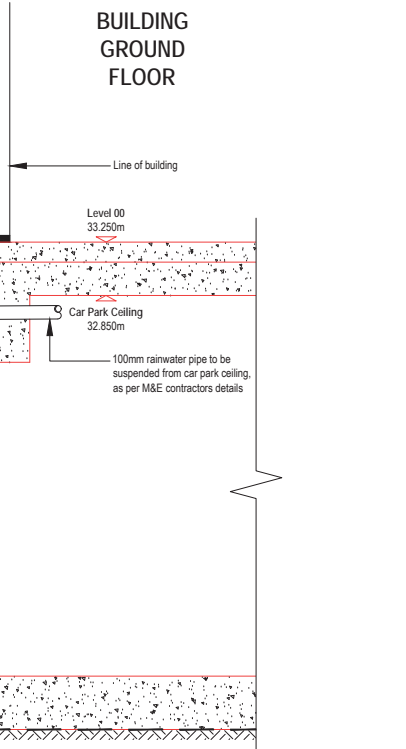
Notes:
 Dimensions provided are indicative only, and should be confirmed and read in conjunction with the Architect's construction drawings at such times as they are available, and/or from site measurement.
 All setting out to be in accordance with the Architect's drawings, any ambiguities to be raised prior to construction.

RESIDUAL RISK REGISTER			
No.	Description of Hazard	Control Measures	Action By
	Standard construction		



Surface Course - 40mm SMA 14 surf 40/60 to BS EN 13108-5 & PD 6691 (Colma cured surface, where applicable, to BCC specifications)
 Binder Course - 60mm minimum AC 20 dense bin 40/60 rec (BS EN 13108-1 & PD 6691) (Clause 906)
 Base - 180mm AC 32 dense base 40/60 rec (BS EN 13108-1 & PD 6691) (Clause 903)
 Permachannel source control and interception unit
 Surface course - 25mm AC 6 close surf 70/100 rec
 Binder course - 70mm AC 20 dense bin 40/60 rec
 150mm Type 1 sub base
 Site Boundary
 255x125mm HxB2 kerb with concrete haunching
 150mm Ø perforated pipe set deep enough to run underneath slot drainage units
 Formation level laid to fall minimum 1 in 100
 Impermeable membrane
 2500x1600mm deep geocellular attenuation tank

Surface Course - 40mm SMA 14 surf 40/60 to BS EN 13108-5 & PD 6691 (Colma cured surface, where applicable, to BCC specifications)
 Binder Course - 60mm minimum AC 20 dense bin 40/60 rec (BS EN 13108-1 & PD 6691) (Clause 906)
 Base - 180mm AC 32 dense base 40/60 rec (BS EN 13108-1 & PD 6691) (Clause 903)
 420-450mm thick course graded aggregate. Type 4/20 to B.S.E.N 7533-13 to be wrapped with an impermeable membrane
 100mm rainwater pipe
 Basement to be tanked with impermeable membrane
 Proposed baffle unit fixed to end of rainwater pipes
 Surface course - 25mm AC 6 close surf 70/100 rec
 Binder course - 70mm AC 20 dense bin 40/60 rec
 150 Type 1 sub base
 50x150 EF footway edging with concrete haunching



PRELIMINARY

P3	MSK	RWP position amended to suit internal positioning	24.08.18
P2	MSK	Minor amendment to notes	23.08.18
P1	MSK	Preliminary Issue	13.08.18
Rev	By	Details	Date
		(P=Preliminary, T=Tender, C=Construction, R=Record)	

Client:
Stoford (Enfield) Ltd.

Project:
**Metaswitch (Office Development)
 Genotin Road, Enfield**

Drawing Title:
Access Road Drainage / Attenuation Tank Details

CP
 CONSULTING CIVIL & STRUCTURAL ENGINEERS
 CHAWFORD LODGE
 ROCK HILL
 BROMSGROVE
 WORCS B61 7JH
 TEL: +44 (0)1527 832307
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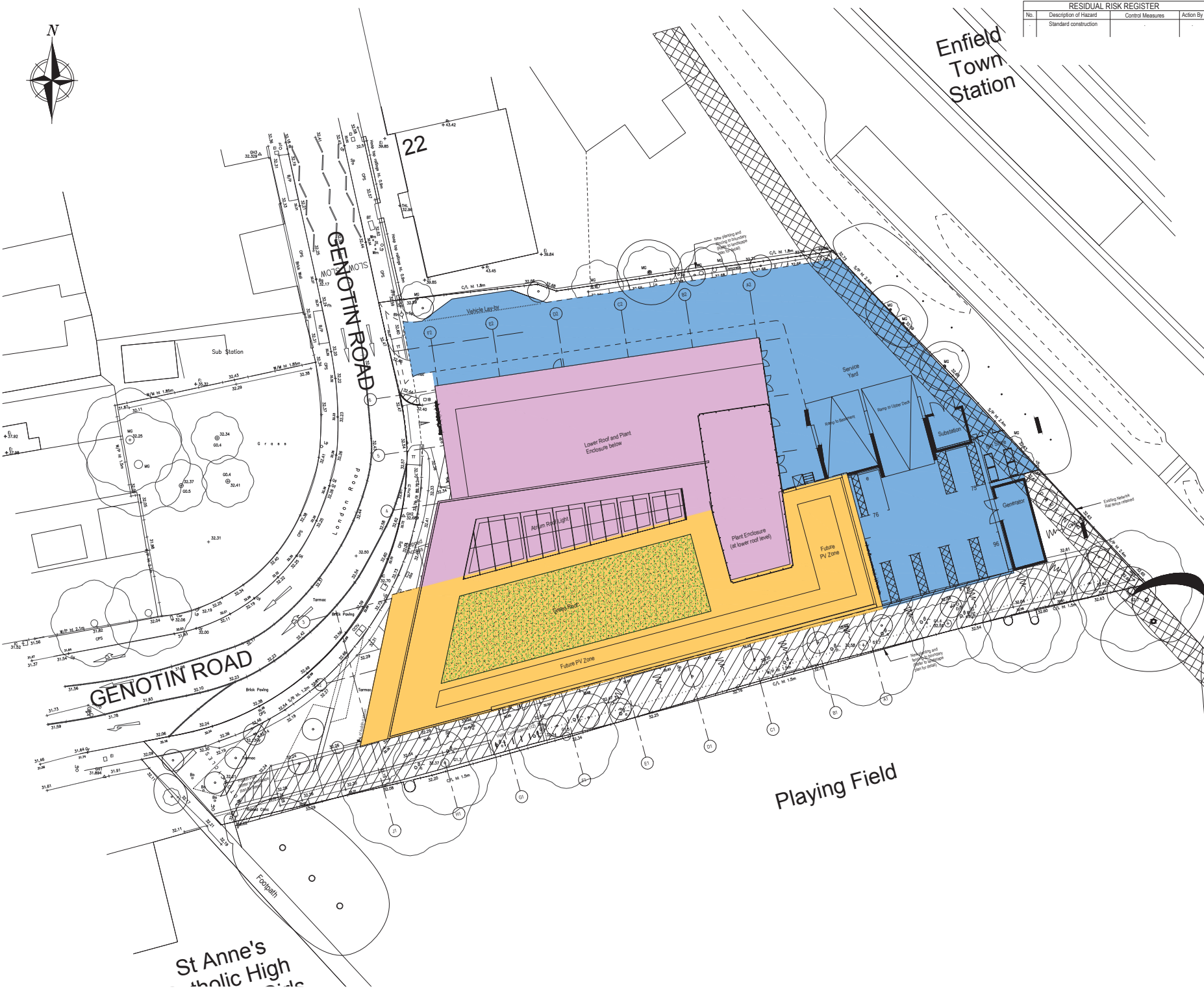
Date:	13.08.18	Drawing No.:	16-6912-sk004	Issue Rev.:	P 3
Scale:	1:100, 20 @ A1				
Drawn:	MSK				
Checked:	JB				



RESIDUAL RISK REGISTER			
No.	Description of Hazard	Control Measures	Action By
	Standard construction		

Notes:
 DO NOT SCALE DRAWING
 Dimensions provided are indicative only, and should be confirmed and read in conjunction with the Architect's construction drawings at such times as they are available, and/or from site measurement.
 All setting out to be in accordance with the Architect's drawings, any ambiguities to be raised prior to construction.
 1. This drawing is to be read in conjunction with all other relevant Engineering details.
 2. All dimensions are in metres unless otherwise stated.
 3. The Engineer shall be notified immediately, in writing, should any errors or discrepancies be found prior to the commencement or continuation of any works.

- Area 1 - Hardstanding Attenuation Tank 1
Plan Area = 1095m²
- Area 2 - Lower Roof Attenuation Tank 1
Plan Area = 852m²
- Area 3 - Upper Roof (Green Roof Area Included) Attenuation Tank 2
Plan Area = 869m²
- Area 4 - Green Roof Area
Plan Area = 275m²



PRELIMINARY

P1 MSK Preliminary Issue 23.08.18
 Rev By Details Date
 (P=Preliminary, T=Tender, C=Construction, R=Record)

Client:
 Stoford Developments

Project:
 Metaswitch (Office Development)
 Genotin Road, Enfield

Drawing Title:
 Drainage Areas

CONSULTING CIVIL & STRUCTURAL ENGINEERS
 CHAWFORD LODGE
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 BROMSGROVE
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COMPLETE DESIGN
 PARTNERSHIP LTD

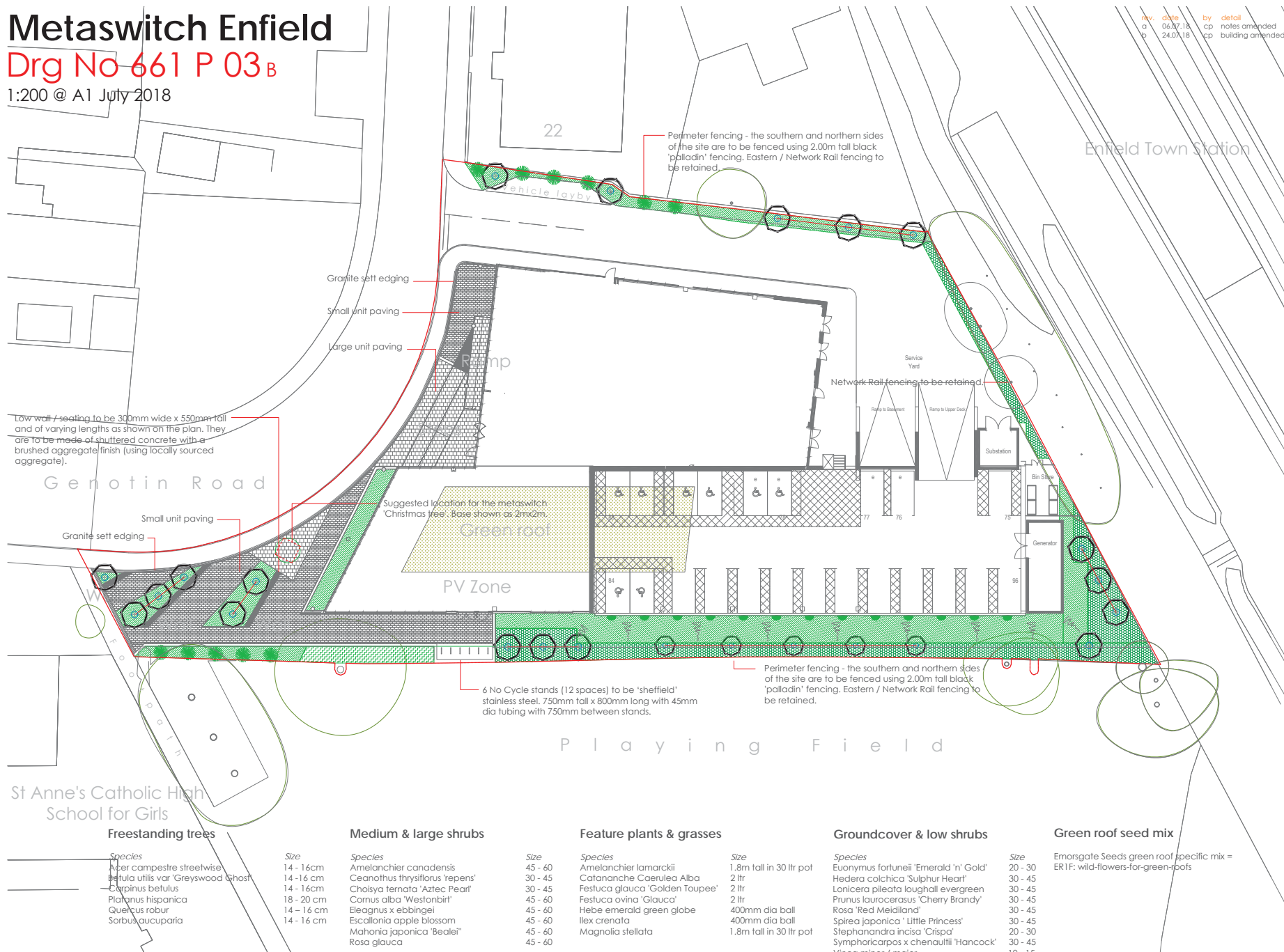
Date: 23.08.18	Drawing No. 16-6912-sk005	Issue Rev. P 1
Scale: 1:200@A1		
Drawn: MSK		
Check: JB		

Metaswitch Enfield

Drwg No 661 P 03 B

1:200 @ A1 July 2018

rev. date by detail notes amended
 06.07.18 CP notes amended
 24.07.18 CP building amended



Outline Specification notes

- TOPSOIL:** To be supplied / spread by main contractor. Depths to be 900 mm where possible to shrub planted areas (absolute minimum 500mm) and 100mm to seeded areas.
- PREPARATION OF PLANTING BEDS:** Apply approved contact herbicide to eradicate any weed growth. Do not commence cultivation until debris is complete.
- CULTIVATION:** Break up compacted ground to full depth of topsoil. DO NOT turn up soil. Cultivate by approved mechanical means and / or by hand and reduce to a tilth. Remove all stones and clay ball exceeding 25mm dia. weed, roots of perennial weeds. Tufts of grass and any other foreign matter.
- LOW MAINTENANCE / WILDFLOWER GRASS SEEDING - EMORGATE MIXES:** These areas must be 'minored' through the addition of fern killers or conditioners. Site clearance / weed control and seed bed cultivation must be carried out in line with guidance given by Emorgate.
- CONDITIONER / COMPOST:** Spread conditioner evenly over all planting areas at 10 litres/m² and spread 25mm depth approved Spent Mushroom Compost evenly over all planting areas and work into topsoil during cultivations. Do not spread any fertilizer or conditioner on any areas to be seeded. Mushroom compost must not contain peat.
- FERTILISER:** Spread evenly over all planting areas at rate of 100 gms/m² and work into topsoil during cultivations.
- WATER STORAGE POLYMER:** Spread evenly over all planting areas at a rate of 100gms/m² and work into topsoil during cultivations.
- MANUFACTURERS RECOMMENDATIONS:** To be strictly followed for storage, handling and application of fertilisers, herbicides and all other chemicals. Remove containers from site.
- TREE PITS:** Excavate as follows -
 Extra heavy standard 1.4m x 1.0m deep
 Selected standard 1.0m x 1.0m deep
 All other standard 1.0m x 0.8m deep
 Feathered trees 750mm x 450mm deep
 Break up bottom of pits to a minimum depth of 300mm and roughen sides. Ensure that pits are free draining and install land drain if required (100mm diameter PVC perforated pipe). Excavated topsoil to be mixed thoroughly with the following (bulk up with additional topsoil if required)
 10% topsoil
 10% compost
 10% fertilizer (if required)
- IRRIGATION TUBING:** For selected standard trees (and bigger), install 50mm dia irrigation tubing to each tree.
- TREES, SHRUBS & OTHER PLANTS:** All plants to comply with the relevant BS and be obtained from approved sources. They will be good, well-learned, healthy specimens, free from disease and damage to stem, branches, roots and foliage and have a vigorous both with a reasonable proportion of fine fibrous roots. Comply with Code of Practice for Plant Handling, during lifting, packaging, transportation to site and while on site until final planting.
- DELIVERY OF PLANTS:** Carefully lift, pack and protect plant material to survive transport to site without damage. Carefully prune any roots, branches or shoots that are slightly damaged. Damaged plants will be rejected.
- SUITABLE PLANTING CONDITIONS:** will be taken to mean mild and damp weather when the ground is moist and workable. Do not plant in persistent cold or drying winds or if the soil is frost bound, waterlogged or excessively dry.
- TEMPORARY STORAGE ON SITE:** Where planting must be delayed Plants to be thoroughly heeled in topsoil or a suitable moisture retentive medium. All plant bundles to be opened and plants loosely distributed within the medium. Containers grown plants to be stored closely together, upright, well drained, weed free ground. Adequately support all trees and other large stock. Water plants as necessary to prevent drying out.
- PLANTING TREES:** Upright and in the centre of the pit, at the same distance and in the same direction to the prevailing wind as when grown in the nursery. Bare root trees - place backfilling material in 150 - 200mm diameter hole or spaces. Firm backfilling material with fully spread roots and elimination of air pockets. Firm backfilling material in 150 - 200mm diameter hole or spaces. Firm backfilling material around root collar. Root balled Container grown trees - firm backfilling material around root ball in 150mm layers taking care not to disturb roots. Prune broken or damaged branches. Water thoroughly immediately after backfilling.
- STAKES FOR TREES:** Pressure impregnated SW post (crescent or top) round, straight, pointed, free from projections and pithing at one end. Use a single short stake per tree for 10-12cm with stake at 45 degrees and two stakes with cross bar for larger trees. Tie with appropriate tree tie.
- PLANTING GENERALLY:** Set out plants evenly spaced and in an informal manner avoiding straight line. Plant upright or well balanced with best side to the front. Carefully return excavated topsoil, packing around evenly spread roots or root ball and heel well in. Finished soil level to be at original soil level and 50mm above surrounding level to allow for settlement. Immediately after planting carefully cut back any damaged, dead or diseased branches and remove any weak, thin, or maldeveloped growth. Where specified, cut back species to encourage growth. After planting fork or rake soil to a fine tilth. Water plants thoroughly immediately after planting.
- SHRUBS:** Excavate planting holes 150mm wider & deeper than root spread and plant to the correct depth.
- BARK MULCH:** After watering spread 50mm of softwood bark mulch over all plant beds. Nominal particle size shall be 5 - 75mm with no more than 1% below 10mm.
- All freestanding trees in grass areas to have a 1m bark mulch circle.
- MAINTENANCE:** To include fertilising, watering, cutting, pruning, limping, cutting stakes/risers/guards, applying approved chemicals, keeping plant beds clear of weeds, and removal of rubbish. Adjacent roadways and pavements should be kept clear of bark mulch and soil. Maintain all planted areas for 12 months from date of practical completion unless specified otherwise. The defect liability period will also run concurrently for 12 months from the date of practical completion. Establishment and ongoing care of grass areas must follow guidance given by Emorgate Seeds.

Low wall / seating to be 300mm wide x 550mm tall and of varying lengths as shown on the plan. They are to be made of shuttered concrete with a brushed aggregate finish (using locally sourced aggregate).

Small unit paving

Granite sett edging

Small unit paving

Granite sett edging

Freestanding trees

Species	Size
Acer campestre streetwise	14 - 16cm
Thuja utilis var 'Greyswood Ghost'	14 - 16cm
Carpinus betulus	14 - 16cm
Platanus hispanica	18 - 20cm
Quercus robur	14 - 16cm
Sorbus aucuparia	14 - 16cm

Proposed green roof

Proposed trees

Existing trees

Green roof

PV Zone

6 No Cycle stands (12 spaces) to be 'sheffield' stainless steel. 750mm tall x 800mm long with 45mm dia tubing with 750mm between stands.

Perimeter fencing - the southern and northern sides of the site are to be fenced using 2.00m tall black 'palladin' fencing. Eastern / Network Rail fencing to be retained.

Medium & large shrubs

Species	Size
Amelanchier canadensis	45 - 60
Ceanothus thyriflorus 'repens'	30 - 45
Choisya ternata 'Aztec Pearl'	30 - 45
Cornus alba 'Westonbirt'	45 - 60
Elaeagnus x ebbingei	45 - 60
Escallonia apple blossom	45 - 60
Mahonia japonica 'Bealeii'	45 - 60
Rosa glauca	45 - 60

Native shrub mix

Tall / medium shrubs

Low shrubs

Feature plants & grasses

Species	Size
Amelanchier lamarckii	1.8m tall in 30 ltr pot
Catananche Caerulea Alba	2 ltr
Festuca glauca 'Golden Toupee'	2 ltr
Festuca ovina 'Glaucia'	2 ltr
Hebe emerald green globe	400mm dia ball
Ilex crenata	400mm dia ball
Magnolia stellata	1.8m tall in 30 ltr pot

Groundcover & low shrubs

Climbing plants

Large unit paving

Green roof seed mix

Species	Size
Euonymus fortuneii 'Emerald 'n' Gold'	20 - 30
Hedera colchica 'Sulphur Heart'	30 - 45
Lonicera pileata loughall evergreen	30 - 45
Prunus laurocerasus 'Cherry Brandy'	30 - 45
Rosa 'Red Meiland'	30 - 45
Spirea japonica 'Little Princess'	30 - 45
Stephanandra incisa 'Crispa'	20 - 30
Symphoricarpos x chenaultii 'Hancock'	30 - 45
Vinca minor / major	10 - 15

Emorgate Seeds green roof specific mix = ERTf: wild-flowers-for-green-roofs

Bicycle stands

Feature low wall

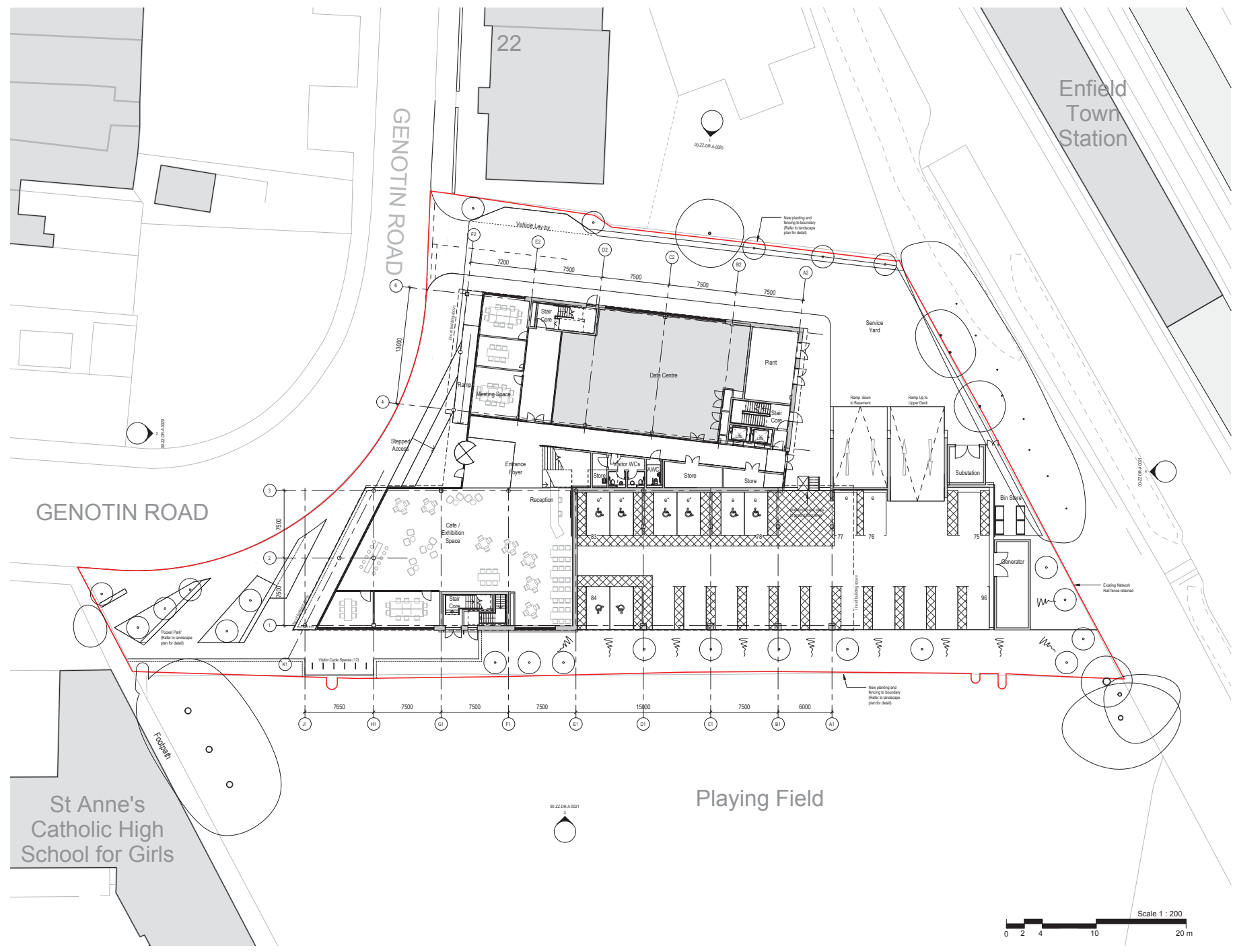
Proposed conifers

Granite sett edging

Small unit paving

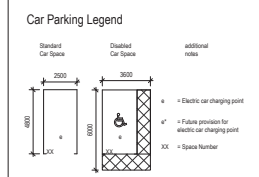
Large unit paving

Small unit paving



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 - THE CDW DESIGN ISSUES REGISTER
 - THE BDD APP SERIES OF DRAWINGS
 - THE PROJECT CDW RISK REGISTER

NOTES



Code	Description	Issue No.	Rev.	Date
PS	Planning Issue - Revised vehicle cycle parking location, additional future provision charging spaces shown	MD	PM	16/05/2018
PT	Issued for Planning	MD	PM	23/02/2018
PE	Issue for Planning	MD	PM	11/02/2018
PS	Issue for Planning	MD	PM	08/02/2018
PS	Issue for Planning	MD	PM	08/02/2018
PS	Issue for Planning	MD	PM	25/02/2018
PS	Issue for Planning	MD	PM	08/02/2018
PS	Issue for Planning	MD	PM	22/02/2018
PS	Issue for Planning	MD	PM	14/02/2018
PS	Issue for Planning	MD	PM	24/02/2018



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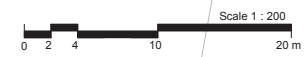
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Metaswitch, Genotin Road

Project Ref:	P2007486	Public Use
Proposed Ground Floor GA Plan	Scale	@ A1 1:200
Issue Date:	24/04/18	
7486-BDP-00-00-DR-A-0010	S2	P8

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 - THE CDW DESIGN ISSUES REGISTER
 - THE BDP RISK REGISTER
 - THE PROJECT CDW RISK REGISTER

NOTES

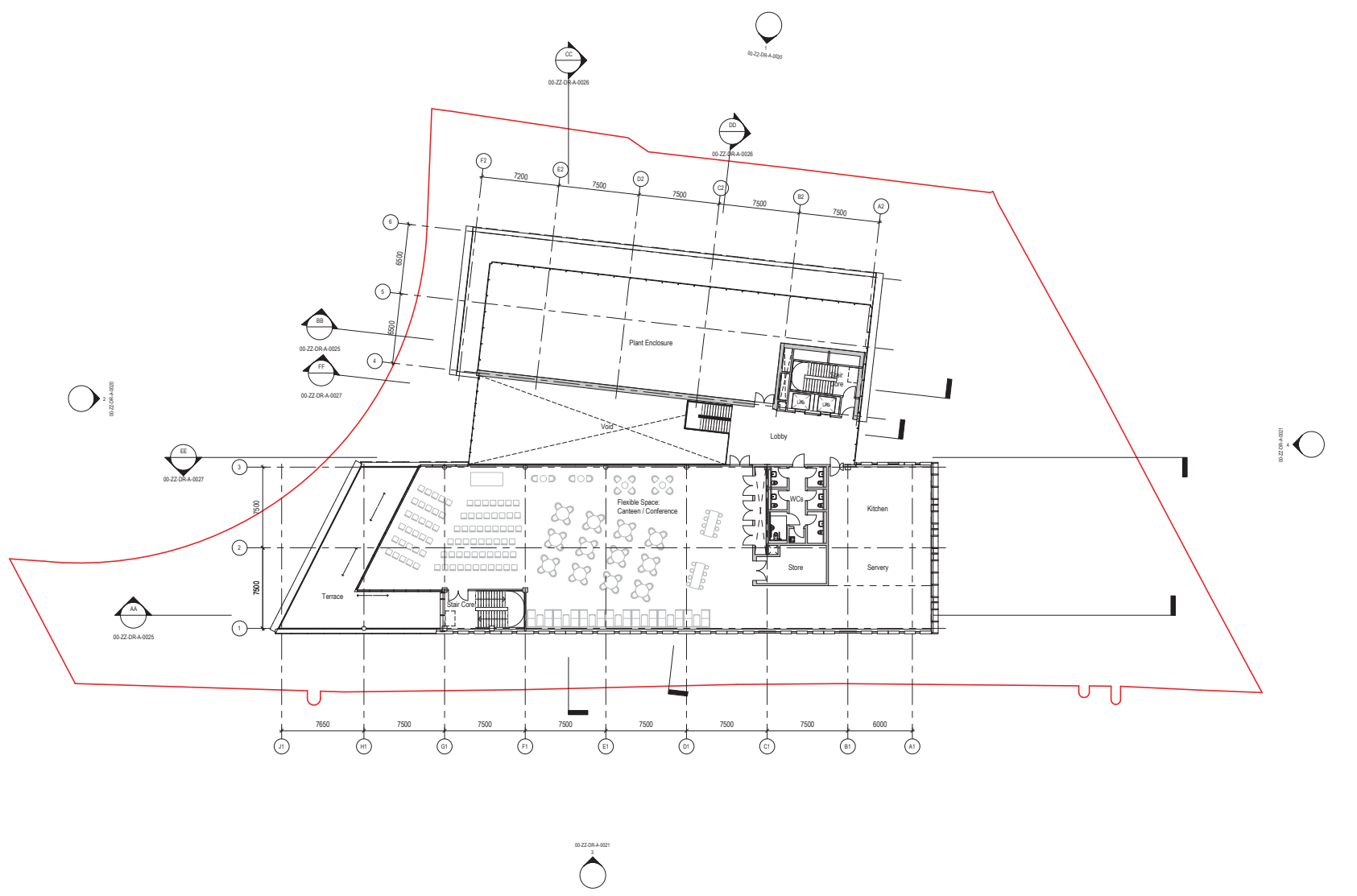
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PE	Issue for Planning Issue	14.05.2018	MD	RM
PS	Issue for Planning Issue	14.05.2018	MD	RM
PR	Issue for Review	14.05.2018	MD	RM
PI	Issue for Information	14.05.2018	MD	RM
PE	Issue for Planning Application	14.05.2018	MD	RM



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Project Name	Metaswitch, Genotin Road
Project Number	P2007486
Project Status	Public Use
Scale	1 : 200
Date	24/04/18
Sheet Number	S2 P7

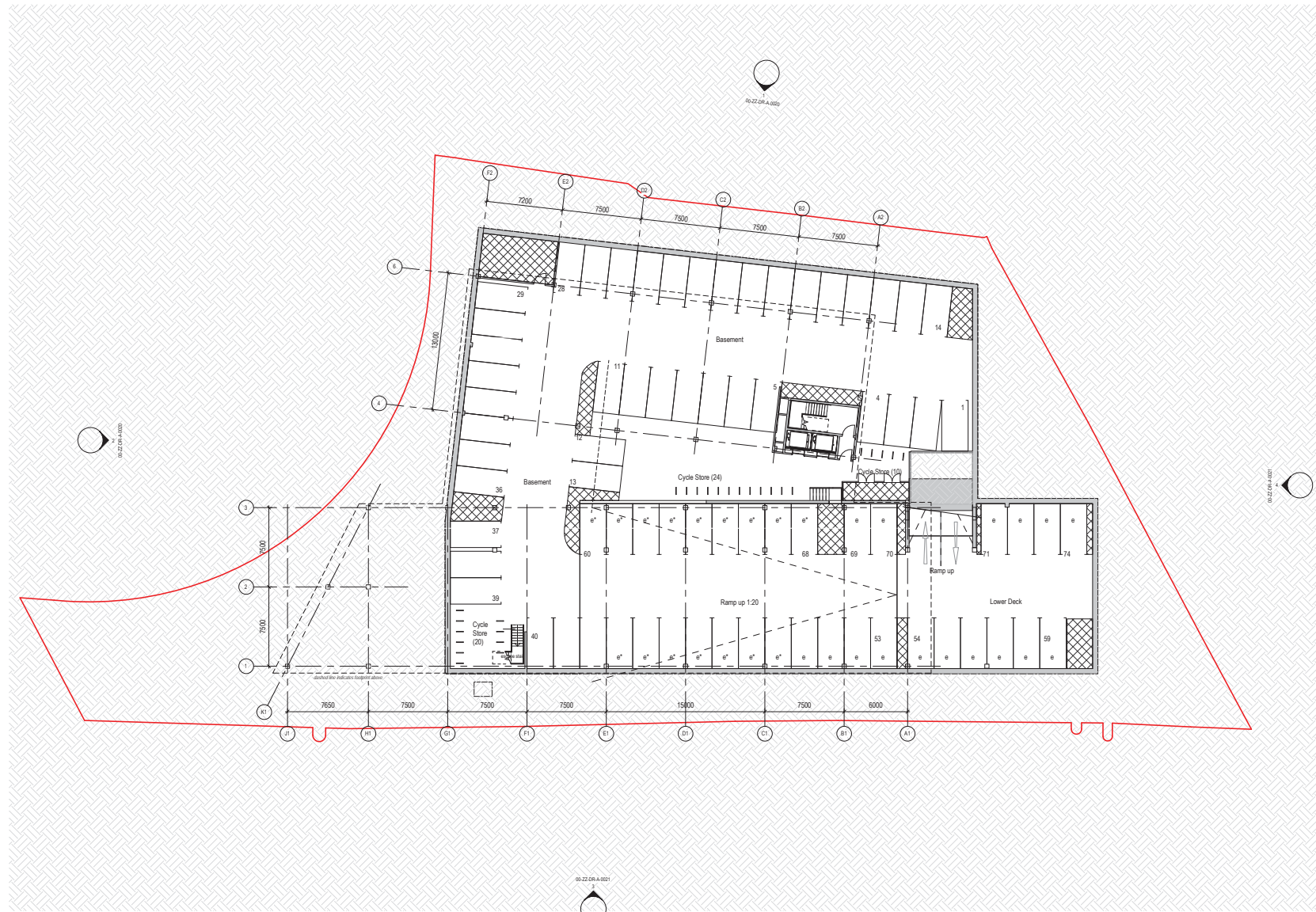


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 - THE CDW DESIGN ISSUES REGISTER
 - THE BDP RISK REGISTER
 - THE PROJECT CDW RISK REGISTER

NOTES

Car Parking Legend

Standard Car Space	Disabled Car Space	additional notes
		= Electric car charging point
		= Future provision for electric car charging point
		XX = Space Number



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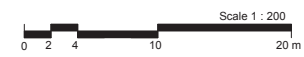
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100	Issued for Planning Application	MD	14/05/2018

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Project Name	Metaswitch, Genotin Road
Project Number	P2007486
Project Type	Public Use
Scale	1 : 200
Date	24/04/18
Sheet Number	7486-BDP-00-B1-DR-A-0009
Sheet Size	S2 P8



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- THE CDM DESIGN ISSUES REGISTER
- THE BDP RISK SERIES OF DRAWINGS
- THE PROJECT CDM RISK REGISTER

NOTES

REVISION	DESCRIPTION	DRAWN	CHECKED	DATE
P3	Issued for Planning	MD	PM	27.07.2018
P2	Draft Planning Issue	MD	PM	11.07.2018
P1	Draft Planning Issue	MD	PM	06.07.2018
P0	Issued for Pre Planning Application	MD/HC	PM	24.04.2018

REVISION DESCRIPTION DRAWN CHECKED DATE

REVISION DESCRIPTION DRAWN CHECKED DATE



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PROJECT TITLE
Metaswitch, Genotin Road

BDP JOB NUMBER
P2007486

BDP 2001 CLASSIFICATION
Public Use

DRAWING TITLE
Existing Site Location Plan

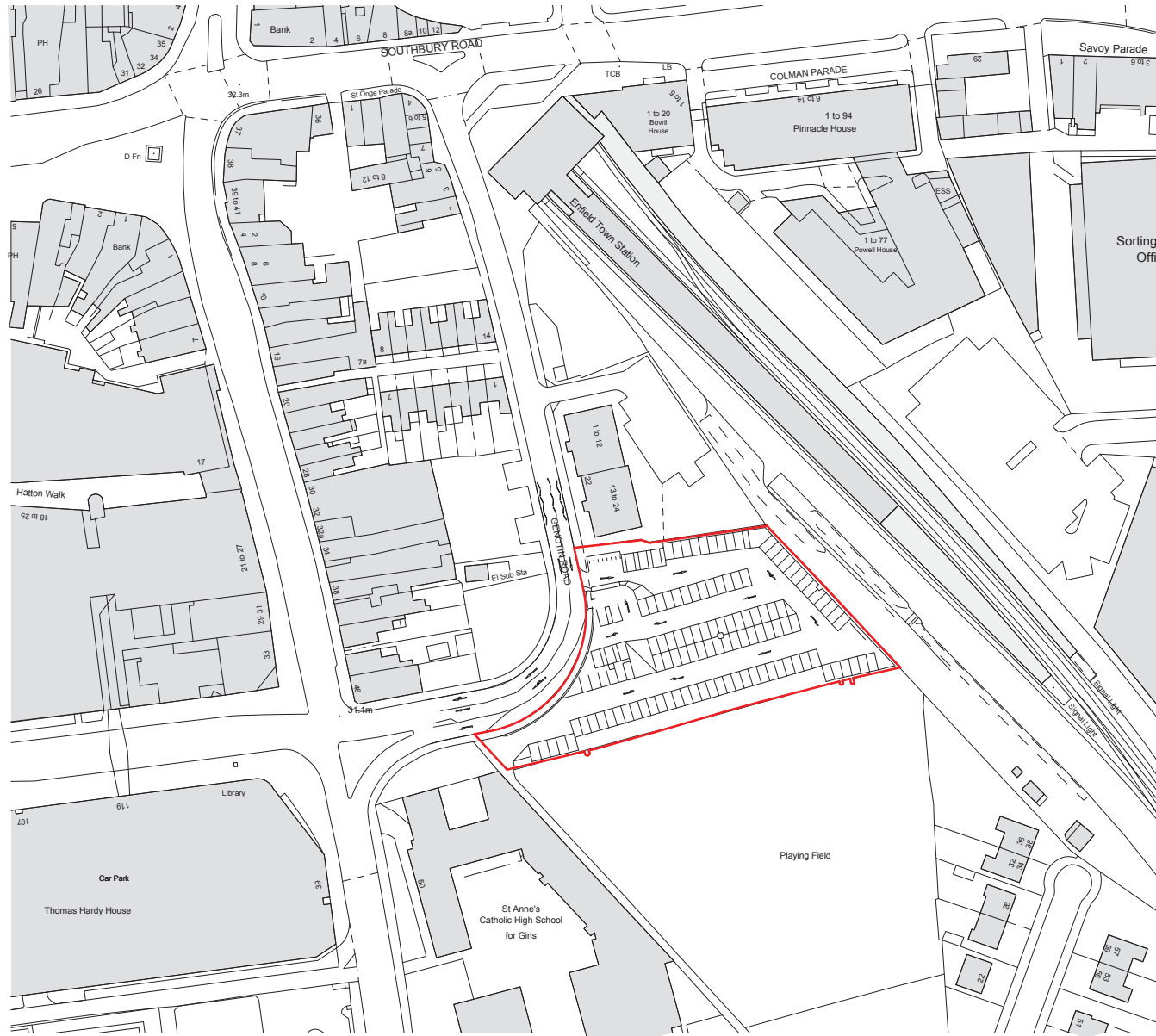
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**@ A3
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DATE FIRST ISSUED
24/04/18

DRAWING NO.
7486-BDP -00-XX-DR-A-0001

SUBMITTAL
S2

REVISION
P3

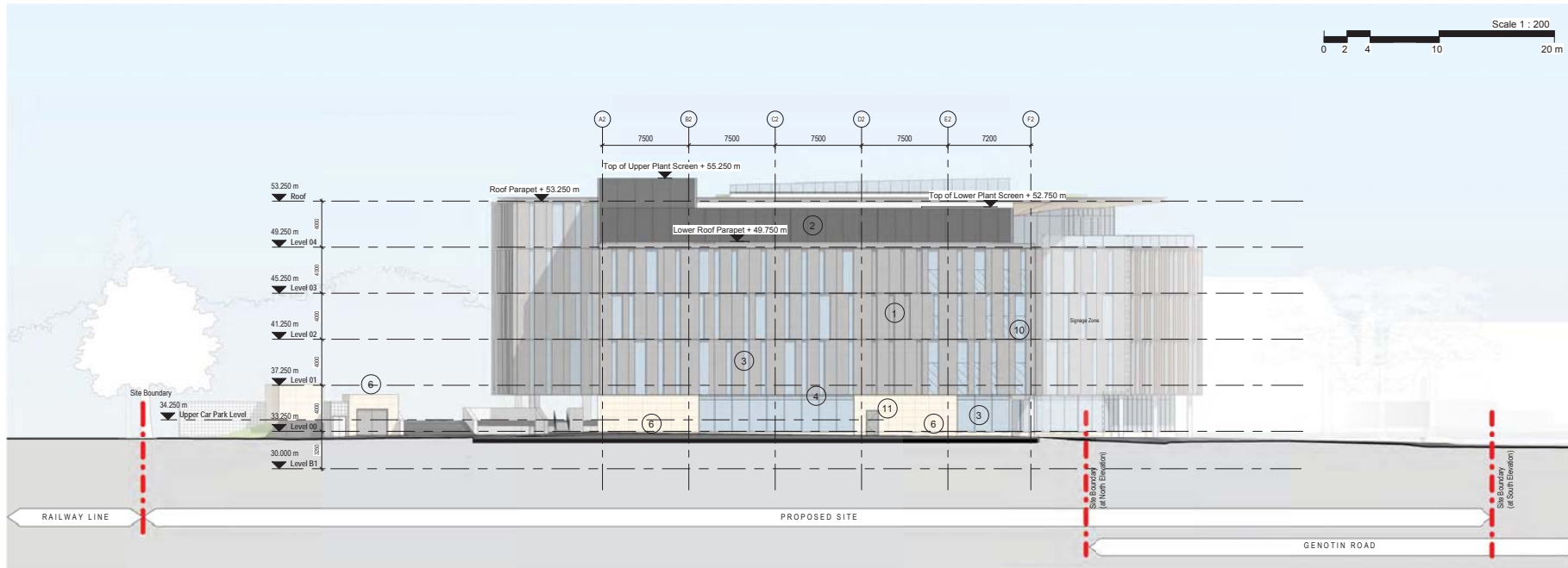


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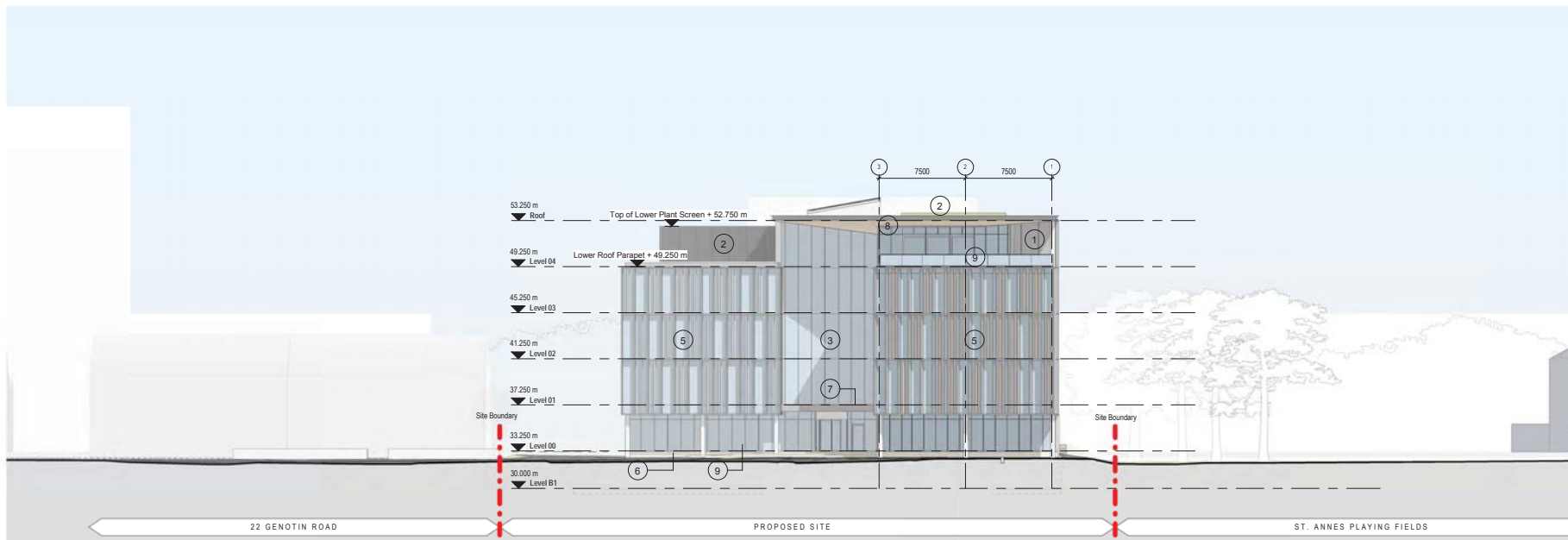
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Scale 1 : 1250





1 GA Elevation - North
1:200



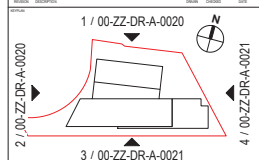
2 GA Elevation - West
1:200

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- THE CDW DESIGN ISSUES REGISTER
- THE BDF RISK REGISTER OF DRAWINGS
- THE PROJECT CDW RISK REGISTER

NOTES

- Materials (subject to planning approval)
- Cladding - Curtain Wall System - Solid Panel
PPC/Anodised Aluminium
Varying Thicknesses
Colour - Silver/Grey
 - Plant Screen
PPC/Anodised Aluminium Louvers
Colour - Silver/Grey
 - Cladding - Curtain Wall System
Glazed Panel - Clear glazing with colour matched back-painted glass spandrel panel at head of window
Mullion colour - Silver/Grey.
 - Cladding - Curtain Wall System
Louvered Panel - PPC/Anodised Aluminium
Colour - Silver/Grey
 - Cladding - Curtain Wall System with Brise Soleil
PPC/Anodised Aluminium.
Mixture of Silver/Grey, Dark Grey & Bronze colours.
 - Plinth / Entrance Ramp / Steps / Ground Floor Walls
Reconstructed stone/ stone cladding with matching pavers.
Sandstone or similar colour.
 - Entrance Canopy
Reconstructed Stone/ Stone Cladding to match plinth.
Sandstone or similar colour
 - Roof Soffit - Metal Cladding, Colour TBC
 - External Balustrade
Glazed with Stainless Steel hand rail
 - Opaque/Frosted Glazing
 - External Doors - Solid / Glazed
PPC/Anodised Aluminium Finish
Colour - Silver/Grey

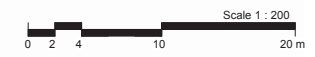
Rev	Description	By	Date
PS	Planning Issue - Elevations updated	MD	16.05.2018
PT	Cost Issue - Additional stone above B ground floor level elevation	MD	16.05.2018
PS	Issued for Planning	MD	27.07.2018
PS	Cost Planning Issue	MD	11.08.2018
PS	Cost Planning Issue	MD	05.07.2018
PS	Issued for Review	MD/HC	28.06.2018
PS	Issued following issue coordination meeting	MD/HC	16.08.2018
PI	Issued for Information	HC	14.05.2018
PS	Issued for Planning Application	MD/HC	24.08.2018



Project Name	Metaswitch, Genotin Road
Reference	P2007486 Public Use
Proposed GA Elevations Sheet 1	As @ A1 indicated
Issue Date	24/04/18
Project Code	7486-BDP-00-ZZ-DR-A-0020
Sheet	S2 P8

Page 1 of 1

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 - THE BDF RISK REGISTER OF DRAWINGS
 - THE PROJECT CDM RISK REGISTER

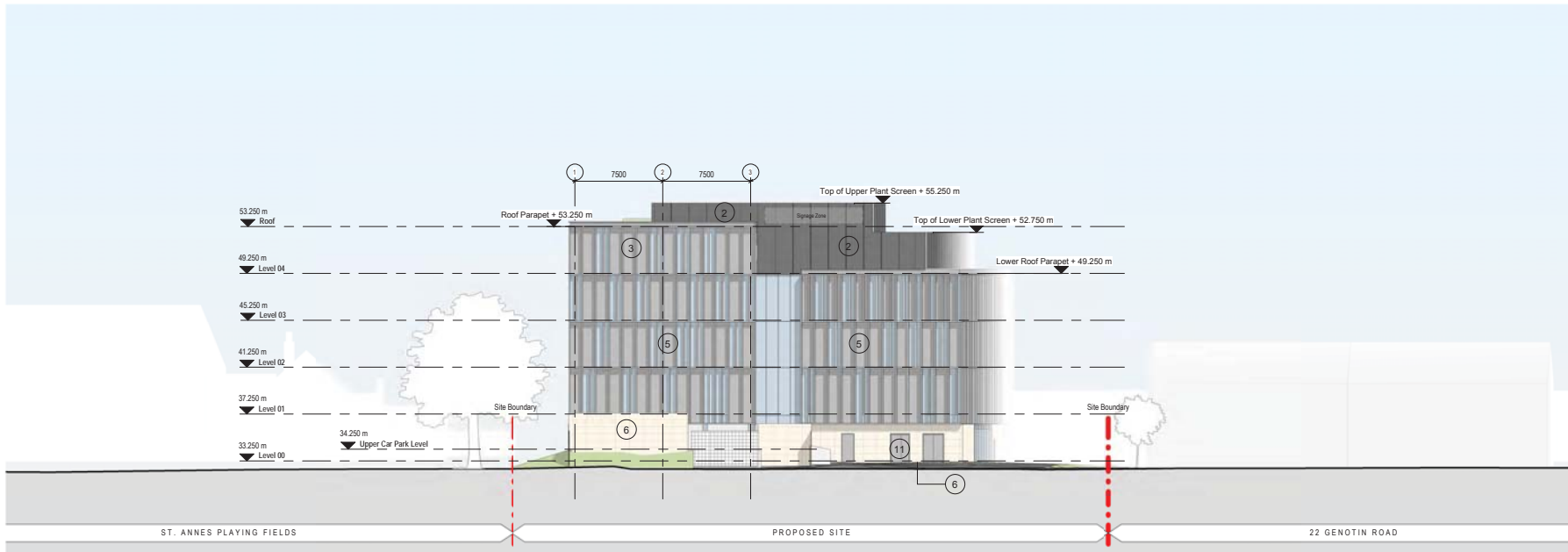


3 GA Elevation - South
1:200

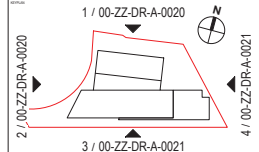
NOTES

- Materials (subject to planning approval)
- Cladding - Curtain Wall System - Solid Panel
PPC/Anodised Aluminium
Varying Thicknesses
Colour - Silver/Grey
 - Plant Screen
PPC/Anodised Aluminium Louvres
Colour - Silver/Grey
 - Cladding - Curtain Wall System
Glazed Panel - Clear glazing with colour matched back-painted glass spandrel panel at head of window
Mullion colour - Silver/Grey.
 - Cladding - Curtain Wall System
Louvre Panel - PPC/Anodised Aluminium
Colour - Silver/Grey
 - Cladding - Curtain Wall System with Brise Soleil
PPC/Anodised Aluminium
Mixture of Silver/Grey, Dark Grey & Bronze colours.
 - Plinth / Entrance Ramp / Steps / Ground Floor Walls
Reconstructed stone/ stone cladding with matching pavers.
Sandstone or similar colour.
 - Entrance Canopy
Reconstructed Stone/ Stone Cladding to match plinth.
Sandstone or similar colour.
 - Roof Soffit - Metal Cladding, Colour TBC
 - External Balustrade
Glazed with Stainless Steel hand rail
 - Opaque/Frosted Glazing
 - External Doors - Solid / Glazed
PPC/Anodised Aluminium Finish
Colour - Silver/Grey

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100	Issued for Information	14.05.2018	JC	MD



4 GA Elevation - East
1:200

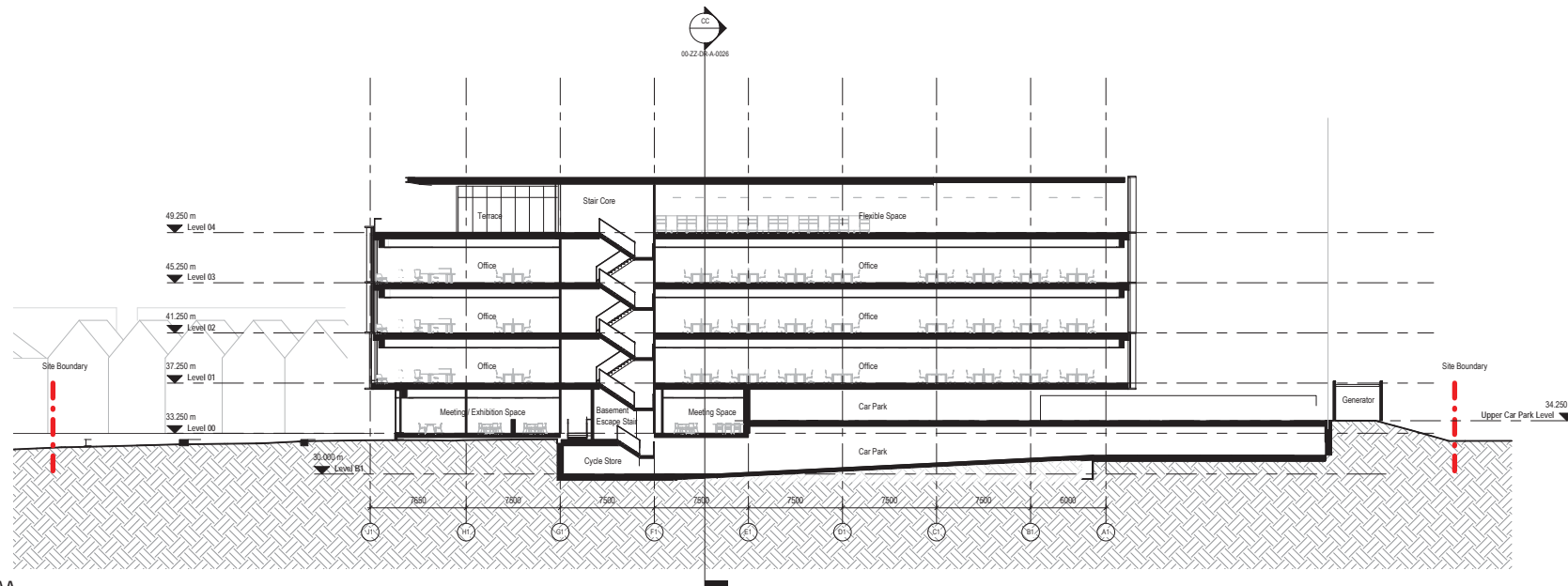


Project Name	Metaswitch, Genotin Road
Project Number	P2007486 Public Use
Proposed GA Elevations Sheet 2	As @ A1 indicated
Issue Date	24/04/18
Project Reference	7486-BDP-00-ZZ-DR-A-0021
Sheet	S2 P8

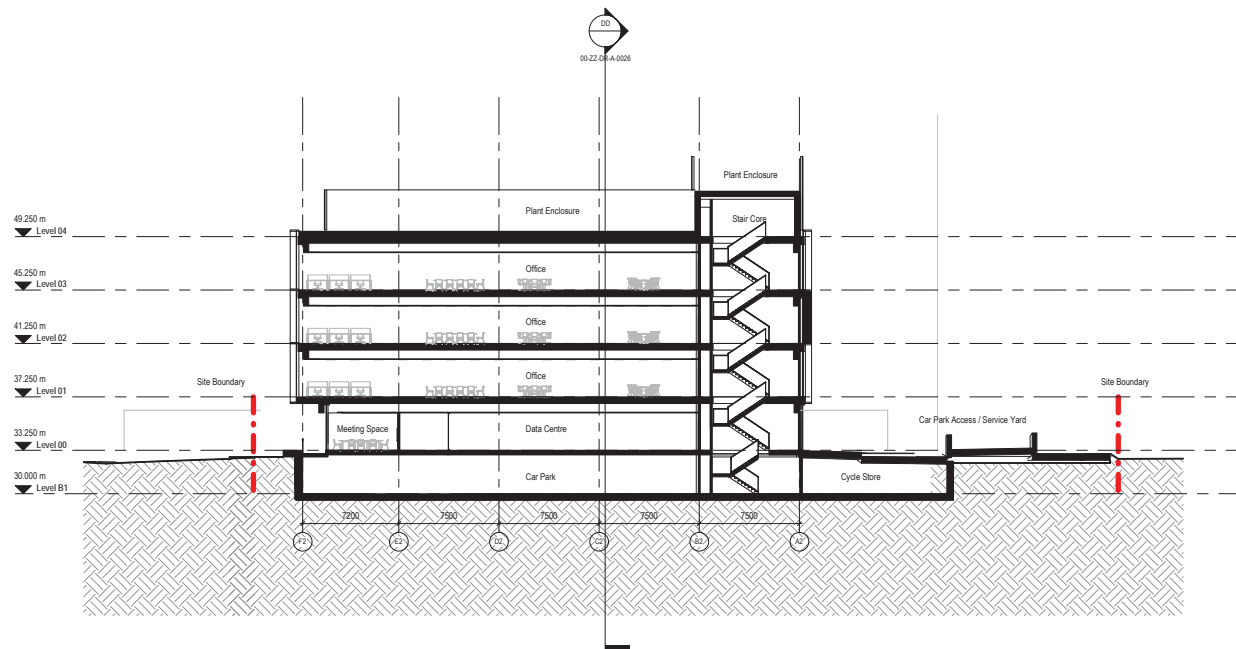
Page 19/22

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 - THE CDV DESIGN ISSUES REGISTER
 - THE BDD RISK REGISTER
 - THE PROJECT CDV RISK REGISTER

NOTES

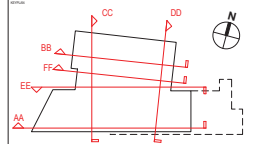


GA Section AA
1:200



GA Section BB
1:200

R4	Issued for Planning	MD	RM	27/02/18
P3	Draft Planning Issue	MD	RM	11/07/17
P2	Draft Planning Issue	MD	RM	18/02/17
P1	Issued for Review	MD/HC	MD	29/06/2016
PR	Issued for Pre-Planning Application	MD/HC	RM	24/08/2016



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Metaswitch, Genotin Road

P2007486 Public Use

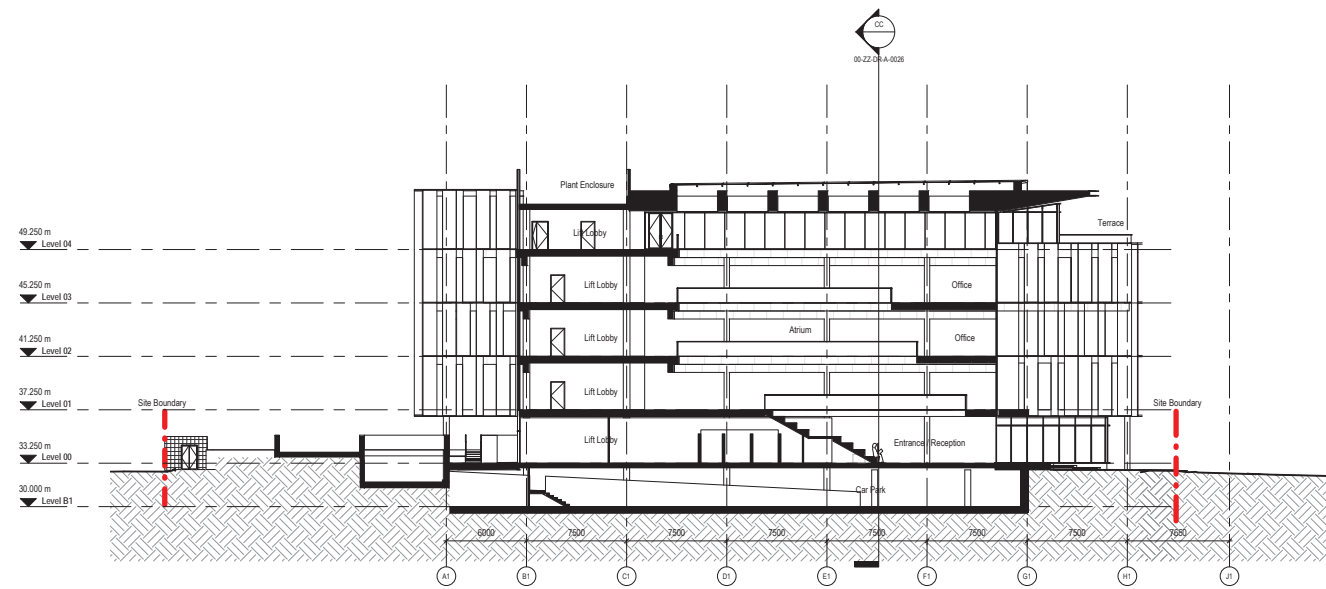
Proposed GA Sections
 Sheet 1

As @ A1 indicated
 24/04/18

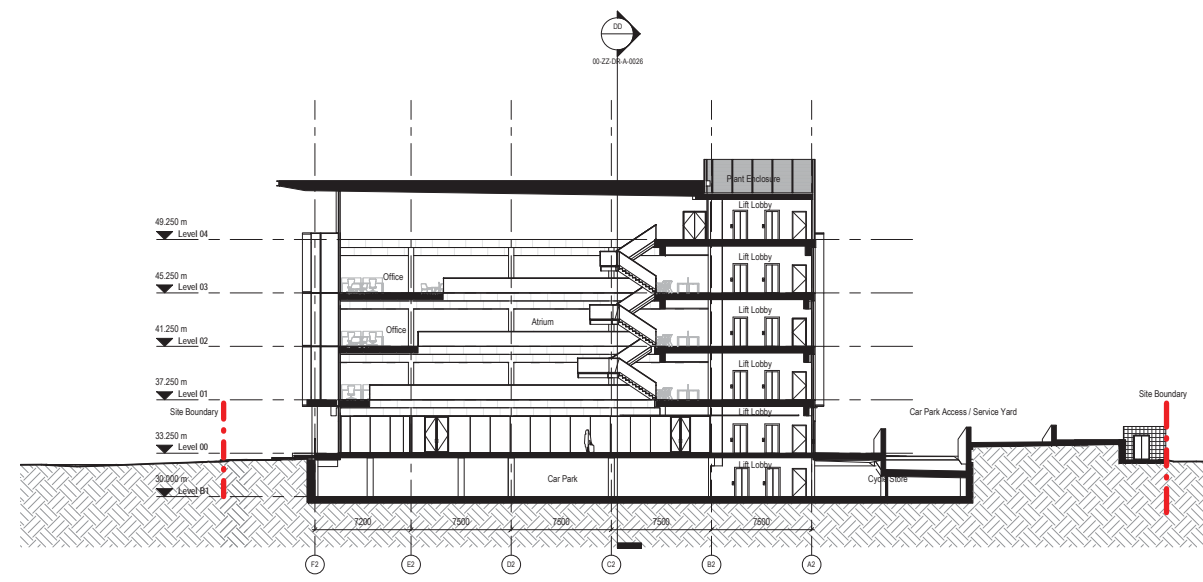
7486-BDP-00-ZZ-DR-A-0025 S2 P4

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 - THE CDM DESIGN ISSUES REGISTER
 - THE BOP FOR SERIES OF DRAWINGS
 - THE PROJECT CDM/RISK REGISTER

NOTES



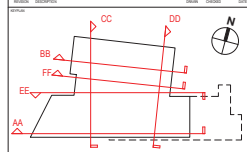
EE GA Section EE
1:200



FF GA Section FF
1:200

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P3	Issued for Planning	MD	PM	27.07.2018
P2	2018 Planning Issue	MD	PM	11.03.2018
P1	Draft Planning Issue	MD	PM	09.07.2018
PR	Issued for Review	MD/CH	MD	25.08.2018



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P2007486 Public Use

Proposed GA Sections

As @ A1 indicated

06/12/18

7486-BDP-00-ZZ-DR-A-0027 S2 P3



Tree Constraints Plan

(Existing Layout)



Photo 1



Photo 2



Photo 3

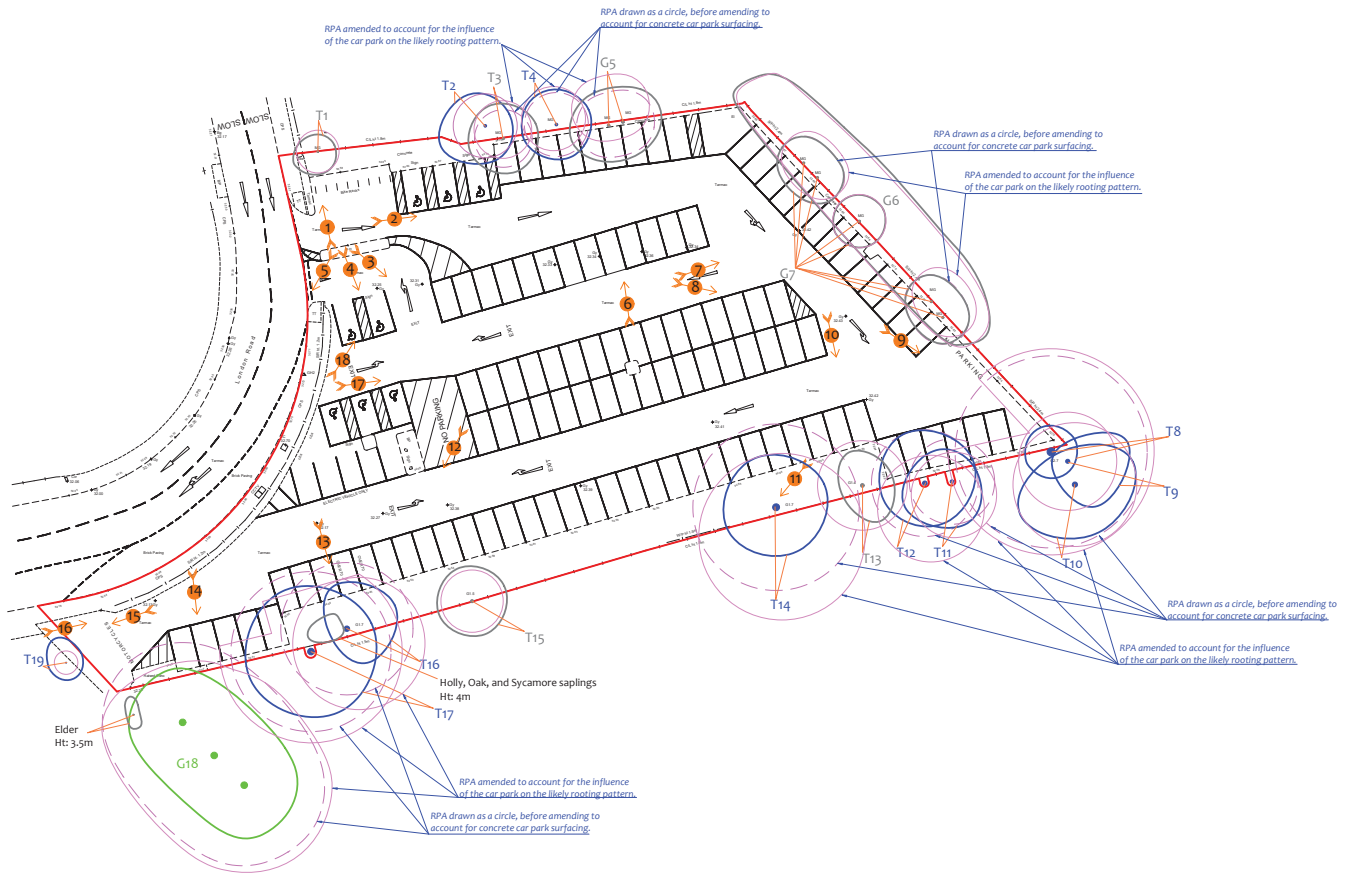


Photo 4



Photo 5

See the accompanying report for more photographs



Drawing No: CCL 10003 / TCP Rev 2
 Title: Tree Constraints Plan (Existing Layout)
 Site: Cavendish Car Park (E10 2JW)
 Scale: 1:500
 Paper Size: A1



Tree Retention Categories
 System & Categories shown
 Category A tree
 Category B tree
 Category C tree
 Category U tree

Tree of high quality with an estimated life expectancy of 60+ years, locally high trees with significant presence or notable trees with excellent form, retention of these trees is highly desirable.
 Tree of moderate quality with a life expectancy of 40+ years, locally medium trees, or notable trees with good form, retention of these trees is desirable though less than Category A trees.
 Acceptable level of tree quality and form, individual specimens are not considered to be a material planning consideration.
 Trees unsuitable for retention due to their very poor condition.

Tree Constraints Plan

RS (R2) Root Protection Area (radius = crown diameter)
 Root Protection Area needing amendment due to site conditions, e.g. presence of existing road or building.
 Root Protection Area having been amended to account for the site conditions.
 T1 - Tree No 1 G2 - Group No 2 H3 - Hedge No 3



MIN = Measured North
 Compass readings are sometimes measured to an approximate N defined by site features. Other more accurate, especially where rows of trees are not aligned N or E-W.

Tree Ref.	Species	Height(m)	Radius(m)	Area(m ²)	Root Protection Area
T1	Sycamore	4	2.4	18	4.3
T2	Norway Maple	4	3.6	41	6.4
T3	Sycamore	6	3.2	33	5.7
T4	Norway Maple	6	3.6	41	6.4
G5	Norway Maple	7	3.6	41	6.4
G6	Sycamore And Ash	5	1.8	10	3.2
G7	Sycamore And Ash	7	3.0	28	6.3
T8	Lombardy Poplar	20	10.8	366	19.1
T9	Oak	9	6.5	106	8.8
T10	Ash	12	7.0	152	12.3
T11	Ash	10	4.8	72	8.5
T12	Sycamore	10	6.0	113	10.8
T13	Sycamore	10	5.0	80	8.9
T14	Sycamore	12	5.4	275	16.6
T15	Sycamore	7	3.5	38	6.2
T16	Oak	12	2.4	174	13.2
T17	Sycamore	15	9.0	254	16.0
G18	Common Elm	15	9.0	254	16.0
T19	Elder Birch	5	1.3	5	2.3

Excerpts from the Arboricultural Impact Assessment

Overview
 It is proposed to construct a multi-story office building with associated car parking as indicated on the plans in Appendix C. The existing layout is indicated in black and the footprint of the proposed layout is indicated in pale green.
 The table below summarises the potential impact on trees due to various activities.

Activity	Trees Potentially Affected
Tree Removal- Retention Category A	None
Tree Removal- Retention Category B	T1, T10, T14, T16, T17
Tree Removal- Retention Category C	T1, T2, G2, G3, T10, T15
Tree Removal- Retention Category U	None
Tree Pruning	G6
RPA Building Foundations	G6
RPA New Hard Surface	None
RPA Existing Hard Surface	T1, T4, G4, G8
RPA Underground Services	None Anticipated
RPA Change of Ground Levels	None
RPA Soil Compaction	T1, T4, G4, G8 (prevented by installing tree protection measures)

Other potentially damaging activities include associated with construction site (excavation or the careless use of plant machinery, hazardous materials, or fire).
 All of the above potential impacts are considered in detail throughout this section. Section 3 specifies the measures proposed to minimise all possible potential risks of damage to the retained trees.

Tree Removal
 All trees to be removed are indicated on the Tree Removal Plan and are listed below:

- Retention Category A:** It is proposed to retain all Retention Category A trees.
- Retention Category B:** It is proposed to remove the following Retention Category B trees, T1, T10, T14, T16 and T17. These trees are located so close to the proposed new building that their retention is not possible.
 T1 is an early mature specimen with isolated bark at ground level to its main stem fork, and also at 1cm above ground level to its secondary stem fork (see Photograph 11). Such isolated bark voids indicate rotting and as a consequence this tree is not considered to be a particularly good specimen.
 T14 is an early mature oak with an unbalanced canopy and a significant lean to the east due to its growth being suppressed by the adjacent overstorey, T10 (see Photograph 13). Consequently, it is considered to be in a relatively poor structural condition and is not considered to be a particularly good specimen.
 T16 and T17 are situated on third party land and grow within 1cm of the site boundary. It is proposed to remove these trees, subject to the consent of the owner. Otherwise, it is proposed to prune the overhanging branches back clear to the boundary.
- Retention Category C:** It is proposed to remove the following Retention Category C trees, T1, T4, G2, G3, T10 and T15.
 These are relatively small trees (maximum height 5m). Consequently they are not considered to have a particularly high amenity value and their removal shall not have a major impact on the visual amenity of the locality.
- Retention Category U:** Our survey did not identify any Retention Category U trees.

Mitigation Planning
 Trees support the amenity planning to mitigate against tree loss. I understand that it is proposed to plant several new trees as part of a green development landscaping scheme within the site. Due to the limited space within the site, it is also proposed to enter into a Section 106 agreement with the local authority to enable additional planting within the locality.

Impact on Tree Canopies
 It is proposed to prune back the branches of G6 that are growing towards the proposed building in order to create a clearance distance of 2m. This shall require the removal of relatively small secondary branches which should be pruned back to the main stem. The pruning works should be undertaken sympathetically (referring to BS 3998: 1999 guidance). These works are specified within the Tree Works Schedule in Section 3.
 It may also be required to trim the overhanging foliage of G6, back to the site boundary to allow a clearance distance of 2m from the sub-base of the existing hard surface. However, this should be done on site before the start of any construction works. If any pruning is required, only the removal of relatively small secondary branches shall be required. Such minimal pruning of Retention Category C trees is not considered to be a material planning consideration.
 All other tree canopies shall be unaffected by the proposals.

Impact on Tree Roots
Foundations:
 The foundations for the multi-story office building and the car parking area do not extend into the RPA of any retained tree. Consequently no restrictions on foundation design or implementation are considered from an arboricultural perspective.
 Foundations for the proposed substation do extend into the very edge of the Imperialist Root Protection Area of G6. However, only a very small portion of the Root Protection Area shall be affected (see the Impact Assessment Plan) so the potential impact is considered to be negligible.

New Surface:
 The Impact Assessment Plan indicates where it is proposed to replace the existing hard surface over the (Imperialist) Root Protection Area of T1, T4, G4 and G8 (over pedestrian surface and access road). However, these areas are unlikely to be affected in the area where excavation is proposed (currently above existing hard surfacing). This is because both beneath the existing impermeable hard surfacing are likely to be relatively dry, compacted and unexcavated.
 However, in order to minimise the impact on roots it is proposed to ensure no further than the existing hard surface and its sub-base. This will ensure that tree roots will not have a significant detrimental impact on these trees. Section 3.6 specifies the measures that will be implemented throughout the construction phase.

Underground Services:
 No underground services should be installed through any Root Protection Area without consulting the appropriate authority and if necessary, getting approval from the local authority.

Changes in Ground Levels:
 No changes of ground levels in excess of 100mm within Root Protection Areas shall be made without consulting the relevant and if necessary, getting approval from the local authority.

Demolition Activities
 The tree protection measures specified within Section 3 should be installed prior to the commencement of all demolition activities (including soil stripping) to prevent any detrimental impact on tree health. Where this is not practicable, demolition of structures within Construction Exclusion Zones shall be undertaken very early in the demolition phase and the protective barriers installed immediately thereafter.

Hazardous Materials
 All hazardous materials (including cement and petroleuminous products) will need to be controlled according to COSHH regulations in order to ensure there is no detrimental impact on tree health. However, it shall need to be made to ensure that cement and cement runoff are contained outside of all Root Protection Areas.

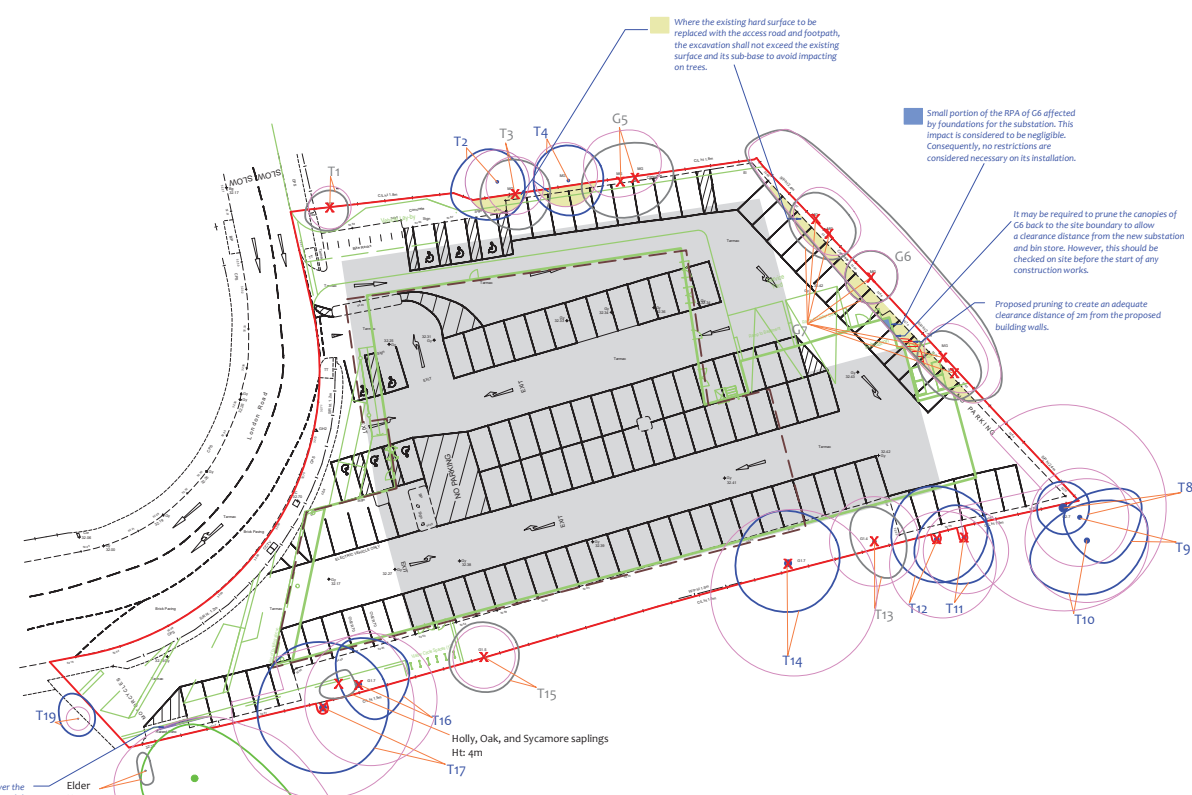
Cables and Site Facilities
 Consideration should be given to the location of any site welfare facilities in terms of potential impact on trees. Where it is proposed to install cabins or site facilities in Root Protection Areas, the appointed arborist should be consulted and approval obtained from the local authority.
 On this site there is ample room for the siting of cables and storage of materials 10m apart during the construction phase without impacting on trees.

Boundary Treatments
 Where it is proposed to amend any boundary features over RPA, the appointed arborist should be consulted and if necessary, approval obtained from the local authority.

Impact of Retained Trees on the Development
 The foundations and any new surface should be designed to accommodate all potential impacts due to future tree rooting activity. These include potential vegetation related subsidence, vegetation related heave, and lifting of surfaces / light structures due to direct root pressure.

Summary
 In order to facilitate the development, it is proposed to remove five Retention Category B trees along with six Retention Category C trees which are located internally to the site. Several new trees are to be planted to mitigate against tree removal.
 G6 requires pruning back away from the proposed in order to create a clearance distance of 2m from the proposed building.
 Pruning works are proposed within the RPAs of T1, T4, G4 and G8. However, there currently exists hard surfacing and so little rooting activity is anticipated where excavation is proposed and excavation shall be undertaken sympathetically.
 Foundations for the substation are proposed within the Root Protection Area of G6. However, the small extent of RPA affected shall ensure no detrimental impact on trees.
 A suitable load spreading surface shall need to be maintained throughout the Retained Activity Zones A. Tree protection measures are specified throughout Section 3 that will ensure no negative impact on retained trees due to construction activity.

Proposed Basement Layout (Dashed Brown)
 Proposed Ground Floor Layout (Pale Green)
 Proposed First Floor Layout (Dashed Brown)



Where the existing hard surface to be replaced with the access road and footpath, the excavation shall not exceed the existing surface and its sub-base to avoid impacting on trees.

Small portion of the RPA of G6 affected by foundations for the substation. This impact is considered to be negligible. Consequently, no restrictions are considered necessary on its installation.

It may be required to prune the canopies of G6 back to the site boundary to allow a clearance distance from the new substation and bin store. However, this should be checked on site before the start of any construction works.

Proposed pruning to create an adequate clearance distance of 2m from the proposed building walls.

A pedestrian surface is proposed over the RPA of G8. Excavation shall not exceed the existing hard surface and its sub-base to avoid impacting on roots

Holly, Oak, and Sycamore saplings
 Ht: 4m

Elder
 Ht: 3.5m

See Section 4 for a more detailed assessment

Drawing No: CCL 1003 / TRP Plan 4
 Title: Tree Removal Plan (Existing Layout with Proposals Overlaid)
 Site: Canelo Park, Epsom
 Scale: 1:500
 Paper Size: A1

Tree Retention Categories
 Retention Category A tree
 Retention Category B tree
 Retention Category C tree
 Retention Category U tree

Trees of high quality with an estimated life expectancy of 40 years, locally important with significant presence or value for wildlife or flora. Retention of these trees is highly desirable.
 Trees of moderate quality with a life expectancy of 20 years. Locally important trees, or trees with good form. Retention of these trees is desirable though loss may be considered to be a material planning consideration.
 Trees of low quality with a life expectancy of 10 years. Retention of these trees is not considered to be a material planning consideration.
 Trees unsuitable for retention due to their very poor condition.

Impact Assessment Plan

(Existing Layout with Proposals Overlaid)

BS 3998: 2013 Root Protection Area (Circular - 100mm diameter)
 Root Protection Area needing amendment due to site conditions, e.g. presence of existing road or building.
 Root Protection Area having been amended to account for site conditions.
 T1 - Tree No 1, G2 - Group No 2, H3 - Hedge No 3

Tree to be removed to facilitate the proposal
 Tree to be removed due to its low condition
 Proposed pruning

MIN - Measured North
 Compex spreads are sometimes measured for an approximate distance by site features. Other more accurate, especially where rows of trees are not aligned N-S or E-W.

Tree Ref.	Species	Height(m)	Caliper (cm)	DBH (cm)	Root Protection Area
T1	Bryonore	4	2.4	18	4.3
T2	Norway Maple	4	3.6	41	6.4
T3	Bryonore	6	3.2	33	5.7
T4	Norway Maple	6	3.6	41	6.4
G6	Norway Maple	7	3.6	41	6.4
G8	Bryonore And Ash	5	1.8	10	3.2
G7	Bryonore And Ash	7	3.0	28	5.3
T8	Lombardy Poplar	20	10.8	366	19.1
T9	Oak	9	5.5	96	8.8
T10	Ash	12	7.0	152	12.3
T11	Ash	10	4.8	72	8.5
T12	Bryonore	10	6.0	113	10.8
T13	Bryonore	10	5.0	80	8.9
T14	Bryonore	12	6.4	275	16.6
T15	Bryonore	7	3.5	38	6.2
T16	Oak	12	2.4	174	13.2
T17	Bryonore	15	9.0	254	16.0
G18	Common Pear	15	9.0	254	16.0
T19	Elder Birch	5	1.3	5	2.3





Genotin Road, Enfield

ACCURATE VISUAL REPRESENTATIONS (AVR3)



Viewpoint A Existing





Viewpoint B Existing









AVR Non Technical Methodology

Overview

The process of generating verified views (also referred to as accurate visual representations (AVR)) for the Proposed Development at Genotin Road, Enfield was carried out by Troopers Hill.

Troopers Hill use a methodology that is compliant with relevant sections of: The Landscape Institute/IEMA Guidelines for Landscape and Visual Impact Assessment (3rd edition 2013); The Landscape Institute Advice Note 01/11 Photography and Photomontage in Landscape and Visual Impact Assessment and The Revised SPG London View Management Framework (March 2012).

High quality/resolution photographs were taken from the agreed locations by Troopers Hill. An adequate number of visible features were subsequently surveyed, including the precise location and bearing of the camera. A development model was generated to correct geographical co-ordinates. With a known camera position and orientation, photographic and surveyed existing visible features, the development model was accurately aligned to the photograph.

The AVRs produced have an accuracy tolerance of +/-20cm.

For correct perspective viewing, the AVRs presented should be viewed at a distance of approximately 300mm when printed at A3.

Site visit

Troopers Hill visited the site on the 11th June 2018 to obtain viewpoint photography. The view positions were documented using photography of the exact positions (marked with paint) which were passed on to the surveyor who later visited the site to record the precise co-ordinates.

Photography

For each agreed photoviewpoint location, a high resolution photograph was taken with a 35mm (full frame) digital SLR camera. The location at which the photograph was taken was marked (where possible) with a nail and / or spray paint to allow the surveyor to record the precise location on a subsequent visit. The camera was levelled horizontally and laterally by means of a tripod mounted levelling base and two camera mounted spirit levels. A tilt/shift or perspective control lens was used to allow vertical rise while avoiding convergence of vertical elements.

Lens Selection Criteria

In order to capture the full extent of the proposed development and an appropriate amount of contextual built form, local townscape views were photographed using a 24mm perspective control lens, in landscape orientation (effective 76° horizontal field of view).

Equipment Used for Photography

- Canon 5DSR digital SLR camera (35mm) (50 million pixel)
- Canon TS-e 24mm f/3.5 LII
- Remote (cabled) shutter release
- Tripod indexed pan head
- Levelling base with bubble level
- Camera (hot shoe) mounted two axis spirit level
- Plumb bob
- Street marking paint
- Hilti nails

Post Production

Each base photograph has had a level of basic colour correction applied to it so that it best represents the impression of the scene as the photographer experienced it in person.

This processing is predominately done to the 16bit RAW file using Adobe Camera Raw and Photoshop. It includes, but is not limited to, adjustments in; colour temperature and tint; levels such as exposure and contrast; shadow and highlight recovery; sky recovery through the use of gradient corrections; and other post processing effects such as sharpening and noise reduction.

Survey

For each agreed photoviewpoint location an instructional document was released to the survey subcontractor. The surveyor was instructed (by means of a marked up photograph, map and tripod (in situ) photograph) to record a range of contextual reference points.

Survey Equipment Required

- Leica 1200 series GPS Smartnet enabled dual receiver (GPS and GLONASS)
- Leica Total Station (1201 or TS16) 1' accuracy with 1000m reflectorless laser

Field Survey Methodology

- Camera locations: where possible, the camera position will be used as a setup point for the total station, enabling the re-creation of the view as seen in the imagery and reducing the risk of wrong interpretation of detail. Connection is usually via GPS Smartnet derived control points in OSGB datum and grid. 3-4 control stations are used, to ensure long distance accuracies and to identify possible outliers.
- Reference points visible in the photography are measured with reflectorless means from the total station. If long distance views have suitable detail too far from the camera station, further setups are used closer to the detail. Common visible detail points are observed from different setup points to check and increase accuracy achieved.
- Accuracies of camera positions are to the low centimetre, while accuracies of surveyed detail will vary due to setup geometry and distance, but will be usually in the low centimetre but always below 30 centimetre (if views are over 5km).

Data Processing & Delivery

Data is processed using industry standard software (Leica GeoOffice and TerraModel) to create points listings. A3 verification plots or digital photos are marked up with the surveyed points to aid identification. All points are to OSGB36 grid and datum, to allow the use of common Ordnance Survey products and industry standard site surveys.

The Proposed Development

Troopers Hill imported a 3D model of the proposed development supplied by the project architect.

The resulting model was checked for accuracy against plans and elevations and subsequently aligned to the OSGB36 co-ordinate system.

The Verification Process

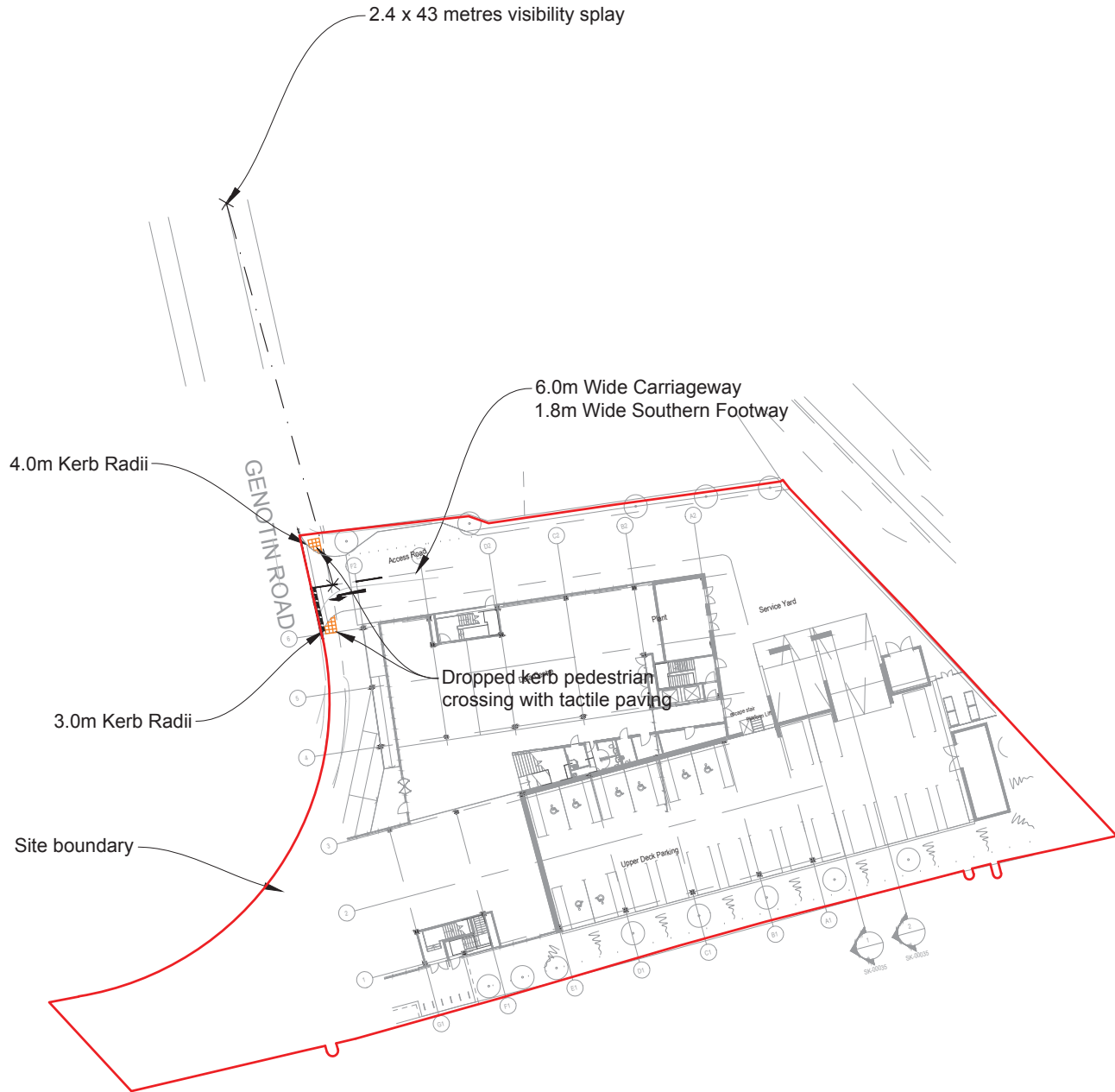
The collected survey reference point data and camera location data was imported into the 3D model environment from the delimited text file (relative to the OSGB36 co-ordinate system) by means of a proprietary script.

At each photoviewpoint location a virtual camera was set up in the 3D software using the coordinates provided by the surveyor. The 3D coordinates of the survey reference points were used to create an accurate 'point cloud' model of the contextual surveyed parts of the scene. The scene was verified by matching the contextual surveyed points to the photograph. To do this, for each photoviewpoint, two renders* were made from the 3D model from the same virtual camera: one render showed only the development (in the chosen method of presentation); the other showed only the survey reference point data.

Using a photo editing package [Adobe Photoshop CC 2018], the photography, survey reference point render and proposed development render were aligned.

With the rendered proposals aligned to the photography, masks were applied to the image to hide features of the proposed development that would be occluded by existing features. This process was performed on all views.

* Rendering is the process of generating an image from a model (or models in what collectively could be called the 3D environment), by means of computer programs - specifically, in this case Chaos Group V-Ray 3.2 for Autodesk 3Ds Max 2015.



- Notes**
1. Do not scale this drawing. All dimensions must be checked/ verified on site. If in doubt ask.
 2. This drawing is to be read in conjunction with all relevant architects, engineers and specialists drawings and specifications.
 3. All dimensions in millimetres unless noted otherwise. All levels in metres unless noted otherwise.
 4. Any discrepancies noted on site are to be reported to the engineer immediately.

Key Plan

Legend

Rev	Date	Details of issue / revision	Drawn	Reviewed
P4	24.07.18	Updated Masterplan	CC	MA
P3	11.07.18	For information: Amended Redline Boundary	ATH	MA
P2	05.07.18	For information	ATH	MA
P1	29.06.18	For information	ATH	MA

Issues & Revisions

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Client
Stoford Properties Ltd and Metaswitch

Project Title
Genotin Road, Enfield, London

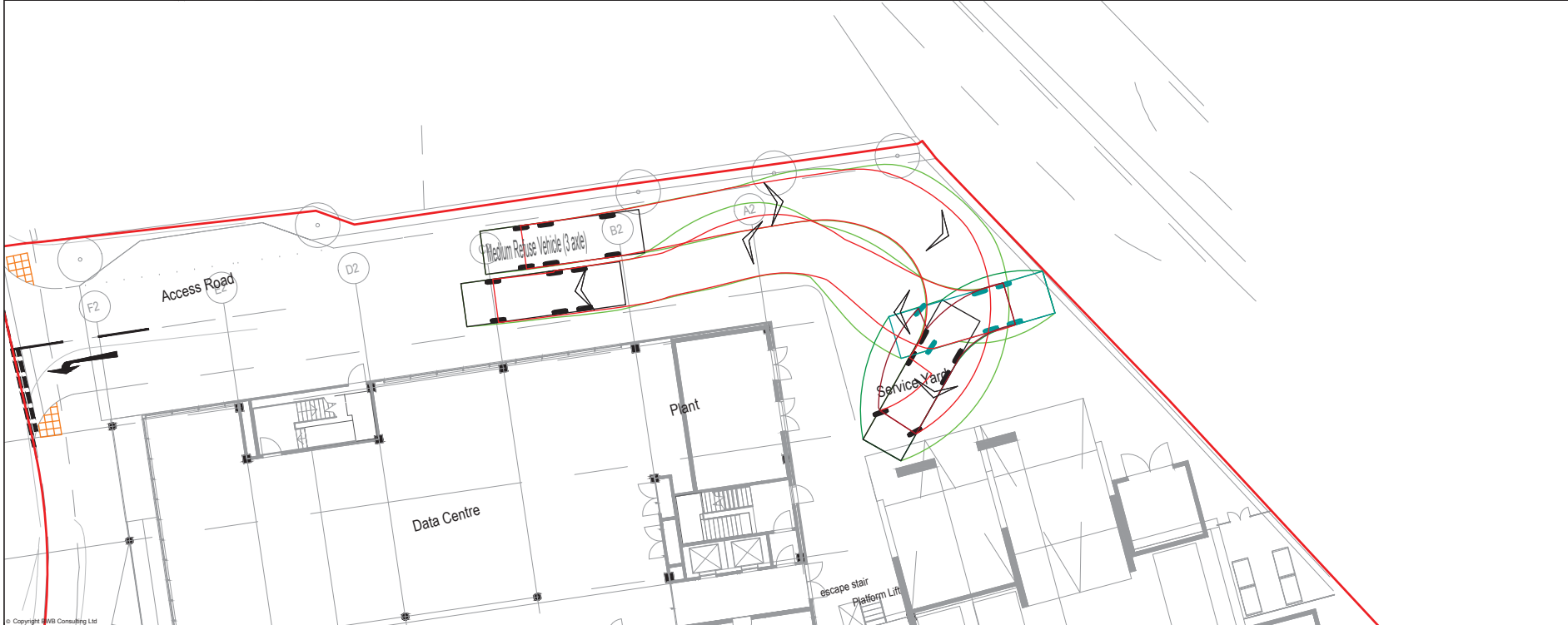
Drawing Title
Proposed Junction General Arrangements

Drawn:	AT. Huneidi	Reviewed:	M. Addison
BWB Ref:	BMT 2330	Date:	11.07.18
		Scale:	AS 1:250

Drawing Status

For Information

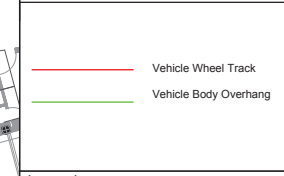
Project - Originator - Zone - Level - Type - Role - Number	Status	Rev
GRE-BWB-GEN-01-DR-TR-100	S2	P4



- Notes**
1. Do not scale this drawing. All dimensions must be checked/verified on site. If in doubt ask.
 2. This drawing is to be read in conjunction with all relevant architects, engineers and specialists drawings and specifications.
 3. All dimensions in millimetres unless noted otherwise. All levels in metres unless noted otherwise.
 4. Any discrepancies noted on site are to be reported to the engineer immediately.

Key Plan

Medium Refuse Vehicle (3 axle)	9.010m
Overall Length	2.450m
Overall Body Height	3.742m
Min Body Ground Clearance	0.299m
Track Width	2.450m
Lock-to-lock time	4.50s
Curb to Curb Turning Radius	8.200m



Legend

Rev	Date	Details of issue / revision	Drawn	Rev
P6	24.07.18	Updated Masterplan	CC	MA
D3	11.07.18	For Information: Amended Redline Boundary	ATH	MA
D2	05.07.18	For Information	ATH	MA
P1	29.06.18	For Information	ATH	MA

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Client

Stoford Properties Ltd and Metaswitch

Project Title

Genotin Road, Enfield, London

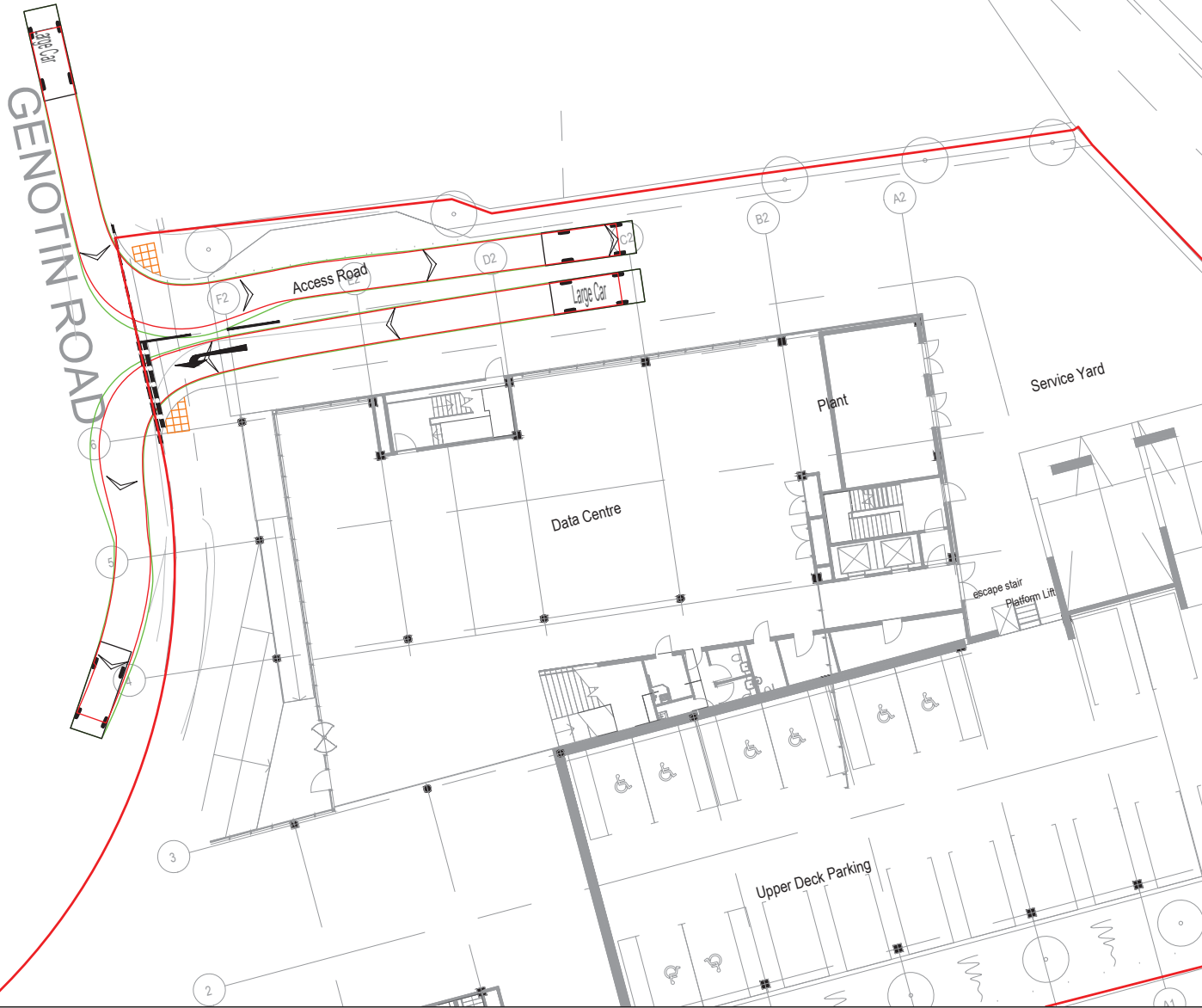
Drawing Title

Swept Path Analysis: Refuse Vehicle Access & Egress

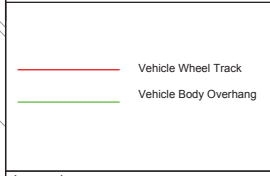
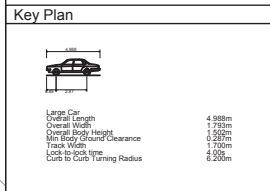
Drawn:	AT, Huneidi	Reviewed:	M, Addison
BWB Ref:	BMT 2330	Date:	11.07.18
Scale:	A3	Scale:	A3

For Information

Project - Originator - Zone - Level - Type - Role - Number	Status	Rev
GRE-BWB-GEN-01-DR-TR-110	S2	P4



- Notes**
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 - This drawing is to be read in conjunction with all relevant architects, engineers and specialists drawings and specifications.
 - All dimensions in millimetres unless noted otherwise. All levels in metres unless noted otherwise.
 - Any discrepancies noted on site are to be reported to the engineer immediately.



Legend

Rev	Date	Details of issue / revision	Drawn	Rev
P6	24.07.18	Updated Masterplan	CC	MA
P3	11.07.18	For Information: Amended Redline Boundary	ATH	MA
P2	05.07.18	For Information	ATH	MA
P1	29.06.18	For Information	ATH	MA

Issues & Revisions

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Client
Stoford Properties Ltd and Metaswitch

Project Title
Genotin Road, Enfield, London

Drawing Title
Swept Path Analysis: Large Car Two-way Movement

Drawn:	AT, Hureldi	Reviewed:	M. Addison
BWB Ref:	BMT 2330	Date:	11.07.18
Scale:	AS	Scale:	1:250

For Information

Project - Originator - Zone - Level - Type - Role - Number	Status	Rev
GRE-BWB-GEN-01-DR-TR-111	S2	P4

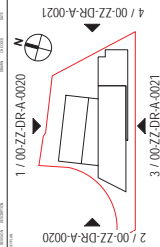
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NOTES

Materials (subject to planning approval)

- Cladding - Curtain Wall System - Solid Panel
PVC/Anodised Aluminium
Colour - Silver Grey
- Panel System
PVC/Anodised Aluminium Louvers
Colour - Silver Grey
- Cladding - Curtain Wall System
Glass Panel - Clear glazing with colour matched back-painted glass spandrel panels at head of window
Mullion colour - Silver Grey
- Cladding - Curtain Wall System
Anodised Aluminium
Colour - Silver Grey
- Cladding - Curtain Wall System with Brise Soleil
Mullions of Silver Grey, Dark Grey & Bronze colours.
Recom. brushed stone, stone cladding with matching pavers.
Sandstone or similar colour.
- Entrance Canopy
Recom. brushed stone, stone cladding to match plain.
Sandstone or similar colour
- Roof Soffit - Metal Cladding Colour 'RBC'
- External Balustrade
Glazed with Stainless Steel hand rail
- Opaque/ frosted Glazing
- External Doors - Solid / Glazed
Anodised Aluminium Finish
Colour - Silver Grey
- Cladding - Curtain Wall System
Glass Panel With Integrated Solar Control - Clear glazing with colour matched back-painted glass spandrel panels at head of window
Mullion colour - Silver Grey

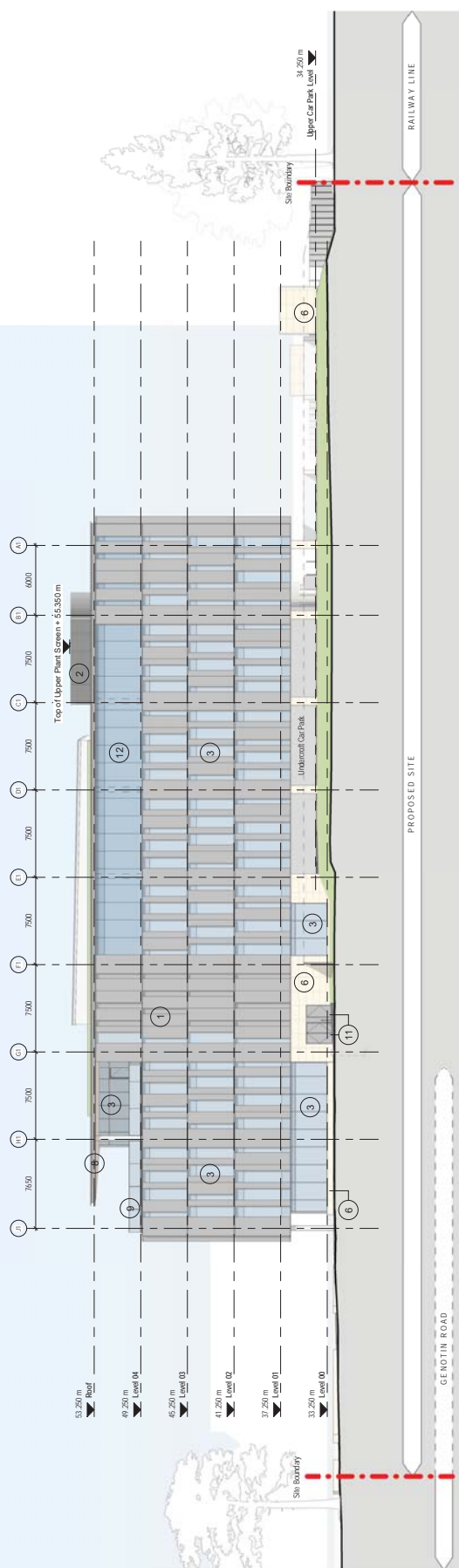
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01	ISSUED FOR PERMIT	20/04/18
02	ISSUED FOR PERMIT	20/04/18
03	ISSUED FOR PERMIT	20/04/18
04	ISSUED FOR PERMIT	20/04/18
05	ISSUED FOR PERMIT	20/04/18
06	ISSUED FOR PERMIT	20/04/18
07	ISSUED FOR PERMIT	20/04/18
08	ISSUED FOR PERMIT	20/04/18
09	ISSUED FOR PERMIT	20/04/18
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11	ISSUED FOR PERMIT	20/04/18
12	ISSUED FOR PERMIT	20/04/18



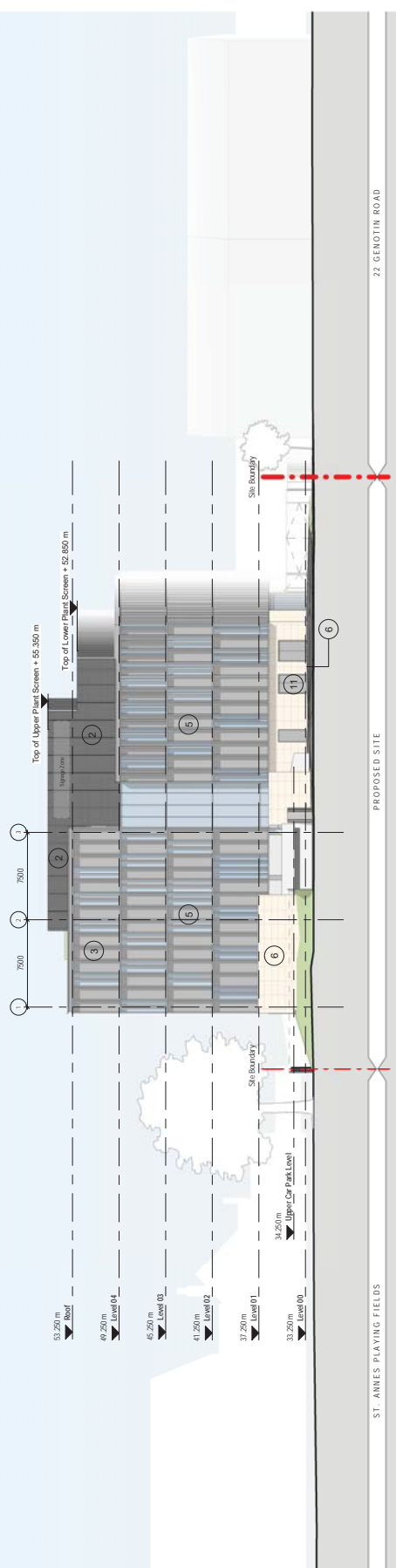
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P2007486 Public Use
Proposed GA Elevations
Sheet 2
7486-BDP-00-ZZ-DR-A-0021 S2 | P9



3 GA Elevation - South
1:200



4 GA Elevation - East
1:200

